



ATTACHMENTS

ORDINARY COUNCIL MEETING

Tunbridge Hall
99 Main Road, Tunbridge

Tuesday, 24th February 2026
10.00 a.m.

Item 5.1	Draft Council Meeting Minutes (Open) – 28 th January 2026
Item 5.2.1	Lake Dulverton & Callington Park Committee Meeting Minutes – 16 th February 2026
Item 5.3.2	TasWaste South Quarterly Report: October – December 2025
Item 12.1.1	Development Application Documents – DA2500105
Item 12.4.1	Supreme Court of Tasmania - Dourias MGH Pty Ltd v Clarence City Council (2025) Public Open Space Policy
Item 12.4.2	Submission to the Draft STRLUS Jensen Plus – Bagdad/Mangalore Response to STRLUS
Item 13.10.1	Black Gate Road Traffic Counter
Item 15.4.1	Community Consultation – Oatlands Village Square
Item 18.1	Deed of Termination – Dept of State Growth and Southern Midlands Council

SOUTHERN
MIDLANDS
COUNCIL



MINUTES

ORDINARY COUNCIL MEETING

Wednesday 28th January 2026
10.00 a.m.

Woodsdale Hall
2310 Woodsdale Road, Woodsdale

DRAFT

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OPEN COUNCIL MINUTES

MINUTES OF AN ORDINARY MEETING OF THE SOUTHERN MIDLANDS COUNCIL HELD ON WEDNESDAY 28th JANUARY 2026 AT THE WOODSDALE HALL, 2310 WOODSDALE ROAD, WOODSDALE COMMENCING AT 10.00 A.M.

In accordance with Regulation 8 of the *Local Government (Meeting Procedures) Regulations 2025*, Mayor Batt advised all attendees that:

- a) this meeting is being recorded;
- b) all persons attending the meeting are to be respectful of, and considerate towards, other persons attending the meeting; and
- c) language and conduct at the meeting that could be perceived as offensive, defamatory or threatening to a person attending the meeting, or listening to the recording, is not acceptable.

1. PRAYERS

Reverend Karen Woolford recited prayers.

2. ACKNOWLEDGEMENT OF COUNTRY

Mayor E Batt recited Acknowledgement of Country.

3. ATTENDANCE

Mayor E Batt, Deputy Mayor K Dudgeon, Cllr A E Bisdee OAM, Cllr D Blackwell, Cllr B Campbell and Cllr D Fish.

Mr T Kirkwood (General Manager) *arrived at 10.46 a.m.*, Mr A Benson (Deputy General Manager), Mr D Richardson (Manager Infrastructure and Works), Mr G Finn (Manager Development and Environmental Services), Mrs Amanda Burbury (Finance Officer), Ms W Young (Manager Community & Corporate Development), Mr D Mackey (Council Planning Consultant) and Ms J Crosswell (Executive Assistant).

4. APOLOGIES

Cllr F Miller

5. MINUTES

5.1 Ordinary Council Meeting

DECISION

Moved by Clr A E Bisdee OAM, seconded by Clr B Campbell

THAT the Minutes (Open Council Minutes) of the Council Meeting held 10th December 2025 be confirmed.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

5.2 Annual General Meeting

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr B Campbell

THAT the Minutes of the Annual General Meeting held 10th December 2025 be confirmed.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

5.3 Special Committees of Council Minutes

5.3.1 Special Committees of Council - Receipt of Minutes

Nil.

5.3.2 Special Committees of Council - Endorsement of Recommendations

Nil.

5.4 Joint Authorities (Established Under Division 4 Of The *Local Government Act 1993*)

5.4.1 Joint Authorities - Receipt of Minutes

DECISION

Moved by Clr D Blackwell, seconded by Clr A E Bisdee OAM

THAT the minutes of the above Joint Authority be received.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

5.4.2 Joint Authorities - Receipt of Reports (Annual & Quarterly)

Nil.

6. NOTIFICATION OF COUNCIL WORKSHOPS

DECISION

Moved by Cllr A E Bisdee OAM, seconded by Cllr D Blackwell

THAT the information be received.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Cllr A E Bisdee OAM	✓	
Cllr D Blackwell	✓	
Cllr B Campbell	✓	
Cllr D Fish	✓	

7. COUNCILLORS – QUESTION TIME

7.1 Questions (On Notice)

Regulation 30 of the *Local Government (Meeting Procedures) Regulations 2025* relates to Questions on notice. It states:

- (1) *A councillor, at least 7 days before an ordinary council meeting or a council committee meeting, may give written notice to the general manager of a question in respect of which the councillor seeks an answer at that meeting.*
- (2) *An answer to a question on notice must be in writing.*

Nil.

7.2 Questions Without Notice

Clr A E Bisdee OAM – Theft at Campania Football Clubrooms: Copper wiring has once again been stolen from the Campania Football Club during renovations of their Clubrooms. What impact has this had on the building? What costs are associated with the loss?
Manager Infrastructure and Works – this is the second time this has happened in recent weeks at this location. We have been in contact with Sorell Police regarding extra patrols of the area and will be discussing this further at an upcoming meeting with police and the General Manager. Costs are estimated at around \$5,000. Insurance excess is around \$5,000 so a claim will not be submitted. Commented that this is extremely frustrating and is slowing down the build.

Clr A E Bisdee OAM – Have we finalised negotiations with Vos Construction for the Oatlands Aquatic Centre?
Deputy General Manager – Negotiations are yet to be finalised at this point in time as there are ongoing issues with claim and counter claim about the solar panels. A meeting has been organised with Patrick Stanton. Council are still withholding approximately \$400,000 for the project.

Clr D Fish – Commented on the success of the Southern Midlands Council Australia Day event held at Broadmarsh Hall. Thanked Jemma Crosswell and Donna Blackwell and the Broadmarsh Hall Committee.

Clr B Campbell – Has received complaints regarding long grass being a snake and fire hazard at 38 Glenelg Street, Oatlands.
Manager Development & Environmental Services – Advised that Councils Compliance Officer undertakes regular inspections of properties to identify potential fire hazards and he will request this property be inspected.

Clr B Campbell – Update - Old Colebrook Shop verandah – during recent winds some pieces have fallen off the roof. When will the damage be repaired?
Manager Infrastructure and Works – Has spoken to the property owner who has been in contact with the Heritage Council and approval by the Heritage Council has been given for work to go ahead. Currently the owner is awaiting quotes.

Clr B Campbell – Update – Woodsdale Football Club
Deputy General Manager – Taken on notice – further advice to be received from the General Manager.

8. DECLARATIONS OF PECUNIARY INTEREST

Mayor E Batt declared an interest in Agenda Item 12.1.1 - (DA2500091) for Compost Facility and Associated Works at 'Somerton' 923 Lovely Banks Road, Colebrook (CT 134018/1), owned by R C Jones. In making the declaration Mayor Batt advised that he did not have a 'pecuniary interest', however he is a neighbour and that he would see that it would be inappropriate for him to be part of the decision making process.

9. CONSIDERATION OF SUPPLEMENTARY ITEMS TO THE AGENDA

In accordance with the requirements of Part 2 Regulation 8 (6) of the *Local Government (Meeting Procedures) Regulations 2025*, the Council, by absolute majority may decide at an ordinary meeting to deal with a matter that is not on the agenda if the General Manager has reported –

- (a) the reason it was not possible to include the matter on the agenda; and
- (b) that the matter is urgent; and
- (c) that advice has been provided under section 65 of the Act.

Nil.

10. PUBLIC QUESTION TIME (SCHEDULED FOR 10.30 A.M.)

Ms Alyson Scott – Woodsdale Back Road - poplar trees have become overgrown and therefore quite dangerous as they are preventing vehicles from properly seeing oncoming traffic and has witnessed near misses.

Manager Infrastructure and Works – Will advise contractor to cut these back when slashing is occurring in the area. These trees will eventually be removed.

Mrs Ann Scott – Queried why fallen trees are often cut up and not completely removed?
Manager Infrastructure and Works – Often this occurs in an urgent 'call-out' situation and Council are unable to remove the whole tree but will take comments on notice for future.

Mrs Ann Scott – Are there plans to seal Buckland Road? Has noticed an increase in traffic recently and is becoming more dangerous.
Manager Infrastructure and Works – Advised that Council can apply for any grants that become available (similar to the recent Interlaken Road upgrades).

10.1 Permission to Address Council

Nil.

**11. MOTIONS OF WHICH NOTICE HAS BEEN GIVEN UNDER
REGULATION 16 (5) OF THE LOCAL GOVERNMENT (MEETING
PROCEDURES) REGULATIONS 2025**

Nil.

12. COUNCIL ACTING AS A PLANNING AUTHORITY PURSUANT TO THE LAND USE PLANNING AND APPROVALS ACT 1993 AND COUNCIL'S STATUTORY LAND USE PLANNING SCHEME

Session of Council sitting as a Planning Authority pursuant to the Land Use Planning and Approvals Act 1993 and Council's statutory land use planning schemes.

12.1 Development Applications

12.1.1 Development Application (DA2500091) for Compost Facility and Associated Works at 'Somerton' 923 Lovely Banks Road, Colebrook (CT 134018/1), owned by R C Jones.

Mayor E Batt declared an interest and departed the meeting. Deputy K Dudgeon took the chair.

DECISION

Moved by Clr B Campbell, seconded by Clr D Blackwell

THAT , in accordance with the provisions of the *Tasmanian Planning Scheme - Southern Midlands* and section 57 of the *Land Use Planning & Approvals Act 1993*, Council **APPROVE** the Development Application (DA2500091) for Compost Facility and Associated Works at 'Somerton' 923 Lovely Banks Road, Colebrook (CT 134018/1), owned by R C Jones subject to conditions detailed below.

CONDITIONS – Part A (Council Permit)

Concordance with the application and permit conditions:

- (1) The use or development must be carried out substantially in accordance with the application for planning approval, the endorsed drawings and reports and with the conditions of this permit and must not be altered or extended without the further written approval of Council or, as necessary, the Environmental Protection Authority.

Environmental Protection Authority – Conditions of Approval:

- (2) The entity responsible for the approved compost activity (referred to in this permit as the Operator) must comply with the conditions contained in Schedule 2 Permit Part B, which the Board of the Tasmanian Environmental Protection Authority has required the planning authority to include in the permit, pursuant to section 25(5) of the Environmental Management and Pollution Control Act 1994.

Amenity

- (3) All external metal building surfaces of the approved development must be clad in non-reflective pre-coated metal sheeting, or painted to the satisfaction of the Council's Manager Development and Environmental Services.

Parking and Access

- (4) The internal access road, parking and associated turning, loading and unloading areas servicing the development must be designed and constructed to the satisfaction of Council's Manager Infrastructure & Works and must include:
 - a. adequate turning space on site to allow vehicles to enter and leave the site in a forward direction;
 - b. an all-weather pavement;

- c. passing bays; and
 - d. stormwater drainage.
- (5) The internal driveway accessing the compost facility is to be constructed and maintained to avoid erosion and sediment transfer or de-stabilisation of the soil on site, or on adjacent properties, and be to the standard required by Council’s Manager Infrastructure & Works.
- (6) All areas set-aside for parking and associated turning, loading and unloading areas and access must be completed prior to cartage of any materials pursuant to the approved compost activity and must continue to be maintained to the satisfaction of Council’s Manager Infrastructure & Works.
- (7) The existing access on Lovely Banks Road must be upgraded and constructed in accordance with LGAT Standard Drawing TSD-R05-v3 “Truck Access to Rural Properties ‘Type A’” (including new culvert and headwalls) and per the approved Traffic Impact Assessment prepared by Terry Eaton, dated November 2022 and be to the satisfaction of Council’s Manager Infrastructure & Works.
- (8) Truck turning signs shall be installed on either side of the access on Lovely Banks Road, with exact locations to be confirmed by Council’s Manager Infrastructure & Works.
- (9) Any damage to Council’s infrastructure, as a consequence of any of the works associated with this development, shall be repaired by the Operator at the Operator’s expense.

Stormwater

- (10) Stormwater produced by the approved use and development must be retained and disposed of on-site in accordance with a Certificate of Likely Compliance or Plumbing Permit issued by the Permit Authority in accordance with the *Building Act 2016* (Tas).

Wastewater

- (11) Wastewater (including sewage) produced by the approved use and development must discharge to and be managed by an on-site waste disposal system in accordance with a Certificate of Likely Compliance or Plumbing Permit issued by the Permit Authority in accordance with the *Building Act 2016* (Tas).

Construction amenity

- (12) The approved development must only be carried out between the following hours unless otherwise approved in advance by Council’s Manager Development and Environmental Services:

Monday to Friday	7:00 a.m. to 6:00 p.m. (inclusive)
Saturday	8:00 a.m. to 6:00 p.m. (inclusive)
Sunday and State-wide public holidays	10:00 a.m. to 6:00 p.m. (inclusive)

- (13) All works associated with the development of the Site must be carried out in a manner that does not unreasonably cause injury to, or unreasonably prejudice or affect the amenity, function and safety of, any adjoining or adjacent land, and of any person therein or in the vicinity thereof, by reason of the:
- a) emission of noise, artificial light, vibration, odour, fumes, smoke, vapour, steam, ash, dust, wastewater, waste products, grit or similar;
 - b) transportation of materials, goods and commodities to and from the Site;

- c) obstruction of any public footway or highway; or
- d) appearance of any building, works or materials.
- (14) Any accumulation of vegetation, building debris or other unwanted material must be disposed of by removal from the Site in an approved manner. No burning of any materials on site will be permitted unless first approved in writing by Council's Manager Development and Environmental Services.
- (15) Public roadways and/or footpaths must not be used for the:
 - a) storage of any construction materials or wastes;
 - b) loading/unloading of any vehicle or equipment; or
 - c) carrying out of any work, process or tasks associated with the project during the construction period, or otherwise.
- (16) The developer must make good and/or clean any footpath, road surface, drain, culvert or other like matter damaged, soiled or obstructed by the development, to the satisfaction of Council's Municipal Engineer.

THE FOLLOWING ADVICE APPLIES TO THIS PERMIT

- A. Subject to subsections 53(3), 53(4) and 53(6) of the *Land Use Planning and Approvals Act 1993* (Tas), this permit takes effect at the expiration of 14 days from the day on which the notice of the granting of this permit was served on all persons who have a right of appeal (*Land Use Planning and Approvals Act 1993* (Tas), subsection 53(1)).
- B. Pursuant to subsection 57(7) of the *Land Use Planning and Approvals Act 1993* (Tas), this permit remains in effect until it:
 - a) lapses under subsection 53(5) of the *Land Use Planning and Approvals Act 1993* (Tas); or
 - b) expires as a result of a condition or restriction contained in this permit; or
 - c) is cancelled under section 65G of the *Land Use Planning and Approvals Act 1993* (Tas).
- C. This planning permit is in addition to the requirements of the *Building Act 2016* (Tas). Approval in accordance with the *Building Act 2016* (Tas) may be required prior to works commencing. A copy of the *Directors Determination – categories of Building Work and Demolition Work* is available via the CBOS website: *Director's Determination - Categories of Building and Demolition Work* (PDF, 504.4 KB) or for Low Risk Building Work information go to: *Consumer Guide to Low Risk Building and Plumbing Work*.
- D. This permit does not imply that any other approval required under any other legislation, by-law or other regulatory regime has been granted.
- E. This permit does not take effect until all other approvals required for the use or development to which the permit relates have been granted (*Land Use Planning and Approvals Act 1993* (Tas), subsection 53(4)).
- F. The issue of this permit does not ensure compliance with the provisions of the *Threatened Species Protection Act 1995* (Tas) or the *Environmental Protection and Biodiversity Protection Act 1999* (Cth). The applicant may be liable to complaints in relation to any non-compliance with these Acts and may be required to apply to the Threatened Species Unit of the Department of Tourism, Arts and the Environment or the Commonwealth Minister for a permit.

- G. The issue of this permit does not ensure compliance with the provisions of the *Aboriginal Relics Act 1975* (Tas). If any aboriginal sites or relics are discovered on the land, stop work and immediately contact the Tasmanian Aboriginal Land Council and Aboriginal Heritage Unit of the Department of Tourism, Arts and the Environment. Further work may not be permitted until a permit is issued in accordance with the *Aboriginal Relics Act 1975* (Tas).
- H. The operator is required to apply to NRE for a dam works permit.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

Mayor E Batt returned to the meeting 10.31 a.m. and resumed the chair.

The General Manager entered the meeting at 10.46 a.m.

12.1.2 Application to Amend the Planning Scheme (Reference DA2500/105), 20 Interlaken Road, Oatlands (CT44807/6, CT20462/5)

Agenda Item 12.1.2 was withdrawn at the request of the applicant.

DECISION

Moved by Clr A E Bisdee OAM, seconded by Deputy Mayor K Dudgeon

THAT Agenda Item 12.1.2 be withdrawn from the meeting at the request of the applicant.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

12.2 Subdivisions

Nil.

12.3 Municipal Seal (Planning Authority)

Nil.

12.4 Planning (Other)

12.4.1 Potential Dark Sky Reserve

DECISION

Moved by Clr A E Bisdee OAM, seconded by Deputy Mayor K Dudgeon

THAT the proposal to investigate obtaining International Dark Sky Reserve status for all or part of the Southern Midlands Municipal Area be investigated, as outlined above in ‘A Way Forward: Stage 1 - Investigation’.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell		✓
Clr D Fish	✓	

12.4.2 Bagdad Mangalore Structure Planning Project

DECISION

Moved by Clr D Fish, seconded by Clr B Campbell

THAT: The *Bagdad Mangalore Structure Plan – January 2026*, as enclosed with this report, be endorsed by Council and that the previous *Bagdad Mangalore Structure Plan 2010* be rescinded.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

**[THIS CONCLUDES THE SESSION OF COUNCIL
ACTING AS A PLANNING AUTHORITY]**

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr D Blackwell

THAT Council adjourn for morning tea at 11.05 a.m.

CARRIED

DECISION		
Councillor	Vote	Vote

	FOR	AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr D Blackwell

THAT Council reconvene at 11.27 a.m.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

13. OPERATIONAL MATTERS ARISING (STRATEGIC THEME – INFRASTRUCTURE)

13.1 Roads

Strategic Plan Reference 1.1

Maintenance and improvement of the standard and safety of roads in the municipal area.

Nil.

13.2 Bridges

Strategic Plan Reference 1.2

Maintenance and improvement of the standard and safety of bridges in the municipality.

Nil.

13.3 Walkways, Cycle Ways and Trails

Strategic Plan Reference 1.3

Maintenance and improvement of the standard and safety of walkways, cycle ways and pedestrian areas to provide consistent accessibility.

Nil.

13.4 Lighting

Strategic Plan Reference 1.4

Ensure adequate lighting based on demonstrated need / Contestability of energy supply.

Nil.

13.5 Buildings

Strategic Plan Reference 1.5

Maintenance and improvement of the standard and safety of public buildings in the municipality.

Nil.

13.6 Sewer / Water and Energy

Strategic Plan Reference(s) 1.6

Increase the capacity of access to reticulated sewerage services / Increase the capacity and ability to access water to satisfy development and Community to have access to reticulated water.

Nil.

13.7 Drainage

Strategic Plan Reference 1.7

Maintenance and improvement of the town storm-water drainage systems.

Nil.

13.8 Waste

Strategic Plan Reference 1.8

Maintenance and improvement of the provision of waste management services to the Community.

Nil.

13.9 Information, Communication Technology

Strategic Plan Reference 1.9

Improve access to modern communications infrastructure.

Nil.

13.10 Officer Reports – Infrastructure & Works

13.10.1 Manager – Infrastructure & Works Report

QUESTIONS WITHOUT NOTICE TO MANAGER, INFRASTRUCTURE & WORKS

Clr D Fish – questioned whether access to water for maintenance grading / gravel re-sheeting purposes was an issue given the dry conditions?

Manager Infrastructure & Works confirmed that this is an issue but at present, there is only limited maintenance grading being undertaken, and no gravel re-sheeting at the present time.

Deputy Mayor K Dudgeon – Woodsdale Back Road – Poplar Trees – Safety Issue.

Manager Infrastructure & Works to inspect and take appropriate action.

Deputy Mayor K Dudgeon – Lemont Road – Sharp Corner (vicinity of Manning's towards Fonthill) – can a sign be installed to warn motorists of sharp corner.

Manager Infrastructure & Works to inspect and take appropriate action.

Deputy Mayor K Dudgeon – Lake Dulverton Walking Track – vegetation encroaching onto walkway

Clr B Campbell – Tunnack Main Road / Blackgate Road intersection - sight distance issue – made reference to the reply received from the Department of State Growth. Asked whether a vehicle counter can be placed on Blackgate Road to assess the number of vehicles using the junction?

Manager Infrastructure & Works confirmed that the Traffic Counter can be placed on the road.

Clr B Campbell – Guide Posts – commented in relation to the number of guide posts that have been damaged and the work programme to renew or repair.

Manager Infrastructure & Works informed the meeting of the work, health and safety issues associated with replacing a single guide post which prevents ad hoc works and the need to undertake a dedicated programme of works.

Clr B Campbell – Midland Highway, Tunbridge – Southern Entrance (South bound acceleration lane) – any feedback from the Department of State Growth?

Manager Infrastructure & Works informed the meeting that the response received from the DSG confirmed that no funding is available to undertake these improvements.

Clr B Campbell – Road defects (i.e. potholes etc.) – mentioned Stonehenge Road; Inglewood Road and Woodsdale Road.

Manager Infrastructure & Works to inspect and take appropriate action.

Deputy Mayor K Dudgeon – Kempton Recreation Ground (Public Toilets) – made general comment in relation to the standard of the facility.

General Manager indicated that the long-term plan involves demolition and construction of a replacement toilet facility.

RECOMMENDATION

THAT the Infrastructure & Works Report be received and the information noted.

DECISION

Moved by Cllr B Campbell, seconded by Cllr D Fish

THAT the Infrastructure & Works Report be received and the information noted.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Cllr A E Bisdee OAM	✓	
Cllr D Blackwell	✓	
Cllr B Campbell	✓	
Cllr D Fish	✓	

14. OPERATIONAL MATTERS ARISING (STRATEGIC THEME – GROWTH)

14.1 Residential

Strategic Plan Reference 2.1
Increase the resident, rate-paying population in the municipality.

Nil.

14.2 Tourism

Strategic Plan Reference 2.2
Increase the number of tourists visiting and spending money in the municipality.

Nil.

14.3 Business

Strategic Plan Reference 2.3
Increase the number and diversity of businesses in the Southern Midlands / Increase employment within the municipality / Increase Council revenue to facilitate business and development activities (social enterprise).

Nil.

14.4 Industry

Strategic Plan Reference 2.4
Retain and enhance the development of the rural sector as a key economic driver in the Southern Midlands / Increase access to irrigation water within the municipality.

14.4.1 Minister for Primary Industries and Water Re: Tas Irrigation (TI) Greater South East Irrigation Scheme (GSEIS) – Service / Design Area (Information Only)

DECISION

Moved by Clr A E Bisdee OAM, seconded by Clr B Campbell

THAT

- a) the information be received and;
- b) Council request an update or progress report from Tasmanian Irrigation in relation to the Greater South East Irrigation Scheme.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

15. OPERATIONAL MATTERS ARISING (STRATEGIC THEME – LANDSCAPES)

15.1 Heritage

Strategic Plan Reference – Page 22

- | | |
|-------|--|
| 3.1.1 | Maintenance and restoration of significant public heritage assets. |
| 3.1.2 | Act as an advocate for heritage and provide support to heritage property owners. |
| 3.1.3 | Investigate document, understand and promote the heritage values of the Southern Midlands. |

14.1.1 Heritage Project Program Report

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr A E Bisdee OAM

THAT:

- a) the Heritage Projects Report be received and the information noted; and
- b) a letter of thanks be drafted for the Mayor's signature thanking Mr. Ryrie for the permanent loan of the Colebrook Probation Station Model.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

5.2 Natural

Strategic Plan Reference – page 23/24

- | | |
|-------|---|
| 3.2.1 | Identify and protect areas that are of high conservation value. |
| 3.2.2 | Encourage the adoption of best practice land care techniques. |

15.2.1 NRM Unit – General Report

DECISION

Moved by Clr D Fish, seconded by Clr D Blackwell

THAT the NRM Unit Report be received and the information noted.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

15.3 Cultural

Strategic Plan Reference 3.3

Ensure that the cultural diversity of the Southern Midlands is maximised.

Nil.

15.4 Regulatory (Development)

Strategic Plan Reference 3.4

A regulatory environment that is supportive of and enables appropriate development.

Nil.

15.5 Regulatory (Public Health)

Strategic Plan Reference 3.5

Monitor and maintain a safe and healthy public environment.

Nil.

15.6 Regulatory (Animals)

15.6.1 Animal Management Report

DECISION

Moved by Clr B Campbell, seconded by Deputy Mayor K Dudgeon

THAT the Animal Management report be received and the information noted.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

15.7 Environmental Sustainability

Strategic Plan Reference 3.7

Implement strategies to address the issue of environmental sustainability in relation to its impact on Councils corporate functions and on the Community.

Nil.

16. OPERATIONAL MATTERS ARISING (STRATEGIC THEME – COMMUNITY)

16.1 Community Health and Wellbeing

Strategic Plan Reference 4.1

Support and improve the independence, health and wellbeing of the Community.

Nil.

16.2 Recreation

Strategic Plan Reference 4.2

Provide a range of recreational activities and services that meet the reasonable needs of the community.

16.2.1 Oatlands Aquatic Centre – Coordinators Report

DECISION

Moved by Clr A E Bisdee OAM, seconded by Clr B Campbell

THAT the information be received and noted.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

16.2.2 Oatlands Recreation Ground – Installation of Electronic Scoreboard / Video Board

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr D Blackwell

THAT:

- a) The report and information be received; and
- b) Council confirm its willingness to fund the project (in full or in part), with Council's total contribution being dependent on the outcome of the AFL Telstra Footy Grant.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

16.3 Access

Strategic Plan Reference 4.3

Continue to explore transport options for the Southern Midlands community / Continue to meet the requirements of the Disability Discrimination Act.

Nil.

16.4 Volunteers

Strategic Plan Reference 4.4

Encourage community members to volunteer.

Nil.

16.5 Families

Strategic Plan Reference 4.5

Ensure that appropriate childcare services as well as other family related services are facilitated within the community / Increase the retention of young people in the municipality / Improve the ability of seniors to stay in their communities.

Nil.

16.6 Education

Strategic Plan Reference 4.6

Increase the educational and employment opportunities available within the Southern Midlands

Nil.

16.7 Capacity & Sustainability

Strategic Plan Reference 4.7

Build, maintain and strengthen the capacity of the community to help itself whilst embracing social inclusion to achieve sustainability.

Nil.

16.8 Safety

Strategic Plan Reference 4.8

Increase the level of safety of the community and those visiting or passing through the municipality.

Nil.

16.9 Consultation & Communication

Strategic Plan Reference 4.8

Improve the effectiveness of consultation & communication with the community.

Nil.

17. OPERATIONAL MATTERS ARISING (STRATEGIC THEME – ORGANISATION)

17.1 Improvement

Strategic Plan Reference 5.1

Improve the level of responsiveness to Community & Developer needs / Improve communication within Council / Improve the accuracy, comprehensiveness and user friendliness of the Council asset management system / Increase the effectiveness, efficiency and use-ability of Council ICT systems / maintain the Business Process Improvement & Continuous Improvement framework

17.1.1 Junior Mayor/Deputy Mayor Policy

DECISION

Moved by Clr A E Bisdee OAM, seconded by Deputy Mayor K Dudgeon

THAT Council:

1. Receive and note the report; and
2. Consider the draft Junior Mayor/Deputy Mayor Program Policy for adoption at February 2026 Council meeting.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

17.1.2 Local Government Electoral Legislation Package

DECISION

Moved by Clr A E Bisdee OAM, seconded by Deputy Mayor K Dudgeon

THAT Council:

- a) Notes the *Local Government Electoral Bill 2025* and the *Local Government (Electoral Reforms) Bill 2025*; and
- b) In response to each of the proposed reforms, Council submit the following to the Office of Local Government (Department of Premier and Cabinet):

Electoral Bill

- Election of Deputy Mayor 'around the table', rather than directly elected. – support the popular vote method
- The current General Manager's roll to be managed by the Tasmanian Electoral Commission (TEC), along with clarification and tightening of who can nominate to vote - supported.

- Provides for flexible voting methods, such as postal, in-person, pre-poll or combinations of these, to be determined by the Electoral Commissioner. The Bill also allows for the Commissioner to provide alternative voting methods for electors that face access challenges - supported.
- Election candidates must complete mandatory pre-election training, which will be provided by LGAT. This does not apply to incumbent councillors – opposed to mandatory training prior to being elected. Mainly based on removing any impediments which may limit the number of candidates nominating.
- Notice of nomination must also be signed by at least 30 electors or 1% of electors in the municipal area (whichever is smaller) – not supported – Council determined that 5 electors was more appropriate to sign a nomination form.
- Electoral expenditure limits aligned with Legislative Council spending rules: \$10,000 for most councils. \$16,000 for Hobart, Clarence, Glenorchy, Kingborough and Launceston – no formal position.

Local Government (Electoral Reforms) Bill 2025

- New section on requiring Personal Interest Returns (PIR) from councillors, which are to be published on the council's website. An example PIR return form is including in the [discussion paper/consultation report](#), Appendix B. – not supported. Very onerous and considered breach of privacy (in some parts of the PIR).
- Formalising caretaker provisions, around council decision-making during election periods. A number of councils, including the Southern Midlands Council, already have policies in place on this matter.
- Thresholds for elector polls or public meetings has been raised to 20 per cent (up from the lesser of 5 per cent or 1,000 electors) - supported.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

17.2 Sustainability

Strategic Plan Reference 5.2

Retain corporate and operational knowledge within Council / Provide a safe and healthy working environment / Ensure that staff and elected members have the training and skills they need to undertake their roles / Increase the cost effectiveness of Council operations through resource sharing with other organisations / Continue to manage and improve the level of statutory compliance of Council operations / Ensure that suitably qualified and sufficient staff are available to meet the Communities need / Work co-operatively with State and Regional organisations / Minimise Councils exposure to risk / Ensure that exceptional customer service continues to be a hallmark of Southern Midlands Council

17.2.1 Tabling of Documents

Nil.

17.2.2 Elected Member Statements

An opportunity is provided for elected members to brief fellow Councillors on issues not requiring a decision.

Deputy Mayor K Dudgeon – commented on the success of the Australia Day Awards event held at the Broadmarsh Community Hall. It was extremely well attended. Congratulated the Hall Committee and Jemma Croswell (Council Executive Assistant) for organising the event.

Deputy Mayor K Dudgeon – recognition of Mr Athol Bennett being awarded an OAM for services to the community of the Southern Midlands. Congratulatory letter to be sent by Mayor Edwin Batt and Mr Bennett be invited to attend a Council Meeting (or similar) to formally acknowledge his award.

Deputy Mayor K Dudgeon – informed the meeting that a Seniors Bus Trip to the Lavender Farm is being held on 29th January 2026.

Clr B Campbell – Woodsdale Football Club Family Day - informed the meeting that he attended the Family Day event, and participated in the Ute Competition. He indicated that the Football Club has signed a new Assistant Coach and has attracted additional players to the Club.

Clr D Blackwell - informed the meeting that she had spoken to a representative of the Tunnack Fire Brigade at the Australia Day Event. It was indicated that Councillors needed to respond to the invitation sent for their Christmas barbecue event.

Deputy Mayor K Dudgeon – Tunbridge Community Meeting – informed all Councillors that the Community Meeting to discuss the future of the Tunbridge Town Hall has been set for Thursday 5th February 226, commencing at 5.30 p.m. at the Town Hall.

17.2.3 Local Government Shared Services – Quarterly Update – Information Only

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr A E Bisdee OAM

THAT the information be received.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

17.2.4SMC External Grant Projects - Quarterly Update

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr D Blackwell

THAT the information be received.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

17.3 Finances

DECISION

Moved by Clr A E Bisdee OAM, seconded by Clr B Campbell

THAT the Financial Report be received and the information noted.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

17.3.2 Woodsdale Football Club Inc. – 2026 Family Fun Day - Request for Donation

DECISION

Moved by Clr B Campbell, seconded by Deputy Mayor K Dudgeon

THAT Council donate \$250 to the Woodsdale Football Club to assist with the conduct of the Club's 2026 Family Fun Day.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

18. MUNICIPAL SEAL

Nil.

19. CONSIDERATION OF SUPPLEMENTARY ITEMS TO THE AGENDA

Nil.

RECOMMENDATION

THAT in accordance with Regulation 17 (1) of the *Local Government (Meeting Procedures) Regulations 2025*, the following items are to be dealt with in Closed Session.

Matter	<i>Local Government (Meeting Procedures) Regulations 2025 Reference</i>
<i>Closed Council Minutes - Confirmation</i>	17(2)
<i>Applications for Leave of Absence</i>	17(2)(h)(i)
<i>Kempton Community Health Centre</i>	17(2)(d)(i)
<i>Flour Mill Park Redevelopment Project</i>	17(2)(g)

DECISION

Moved by Clr B Campbell, seconded by Clr A E Bisdee OAM

THAT in accordance with Regulation 17 (1) of the *Local Government (Meeting Procedures) Regulations 2025*, the following items are to be dealt with in Closed Session.

Matter	<i>Local Government (Meeting Procedures) Regulations 2015 Reference</i>
<i>Closed Council Minutes - Confirmation</i>	17(2)
<i>Applications for Leave of Absence</i>	17(2)(h)(i)
<i>Kempton Community Health Centre</i>	17(2)(d)(i)
<i>Flour Mill Park Redevelopment Project</i>	17(2)(g)

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

RECOMMENDATION

THAT in accordance with Regulation 17(2) of the *Local Government (Meeting Procedures) Regulations 2025*, Council move into Closed Session and the meeting be closed to members of the public.

DECISION

Moved by Deputy Mayor K Dudgeon, seconded by Clr D Fish

THAT in accordance with Regulation 17(2) of the *Local Government (Meeting Procedures) Regulations 2025*, Council move into Closed Session and the meeting be closed to members of the public.

CARRIED

DECISION (MUST BE BY ABSOLUTE MAJORITY)		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

CLOSED COUNCIL MINUTES

20. BUSINESS IN “CLOSED SESSION”

20.1 Closed Council Minutes - Confirmation

In accordance with the Local Government (Meeting Procedures) Regulations 2025, the details of the decision in respect to this item are to be kept confidential and are not to be communicated, reproduced or published unless authorised by Council.

Item considered in Closed Session in accordance with Regulation 17(2) of the Local Government (Meeting Procedures) Regulations 2025.

20.2 Applications for Leave of Absence

In accordance with the Local Government (Meeting Procedures) Regulations 2025, the details of the decision in respect to this item are to be kept confidential and are not to be communicated, reproduced or published unless authorised by Council.

Item considered in Closed Session in accordance with Regulation 17(2)(h)(i) of the Local Government (Meeting Procedures) Regulations 2025.

20.3 Kempton Community Health Centre Conversion to Child Care Centre (including ‘Before & After School Care Program’ Facility)

In accordance with the Local Government (Meeting Procedures) Regulations 2025, the details of the decision in respect to this item are to be kept confidential and are not to be communicated, reproduced or published unless authorised by Council.

Item considered in Closed Session in accordance with Regulation 17(2)(d)(i) of the Local Government (Meeting Procedures) Regulations 2025.

20.4 Flour Mill Park - Redevelopment Project (Including the Flour Mill Park Building)

In accordance with the Local Government (Meeting Procedures) Regulations 2025, the details of the decision in respect to this item are to be kept confidential and are not to be communicated, reproduced or published unless authorised by Council.

Item considered in Closed Session in accordance with Regulation 17(2)(g) of the Local Government (Meeting Procedures) Regulations 2025.

RECOMMENDATION

THAT Council move out of “Closed Session”.

DECISION

Moved by Clr A E Bisdee OAM, seconded by Clr B Campbell

THAT Council move out of “Closed Session”.

CARRIED

DECISION		
Councillor	Vote FOR	Vote AGAINST
Mayor E Batt	✓	
Deputy Mayor K Dudgeon	✓	
Clr A E Bisdee OAM	✓	
Clr D Blackwell	✓	
Clr B Campbell	✓	
Clr D Fish	✓	

OPEN COUNCIL MINUTES

21. CLOSURE

The meeting closed at 12.55 p.m.

**LAKE DULVERTON & CALLINGTON PARK MANAGEMENT COMMITTEE
MINUTES**

Monday 16th February 2026

**Council Chambers
Oatlands**

1.30 p.m.

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LAKE DULVERTON & CALLINGTON PARK MANAGEMENT COMMITTEE

MINUTES

Monday 16th February 2026

1.30 p.m.
Council Chambers
Oatlands

MEMBERS:

Chairman: Councillor Don Fish (Deputy Mayor K Dudgeon – proxy)

Parks & Wildlife Rep: Oliver Lawler. Proxy rep:

Resident Representatives: Mrs Maria Weeding, Mr Athol Bennett, Dr Robert Simpson, Mrs Karen Dudgeon, Ms Helen Geard, Mrs Jenni Muxlow, Mr Grant Wilson

The meeting opened at 1.30 p.m.

1. ATTENDANCE

Councillor Don Fish, Athol Bennett, Helen Geard, Maria Weeding, Robert Simpson, Jenni Muxlow, Grant Wilson, Deputy Mayor Karen Dudgeon, Ben Clark- via Teams meeting video link (Parks & Wildlife - Acting Parks and Reserves Manager – South Central).

2. APOLOGIES

Oliver Lawler (new Parks & Wildlife rep – replacing Dominic Neyland as of January 2026).

The committee members introduced themselves to Ben Clark from Parks & Wildlife who attended the meeting in place of Oliver Lawler.

3. CONFIRMATION OF MINUTES

The Committee to confirm the 1st December 2025 minutes.

RECOMMENDATION

That the Committee confirm the minutes of the Lake Dulverton & Callington Park Management Committee meeting held on 1st December 2026.

SUB COMMITTEE RECOMMENDATION TO COUNCIL:

MOVED Dr Robert Simpson

SECONDED Mr Athol Bennett

THAT the Committee confirm the minutes of the Lake Dulverton & Callington Park Management Committee meeting, held on 1st December 2025.

CARRIED

4. BUSINESS ARISING FROM PREVIOUS MEETING

4.1 FORMAL RECOGNITION OF DOGS ON THE LAKE FORESHORE

Following on from Council receiving formal written approval by Parks authorities, to allow dogs on defined sections of the Lake foreshore (Conservation Area), the signage is being progressed. There will also need to be minor changes to the Dog Management Policy to reflect the areas that dogs are allowed. The community were advised via the ratepayer newsletter (December 2025 edition) as to the changes, noting that the changes are not going to come into effect until the new signage is installed.

It was explained to the Committee that the Councils change to the Dog Management policy was listed as an item on the February Council agenda and signage was being organised.

RECOMMENDATION

That the information be noted and works continue regarding dog areas on the Lake foreshore.

SUB COMMITTEE RECOMMENDATIONS TO COUNCIL:

RESOLVED

THAT the Committee note the information.

CARRIED

4.2 1980 LAKE DULVERTON MANAGEMENT PLAN & MANAGEMENT STATEMENT PROPOSAL

In February 2024 the Committee was informed that the 1980 Lake Dulverton Management Plan must be the over-riding guiding document to manage the Lake. Since that time, the Committee have been working to have the Plan either fully updated or rescinded and replaced with another plan. There have been a lot of long and detailed discussions on this at several meetings. At the last meeting of the committee it was suggested by Parks that there was two options for the Management Statement worthy of consideration by the committee. They are:

1. Have a Management Statement that is quite detailed, which deals with the elements of the lake that Southern Midlands Council and the Committee actively manage. It was suggested that items like history and geology not be included in the document. This would essentially be a management and an operational component combined document. Additionally, it was suggested by Parks that unless there is active management on that aspect of the lake, such as Geology of the Lake, then do not include that as an item in the Management Statement.
2. Have a Management Statement that is extremely brief. Then have a second document that covers off on the actual operational side of the management of the Lake. This would be an Operational Plan that was also signed off by Parks & Wildlife. The plan would cover off on the relevant pieces of legislation that apply to the lake, covering the proposed operational items. The Operational Plan could be formulated after the brief Management Statement has been signed off and approved by Parks.

In either of option 1 or 2, the history of the lake was not included, such as the management of the lake, the early settler history, the water pipeline history etc. The details of bird recordings would not be included, as that is not an operational matter. There was discussion on the paths – walking paths or paths for bicycles as well? It was agreed that the use of bicycles would need to be included as a permitted use. This needs to be noted, as under standard Parks rules for a Conservation Area, push bikes are prohibited.

There was much discussion, with some of the committee in favour of the first option and others in favour of the second option. As for the non inclusion of history, there was no decision.

Parks were asked to provide an example of a very brief Management Statement (that may suit option 2), noting that the Committee had received the Meehan Ranges document as an example, and to date had followed that as the format (option 1 document).

It was suggested that the current draft document compiled to date by the Committee be updated to reflect points of discussion from the meeting, including deleting overly detailed information. The then updated draft would be sent to Parks for further appraisal. There was no firm decision on how detailed the draft Management Statement should be.

Since the 1st Dec 2025 committee meeting, Council was been advised that our Park representative, Dominic, had moved to another role in Parks, so would no longer be the Parks person on this committee. Oliver Lawler is now going to be the contact / representative for Parks as of January 2026.

Prior to the February Committee meeting Maria spoke with Oliver, and suggested that it would be good to have a meeting with some of the Parks officers to come up with an agreed format for the replacement plan for the 1980 plan. This is yet to be arranged, as some key Parks staff have been away / on leave.

A fundamental agreement needs to be reached in regard to the format, before Council officers spend more time on this issue.

RECOMMENDATION

That the Committee, Parks & Wildlife and Southern Midlands Council officers continue to work on the development of a Management Statement (draft) for Lake Dulverton, with the format / content headings being agreed between Parks and Southern Midlands as the next step.

SUB COMMITTEE RECOMMENDATION TO COUNCIL:

RESOLVED

THAT 1. the Committee note the information.

2. the Committee, Parks & Wildlife and Southern Midlands Council officers continue to work on the development of a Management Statement (draft) for Lake Dulverton, with the format / content headings being agreed between Parks and Southern Midlands as the next step.

CARRIED

4.3 MYRIOPHYLLUM - IN 51 HA AREA - RECREATION & CONSERVATION ZONES

The Committee are well aware of the native aquatic plant, Myriophyllum, that has grown extensively over the last 10 years and now expanded to cover an estimated 70% in area of the 51 Ha area of the Lake. Local citizens that have regularly observed the Lake for over 50 years, say that this plant has not been observed to dominate the Lake aquatic vegetation in such a manner as it is now. The prolific growth of the Myriophyllum is effectively dominating and the previously abundant water ribbon plant that typically covered the lake. The water ribbon plant (Cycnogeton procerum) is observed to be in decline in the 51 Ha area (but not in decline in the other two areas of the Lake – Fountain Zone and the Natural Zone).

The Myriophyllum plant was confirmed (in late 2024) by the Tasmanian Herbarium staff and is specifically Myriophyllum salsaugineum, which is a widespread common native plant typically found in lowland lakes.

At an earlier meeting of the Committee, it was suggested that a consultant be approached to conduct a Lake baseline study to capture information about health, nutrient levels, water temperature, organic matter, oxygen levels etc. The results from the baseline study will inform next steps and assist with applying for funding grants to conduct future research and public consultation to develop appropriate management options.

The Committee committed \$5000 of their funds from donations to have a Consultant undertake a Lake baseline health study and give any identified recommendations to improve the Lake.

Luke Findlay from Elgin and Associates were given the task. On Tuesday 27th January 2026 Luke installed water monitoring equipment in the lake. He also took some water samples from four locations for water quality and nutrient testing. More testing will occur over time.

An email giving a basic summary of the first set of observations was sent to Council on 2nd February 2026 (see attached). Graphs from the on line web site showing the real time monitoring results was also printed out for the committee to note as an example of some of the data being collected. It was noted that the dissolved oxygen reader may not be working fully – given the oxygen graph results. Maria is to contact Luke and asked for this to be checked (see graphs attached).

The Committee will eventually receive a summary report on the water quality that may include recommendations. The Committee will then determine what actions (if any) are feasible and within budget capacity to make a positive difference to the Lake.

RECOMMENDATION:

That the information be noted.

SUB COMMITTEE RECOMMENDATIONS TO COUNCIL:

RESOLVED

THAT the Committee note the information.

CARRIED

5.0 TREASURER’S REPORT

A statement detailing the final Receipts and Expenditure for the 2025/2026 financial year to date was tabled at the meeting.

RECOMMENDATION

The statement detailing the Receipts and Expenditure for the 2025/2026 financial year to date be received and noted.

SUB COMMITTEE RECOMMENDATIONS TO COUNCIL:

MOVED Mr Grant Wilson
SECONDED Dr Robert Simpson

THAT the statement detailing the Receipts and Expenditure for the 2025/2026 financial year to date be received and noted.

CARRIED

SOUTHERN MIDLANDS COUNCIL
LAKE DULVERTON MANAGEMENT COMMITTEE
2025/26 STATEMENT OF RECEIPTS AND PAYMENTS
For the period 27th November 2025 to 16th February 2026

	RECEIPTS \$		PAYMENTS \$	COMMENTS
Opening Balance 01.07.25 Commonwealth Bank Account	51,471.24			
Capital				
Nil				
Operating				
Callington Park - Community Notice Board	3,000.00		0.00	
Lake Dulverton - Committee Budget / Dog Signage	2,000.00		76.36	Postage
Tas Irrigation - Water Operational Costs	26,875.00	Operational Charge	6,899.35	
		Asset Renewal Levy	2,580.00	
		Water Usage	13,378.00	
Interest On Committee Bank Account	120.12	Bank Charges	0.00	
Roadside Stopover Cash - Deposited to Committee Account	2,927.80			
Roadside Stopover Deposited to SMC Account	591.59			
		Total Expense to date	\$22,933.71	
		Funds on hand	64,052.04	
	\$ 86,985.75		\$ 86,985.75	

Funds on hand are represented by:

Comm. Bank Account No.06 7004 28003859 - 31.12.25	54,519.16
Special Projects - Unexpended Budget	9,532.88
	\$ 64,052.04

6.0 OTHER MATTERS

6.1 Budget for 26/27 year.

The committee considered items to be included in the 26/27 FY budget. The request is to go to Council, to form part of their budget deliberations. Proposals need to be submitted to Council's finance department in the first instance, by the end of February.

The committee made a number of suggestions including a new seat where the old wooden seat was removed from the shore edge at Mahers Point (concrete base still there). Suggestion of an umbrella for shade during the summer months to be placed at the table and seat combination located in Callington Park (installed late 2025). Also – asking for a further \$1000 for dog signage (to add to the existing \$2000 budget).

There was discussion about the Dulverton walkway - at some locations there is a need to top up the gravel due to the incursion of weeds onto the pathway. This is a maintenance issue and has been mentioned by some Councillors to the Works Dept. Maria was asked to follow up.

Query about the bridge to Marys Island item listed on the budget. The idea of a bridge is itemised in the Lake Dulverton Action Plan(s). Considerable funds and planning approval is needed to enable this to come to fruition; however, the committee still see this as a longer-term addition to the visitor and general community experience and enjoyment of the Lake. Grant mentioned that the pedestrian access bridge used during the construction of the new bridge at Bridgewater was potentially available for use by the Committee. Grant is still pursuing this – but his main contact person is no longer available (since the Bridgewater Bridge was completed).

See table attached re the budget submission.

MOVED Mrs Maria Weeding

SECONDED Mrs Jenni Muxlow

THAT the committee's proposed be submitted to Council for consideration for the 2026 /2027 committee budget allocation.

CARRIED

	Item/Activity	Lake Dulverton & Callington Park Management Committee Proposed 26/27 Works Schedule and Budget Detail	Funds requested from Council - 26/27
	Callington Park / or Lake Dulverton	Table and seat - Strabe city table and setting - steel frame with Eco Slat battens \$4500 + concrete pad base \$1500. Seat - replace seat at Mahers Point - Strabe City Seat with Arm rest - Ecoslat Battens \$2000. No base required. Freight \$2000 Note - Committee purchased one Strabe setting (table and seat) and another seat in late 2025 with funds from donations recieved.	10000.00
	Callington Park	Umbrella (Commercial grade) for table at end of Callington Park - summer months. \$6745 + freight + install. Wind up handle. Placed at end of the table - small concrete pad required.	8000.00
	Lake Dulverton foreshore & corridor	Renew faded interpretation signage - \$8000.00. General expenditure - \$1000.	9000.00
	Lake Dulverton foreshore & corridor	Compliance signage - dog area signage - additional funds to complete task	1000.00
	Lake Dulverton	Health of the Lake - options for management of the water quality. More detailed work by a water ecologist, including a report and other expenses. \$20,000. Note: in 25/26 year - the committee are currently paying \$5000 of donation funds to have an initial appraisal done -which is being undertaken as at Feb 2026.	20000.00
	Midlands Water Scheme	Operating budget - purchase of water for Lake Dulverton	33436.00
	Marys Island	Investigate fesibility of some form of access from end of Mahers Point across to Marys Island. Planning, public consultation and preliminary engineering.	0.00
			81436.00

6.2 Power Upgrade to the Toilet Block on the Foreshore

Maria informed the committee that TasWater was planning to upgrade the switchboard associated with the pump station at the foreshore toilet block. This change has been discussed with the Committee back in mid 2024, (when the committee requested that the cladding on the new switchboard be clad in Colourbond Paperbark – not the standard grey colour).

TasWater discovered that the power for the foreshore BBQ hut was connected to their pump station power metre, and requested that this be changed to connect to the Southern Midlands power metre, which is associated with the toilet block (lighting and hand-dryers).

Unfortunately, for Southern Midlands this additional power demand has tipped the current wiring capacity into needing to be upgraded to a higher capacity. This involves renewing a relatively short section of some existing underground cables. Both Taswater and Southern Midlands will work simultaneously at the site.

Works are expected to be undertaken before mid-2026. An Aboriginal Heritage Tasmania assessment has been undertaken by both parties and both have now submitted a Reserve Activity Assessment to Parks & Wildlife for works approval. It was suggested that a combined assessment be undertaken, but TasWater were keen to be independent to Southern Midlands (even though the work by Southern Midlands is very minor in nature).

RESOLVED

THAT the Committee note the information.

CARRIED

6.3 Congratulations to Committee Member Athol Bennett on his OAM award

The committee expressed their congratulations to Athol Bennett on his well-deserved Medal of the Order of Australia (OAM) – recently announced on Australia Day - January 2026.

7.0 NEXT MEETING

The next meeting of the committee is on Monday 13th April 2026 – 1.30 p.m. Council Chambers, Oatlands

The meeting finished at 2.53 p.m.

* * * * *

Maria Weeding

From: Luke Finley
Sent: Monday, 2 February 2026 5:12 PM
To: Maria Weeding
Cc: Annette
Subject: Re: on site visit to set up devices

Hi Maria,

Just sent through access to the online telemetry and a snip of some data from the telemetry device. You should have an email from Eagle.io, which may be in your spam. Once you set up you should be able to see a dashboard like the one below.

Some key observations thus far:

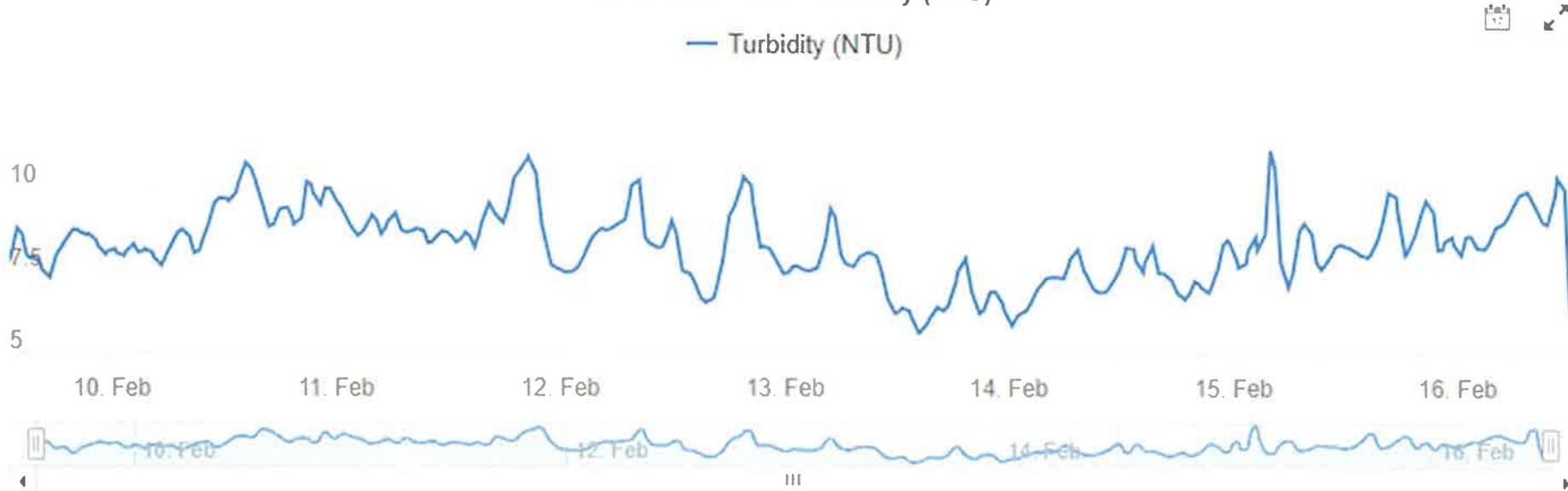
1. Turbidity is around 4-6NTU which is clear and matched the spot checks done last week.
2. Oxygen is very low, to the point where fish would not survive. This may be influenced by the casing, but it was low on the spot check at the logger location as well.
3. pH near neutral at 7.2.
4. Temperature 12-15 degrees, but has been pretty stable during day/night, possibly due to the nearby shade. Other locations were in the 19 - 25 degrees during spot checks.
5. Electrical Conductivity 1800-1900 mS/cm which is slightly brackish and consistent with spot checks last week

Let me know if you have any queries.

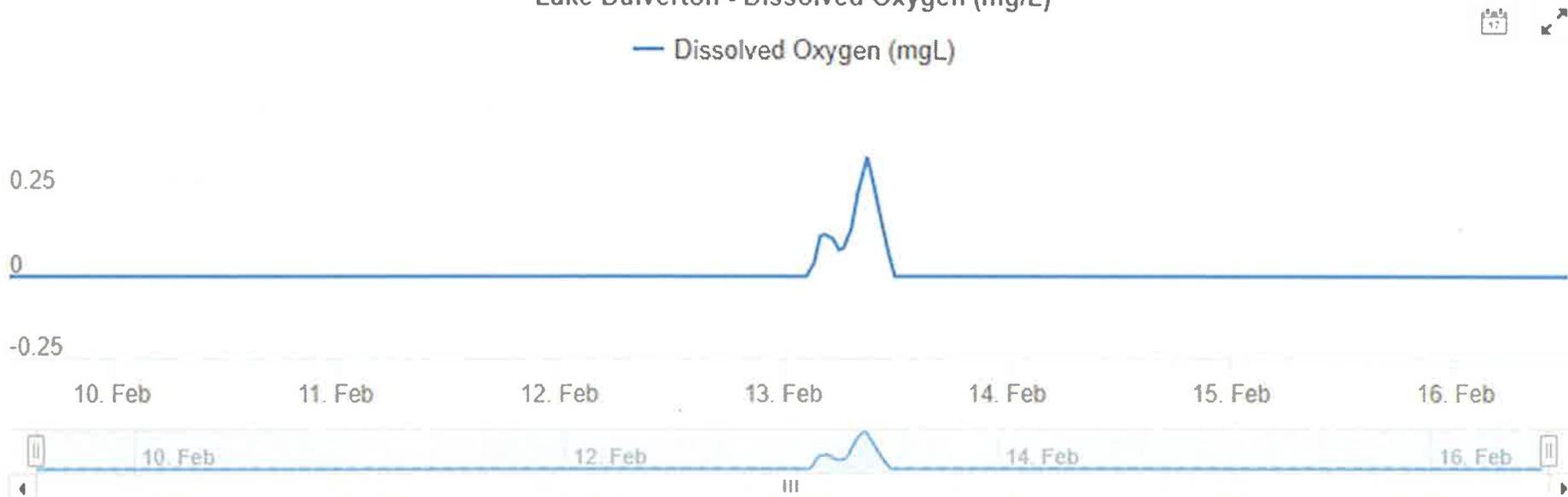
Kind regards,

Luke

Lake Dulverton - Turbidity (NTU)



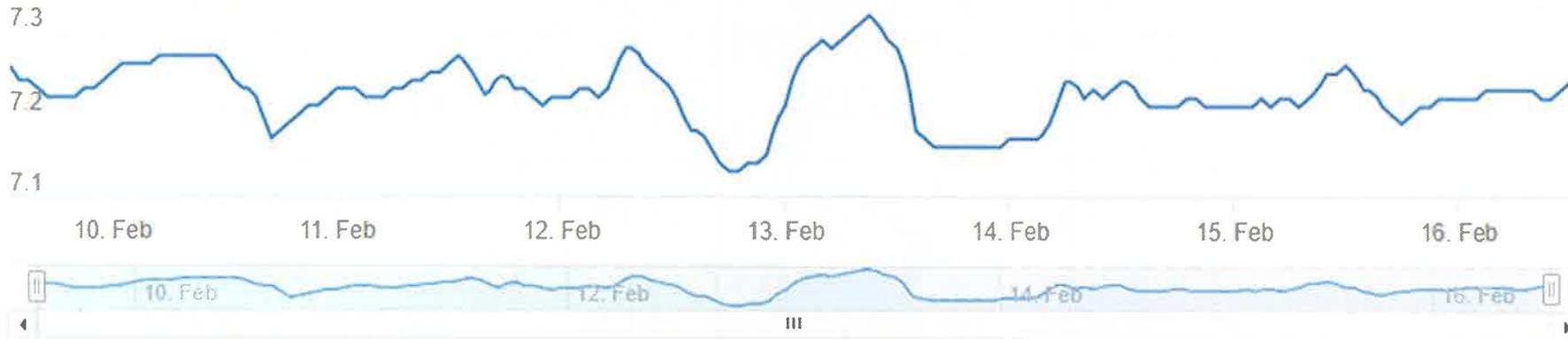
Lake Dulverton - Dissolved Oxygen (mg/L)



*Example: Results from real time monitoring device.
Note: Oxygen monitor - Not working??*

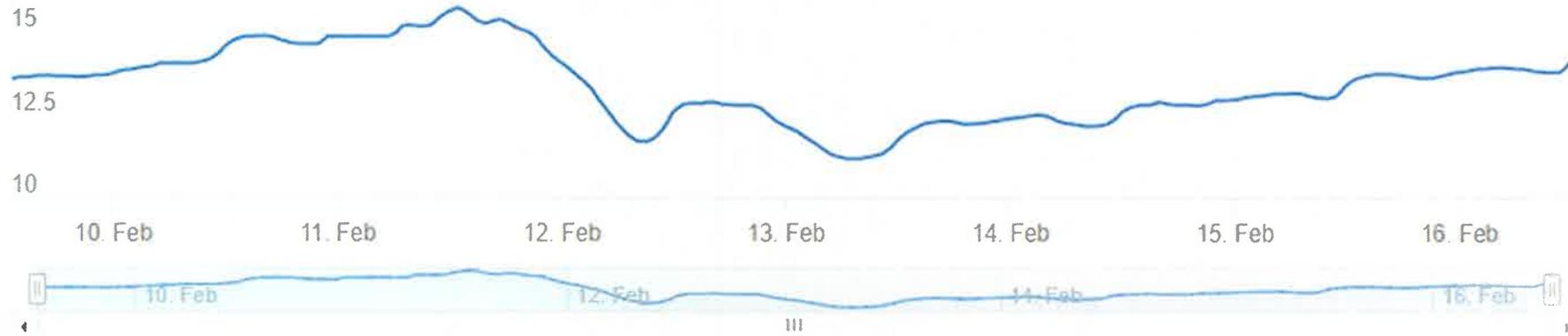
Lake Dulverton - pH

— pH



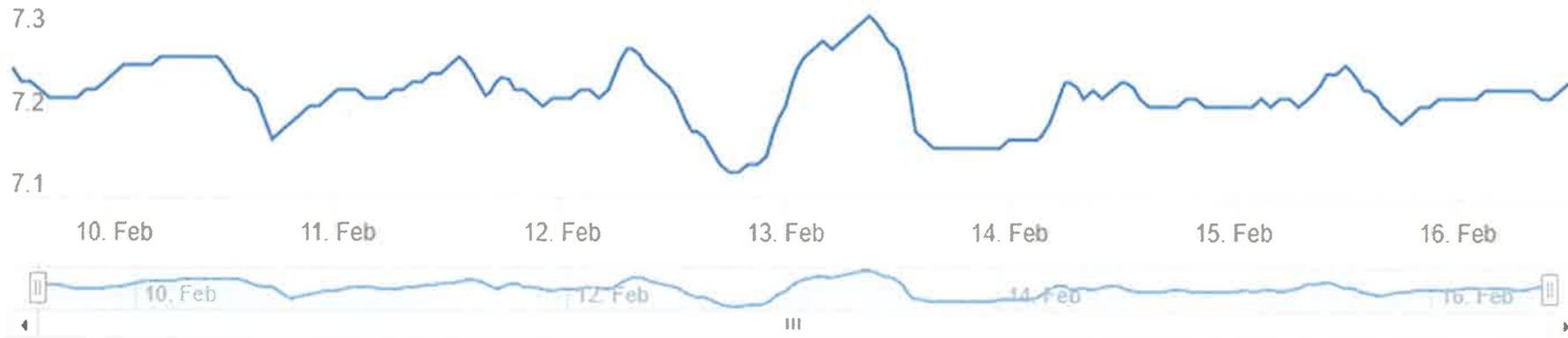
Lake Dulverton - Water Temperature (oC)

— Water Temperature (oC)



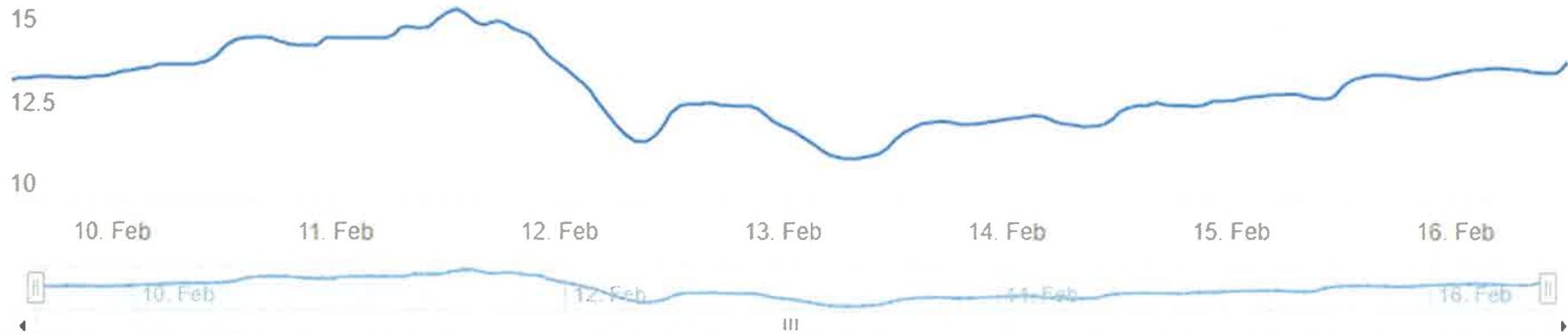
Lake Dulverton - pH

— pH

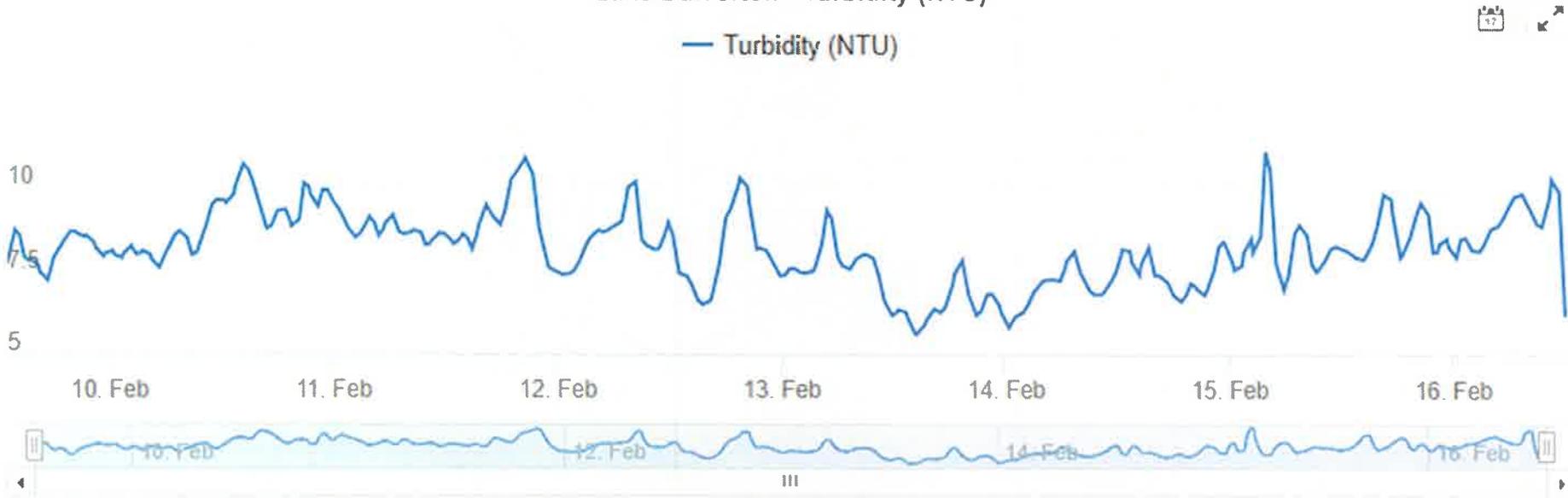


Lake Dulverton - Water Temperature (oC)

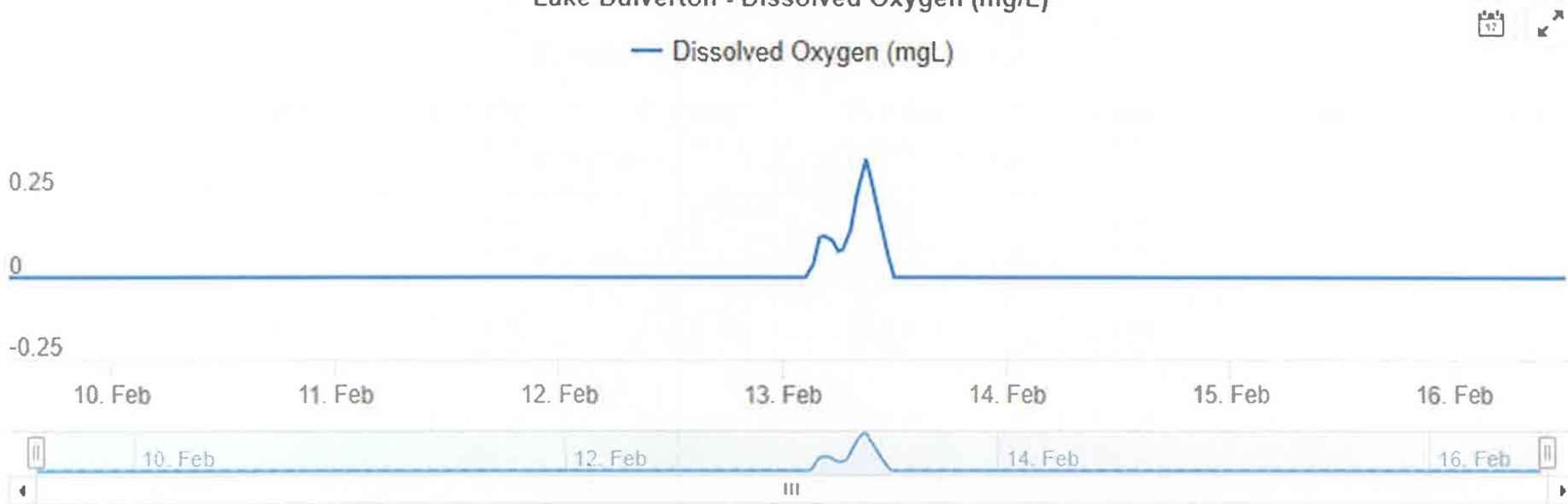
— Water Temperature (oC)



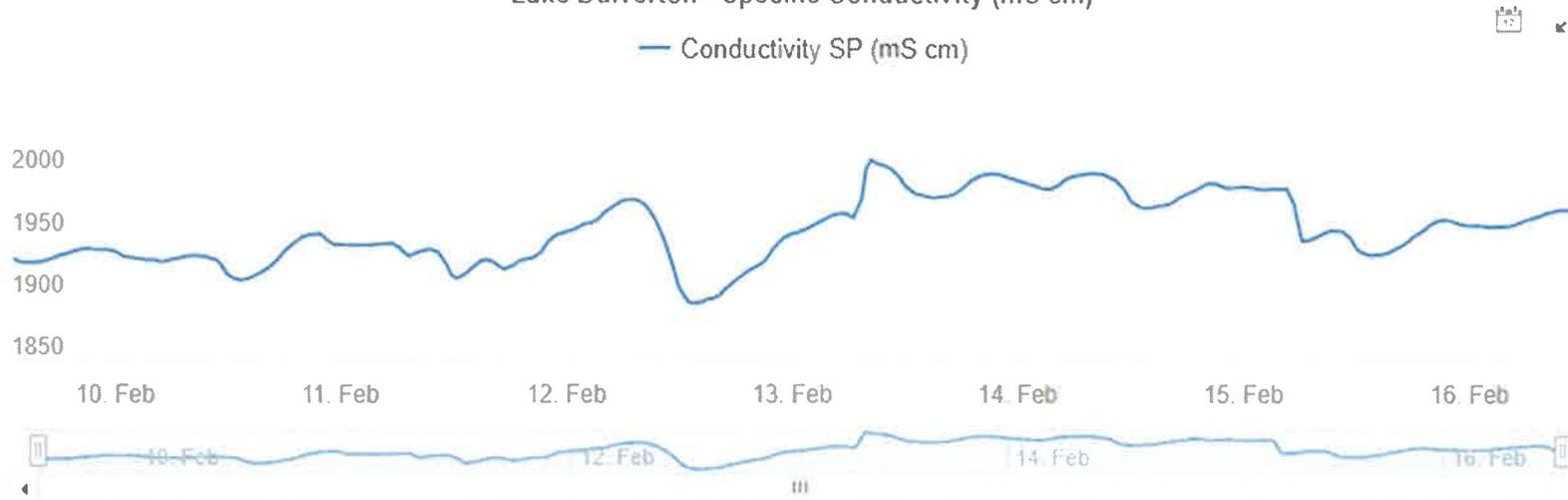
Lake Dulverton - Turbidity (NTU)



Lake Dulverton - Dissolved Oxygen (mg/L)



Lake Dulverton - Specific Conductivity (mS cm)



Historic table					
Time	Turbidity (NTU) (NTU)	pH (pH)	Oxygen (mg/L) (mg/l)	Conductivity SP (mS cm) (uS/cm)	Temperature (oC) (C)
2026-02-16 12:00:11	6.18	7.24	0	1955.9	14.14
2026-02-16 11:00:11	11.83	7.23	0	1969.4	13.81
2026-02-16 10:00:28	8.85	7.22	0	1970.4	13.78
2026-02-16 09:00:11	8.97	7.22	0	1969	13.81
2026-02-16 08:00:28	9.56	7.23	0	1967.1	13.84
2026-02-16 07:00:11	10.07	7.23	0	1963.7	13.9
2026-02-16 06:00:11	9.54	7.23	0	1961.9	13.91
2026-02-16 05:00:11	8.96	7.23	0	1958.6	13.94
2026-02-16 04:00:11	8.81	7.23	0	1956.4	13.95
2026-02-16 03:00:11	8.16	7.23	0	1956.9	13.91
2026-02-16 02:00:28	8.2	7.22	0	1955.7	13.9
2026-02-16 01:00:11	8.72	7.22	0	1957.7	13.84
2026-02-16 00:00:07	7.61	7.22	0	1956.9	13.8
2026-02-15 23:00:11	8.99	7.22	0	1958.4	13.75
2026-02-15 22:00:11	7.75	7.22	0	1961.3	13.67
2026-02-15 21:00:11	10.03	7.21	0	1962.5	13.61
2026-02-15 20:00:11	9.22	7.21	0	1959.2	13.61

TasWaste South



Quarterly Report

Reporting Period: Q2 – October to December 2025

Date of Issue: 30 January 2026

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Tasmanian
**Waste & Resource
Recovery Board**

Reporting requirements

This Quarterly Board Report is a statutory requirement for TasWaste South as a Joint Authority established by the 12 southern Tasmanian councils under the *Local Government Act 1993*. The relevant section of the Act is included below.

36B. Quarterly reports of authorities

- A single authority or joint authority must submit to the single authority council or participating councils a report as soon as practicable after the end of March, June, September and December in each year.
- The quarterly report of the single authority or joint authority is to include –
- a statement of its general performance; and
- a statement of its financial performance.

Statement of general performance

The second quarter of 2025/2026 saw the progression of key projects outlined the annual plan. These critical, foundational projects that will guide the future work and resourcing of TasWaste South.

Preparations for the 2026-2027 financial year have also begun with the Board workshopping the budget and annual plan at their December meeting.

Governance and strategy

Annual Report

The Annual Report for TasWaste South for 2024-2025 was finalised and released during this quarter. The report highlights the significant progress that has been made over the financial year. Some of the key achievements include:

- Conducted bin audits across the region
- Secured increased resourcing and funding for the Rethink Waste program
- Coordinated Household Hazardous Waste collections
- Commenced development of the Infrastructure Plan

It is available on TasWaste South's [website](#).

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Forum and AGM

The Local Government Forum and AGM were held on 27 November at the Kingborough Community Hub. Several presentations were made.

- The CEO of TasWaste South presented the Annual Report for 2024-2025 and provided an update on current activities of TasWaste South.
- An update on the progress of the regional infrastructure plan was provided by MRA Consulting.
- The Rethink Waste Engagement Officer for the Southern region, Kimberley Eaton provided an overview of the activities being undertaken since she commenced in that role in July 2025.
- Kingborough Council presented on their recycling truck art competition.

Projects

While, as previously outlined, this quarter has focused on the progression of already commenced foundational projects, TasWaste South has also initiated a number of new projects.

- New projects

Waste Transfer Station Operational Concept Design

Resource Hub has been commissioned to complete conceptual diagrams to inform future upgrades and/or new build Waste Transfer Stations (WTS) with a focus on layout, design and traffic flows to optimise customer satisfaction and resource recovery activities.

A key consideration is that diagrams will be used as a starting point for any current WTS facility upgrades or future new-build WTS to bring uniform design principles and considerations to inform detailed engineering design. This will allow a similar customer and operational experience at all facilities in the region.

The work will also provide quick reference guides for the public to enhance customer knowledge and better prepare customers prior to attending site. This may include how to best pack trailers/utes to most easily drop-off and sort materials and how to navigate sites to improve onsite satisfaction whilst improving outcomes for staff, site safety and resource recovery.

Role of TasWaste South in provision of Grants

Consistent with an action in TasWaste South's Strategic Plan, the role of TasWaste South in the provision of grants is currently being explored. This includes consideration of approaches

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in other regions and jurisdictions to ensure that the value of the funding can be maximised and duplication and competition can be avoided.

Soft Plastics Collection

Initial conversations have commenced with Soft Plastics Stewardship Australia (SPSA) to explore a regional approach to soft plastics collection. SPSA is a stewardship scheme with voluntary membership that is attempting to deal with the soft plastics issue. They have recently received ACCC approval to handle bulked-up soft plastics collection and provide them to processors. It is understood that the SPSA meet the costs of having them transported and processed.

The SPSA is seeking to work with councils in Tasmania to facilitate collection of soft plastics via kerbside collection in recycling bins which are then captured at the MRF and provided to a processor.

Initial conversations have occurred with feedback on potential issues provided. Meetings with council waste and resource recovery officers will occur in the first quarter of 2026 to develop a potential model for the region.

The initiation of this project reflects the demand and success of soft plastics collection trials from some councils within the region.

- Ongoing projects

Regional Infrastructure Plan

This project is progressing well with the data gathering portion of the project now complete and compiled in a report. This phase of the project included meetings between the consultant and numerous stakeholders as well as a workshop at the August Waste Officer Network Meeting. The modelling work is now underway. The project remains on schedule and expected to be completed in the first quarter of 2026.

Regional Organics Plan

This work was awarded to Talis Consultants. The project inception meeting has been completed and the project is now in the data collection phase. The project team met with the recently formed Southern Tasmanian Organics Working Group facilitated by NRE on October 22 and were well received with an additional update to be provided to the group at the next meeting.

Household Hazardous Waste Drop-off Days

A further four days of household hazardous waste (HHW) drop off days have been conducted over five locations including Orford, Oatlands, Huonville, Sorell and Glenorchy. These days

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were the first of a new statewide approach to HHW. Collateral has been completed on a statewide level with all three regional bodies and WRRB logos present on high level advertising and social media tiles. (Regionally focused advertising is branded with only the individual region and WRRB logos). All regions are using the same survey at each event allowing for the development of a statewide dataset for the first time.

TasWaste South kicked off the statewide campaign beginning with Orford and Oatlands on September 20 and Huonville on September 21. A last-minute date clash required that the south pivot and deliver early which resulted in a very short advertising lead-in to the first weekend. Despite this and wild spring weather participation numbers remained strong.

Advertising was via social media, newsprint and posters at key local venues.

In total more than 3,000 kilograms of hazardous waste was collected from a total of 108 participants across the five locations.

Class 2 Gasses (e.g. aerosols)	1 kg
Class 3 Flammable Liquids (e.g. fuel)	1,386 kg
Glass 5.1 Oxidisers (e.g. pool chemicals)	62 kg
Class 6 Toxics (e.g. herbicides)	1,201 kg
Class 8 Corrosives (e.g. acid and alkalis)	516 kg
Class 9 Environmentally Hazardous	33 kg
Vapes	19 units
Total	3,199 kgs

The success of the events demonstrates the ability of the south to be agile and highlights how involved the southern councils are. Councils should be applauded for how responsive they were in finding suitable venues and how active they were in local advertising of the events which contributed to their success.

The northern region completed their events in October, and the statewide campaign wrapped up in the northwest through November.

High Priority Infrastructure Grant

Construction completion of the hazardous waste storage facility is imminent with only minor works such as installation of lighting outstanding. Focus was somewhat diverted due to a fire at the Baretta facility in July resulting in smoke damage to the shed requiring significant rectification works which remain ongoing.

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Once final documentation is completed an opening event will be delivered.

Circular Economy Procurement

Representatives from LGAT presented some work on this topic as a follow-on from the work conducted by TasWaste South at the August Waste Officer Network meeting. Participation from the group was good with collective agreement that it is an important area to focus on. TasWaste South will also continue engagement with LGAT who have committed to include circular economy focus in the procurement training they are providing early in the New Year.

Lutana MRF Contract Review

This work has been awarded to Talis Consultants following a competitive tender process and stakeholder engagement is underway.

UTAS Sustainability Student Placement

This work has involved exploring the use of an app for waste information on a regional basis. The results will be considered and subject to consultation with council officers before being considered by the Board of TasWaste South.

Engagement and education

ReThink Waste

Through ReThink Waste, TasWaste South is meeting its education priorities. ReThink Waste at the Libraries kicked off in December at Rosny, Hobart, and Huonville Libraries. Six workshops have already been delivered and the next six events will take place during February and March.

Expressions of interest have been sent for 2026 Schools' workshops and the calendar is filling up. So far 8 workshops in schools have been delivered in the southern region reaching 155 students.

Coming up in 2026 ReThink Waste has already committed to supporting the following events:

- Sustainable Living Tasmania's Harvesting Connections Circular Living Event;
- Agfest; and
- Tasmanian Principal's Association, 2026 Tasmanian Educational Leaders Conference.

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Operational updates

Resourcing

Finalisation of TasWaste South's grant deed for the next four years is imminent. In anticipation position descriptions have been prepared for a Project Officer (reporting to Waste Program Manager) and a communications/stakeholder engagement role with recruitment to commence once funding is confirmed. These positions will allow the organization to progress priority projects more rapidly and ensure better ongoing communication with our councils and their communities.

Legal matters, risks and issues

MRF Contract

Councils may be aware that Cleanaway has restructured its business into a vertical structure which took effect on 1 January 2026. As a result of this restructuring the position of Matt Eiszele, who was the Regional Manager for Tasmania, was made redundant and the Tasmanian MRF is now being overseen at a national level. There haven't been any changes in terms of the day-to-day operation of the MRF, however relationships at the contract management level must now be reestablished.

Container Refund Scheme

A draft refund sharing agreement was prepared and provided to Cleanaway, with their response received just prior to Christmas. Cleanaway have suggested a meeting to discuss and resolve the outstanding issues along with other contractual changes proposed. At the time of writing the meeting has yet to occur and the delay has been contributed to by operational changes within Cleanaway noted above, however it is in the process of being scheduled.

Looking ahead

The immediate priorities for the next period include the following:

- Finalising funding arrangements beyond 30 June 2026;
- Undertaking recruitment for two proposed new positions noted above;
- Concluding projects currently underway, notably the regional infrastructure plan and organics strategy as the outcomes of these projects will inform future priorities;
- Progressing the annual planning and budget cycle for 2026-2027.

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Statement of financial performance

Funding Update

A draft grant agreement has been received for the period 1 July 2026 to 30 June 2030. Comments have been provided on the draft deed on a consolidated basis with the other two regional bodies. This is a key priority for TasWaste South and is being progressed as such.

The P&L to 31 December is included below. Further expenditure will occur as key projects progress and milestones are met.

Profit and Loss

Southern Tasmanian Regional Waste Authority T/A TasWaste South
For the 6 months ended 31 December 2025

Account	Jul-Dec 2025
Trading Income	
Council Contributions	80,000.00
Rethink Waste Grant	1,163,873.00
Operational Grant	2,000,000.00
Total Trading Income	3,243,873.00
Gross Profit	
3,243,873.00	
Other Income	
Interest Revenue	626.30
Total Other Income	626.30
Operating Expenses	
Accounting and Auditing Costs	21,695.00
Advertising	2,948.13
Banks Fees	608.08
Board Expenses	714.82
Board Fees	25,162.31
Communications and Marketing	31,291.09
Conferences & Seminars	1,261.41
Corporate Memberships	4,666.37
CRS	17,799.50
Depreciation	5,945.62
Educational Consumables - Merchandise	1,933.56
FBT Expenses	4,574.00
FOGO	12,258.50
Garage Sale Trail	25,800.00
Graphic Design & Website	8,791.25
Hazardous Waste Collection Program	56,851.75
Insurance Costs	14,175.75

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Interest Charged	5.55
IT Expenses	3,956.32
Legal Costs	7,803.44
Light and Power	722.14
Local Government Forum	3,038.54
Meeting Expenses	504.26
Motor Vehicle Costs	8,936.43
MRF	3,615.00
Office Equipment (2030-05)	753.59
Office Rental	15,430.02
Officer Network	906.28
Other	100.75
Printing & Stationary	1,196.02
Procurement for Local Government	10,352.50
Professional Development - Board	5,800.00
Professional Development and Training	11,015.67
Public Events (Including Awards)	3,186.88
Regional Infrastructure Plan	5,967.50
Repairs and Maintenance	435.73
Rethink Waste Grant Payment to CCWMMG	227,371.17
Rethink Waste Grant Payment to CN	201,468.25
Salaries & Wages	287,981.90
Stakeholder Engagement	1,535.00
Subscriptions & Licences	8,446.45
Sundry Office Expenses	3,951.26
Superannuation	35,022.13
Superannuation - Board	2,844.66
Telephone & Internet	5,101.37
Transport	451.13
Travel & Accommodation	10,480.54
Uniforms	1,022.29
Work Health and Safety	15,596.78
Motor Vehicle Costs - Paul	7,499.96
Total Operating Expenses	1,128,976.65
Net Profit	2,115,522.65

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TASMANIAN PLANNING SCHEME – SOUTHERN MIDLANDS

Amendment DA2500105

Instrument of Certification

Amend SOU-Site specific Qualifications within the Southern Midlands Local Provisions Schedule as described below:

Reference Number	Site reference	Folio of the Register	Description (modification, substitution or addition)	Relevant Clause in State Planning Provisions
SOU-20.6	20 Interlaken Road, Oatlands	44807/6	<p>an additional Discretionary Use Class for the site is:</p> <p>Vehicle Fuel Sales and Service if only for the sale of fuels, charging of electric vehicles, and provision of a dedicated rest area, provided no other activity associated with the use is proposed; and</p> <p>The Rural zone is an applicable zone of the Table C1.6 for Canopy signs, or Blade signs, if for directing attention to the sale of fuel, or charging of electric vehicles, and no other use or activity</p>	<p>Rural Zone – clause 20.2 Use Table</p> <p>Signs Code clause C1.6.1 Design and Siting of Signs</p>

And amend the planning scheme maps to show a site-specific qualification on 20 Interlaken Road, Oatlands (folio of the Register 44807/6)

THE COMMON SEAL of the

Southern Midlands Council is affixed, pursuant to the Council's resolution in the presence of:

Date:

Tim Kirkwood

General Manager



Application for Planning Scheme Amendment

Lodgement Date:	Property Id No:
Application No:	Zoning:

Type of Amendment: Change to Maps Change to Ordinance

Description of Amendment: Site Specific Qualification to insert Vehcile Fuel Sales and Hire Use.

Location of Proposed Development:
 Address: 20 Interlaken Road
 Suburb/Town: Oatlands Postcode: 7120
 Certificate of Title No: 44807/6 Lot No:

Current Owners: C&M LIVESTOCK CONSULTING TASMANIA PTY LTD

Is a related application for development or subdivision also being submitted in accordance with Section 43A of the Land Use Planning and Approvals Act 1993? YES NO

Applicant:
 Name (Mr/Mrs/Ms) e3planning pty ltd
 Address: PO Box 58 South Hobart
 Suburb/Town: 0438376840 Postcode: 7004
 Telephone (Daytime Contact): Facsimile:

If you have had pre-application discussions with a Council Officer, please give their name. Grant Finn

Crown Signature:

Owners Signature: *C Maxwell* *Grant Finn* Date: 01/06/2025

Declaration:

- I have read the Certificate of Title and Schedule of Easements for the land and I am satisfied that this application is not prevented by any restrictions, easements or covenants.
- I authorise the provision of a copy of any documents relating to this application to any person for the purposes of assessment or public consultation. I agree to arrange for the permission of the copyright owner of any part of this application to be obtained. I have arranged permission for Council's representatives to enter the land to assess this application.
- In accordance with Section 33(2A) of the Land Use Planning and Approvals Act 1993, the written consent of the owners to the making of the request is attached.
- I declare that the information in this application is true and correct.

Applicant's Signature: *Grant Finn* Date: 01 June 2025

PLEASE SEE CHECKLIST OVER PAGE

PRIVACY STATEMENT

The Southern Midlands Council abides by the Personal Information Protection Act 2004 and views the protection of your privacy as an integral part of its commitment towards complete accountability and integrity in all its activities and programs. Collection of Personal Information: The personal information being collected from you for the purposes of the Personal Information Protection Act, 2004 and will be used solely by Council in accordance with its Privacy Policy. Council is collecting this information from you in order to process your application. Disclosure of Personal Information: Council will take all necessary measures to prevent unauthorised access to or disclosure of your personal information. External organisations to whom this personal information will be disclosed as required under the Building Act 2000. This information will not be disclosed to any other external agencies unless required or authorised by law.

Correction of Personal Information: If you wish to alter any personal information you have supplied to Council please telephone the Southern Midlands Council on 62545050 Please contact the Council's Privacy Officer on (03) 6254 5000 if you have any other enquires concerning Council's privacy procedures.



Planning Scheme Amendment Checklist

To ensure that we can process your application as quickly as possible, please read the following checklist carefully and ensure that you have provided all the necessary information. If you are unclear on any aspect of your application. Please contact our Planning Department on 6259 3011 to discuss or arrange an appointment concerning your proposal.

All requests for Amendments require the following information to be provided at the time of submitting the application. *However upon assessment, additional information may be requested.*

- A completed Application for Planning Scheme Amendment form. Please ensure that this form has the correct address and contact details, contains an accurate description of the proposal, is signed by the applicant and is dated.
- A current copy (less than 3 months old) of the Certificate of Title to the land containing the:
 - Search Page
 - Plan, Sealed Plan or Diagram
 - Any Schedule of Easements, Covenants, Council Notifications or Conditions of Transfer
- 1 copy of a written submission supporting the amendment including:
 - Detailed description of the requested amendment identifying the extent of its application.
 - Consideration of the Objectives of the Land Use Planning and Approvals Act 1993
 - Consideration of the relevant provisions of the Planning Scheme such as the Intents, Development Principles, Objectives and detailed provisions of this zone
 - Strategic impact of the proposal such as alternative uses, flow on development and cumulative impacts
- Relevant Application Fees (See Application Fees form)



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APPLICATION FOR PLANNING PERMIT DEVELOPMENT / USE

Use this form to apply for a permit in accordance with section 57 and 58 of the *Land Use Planning and Approvals Act 1993*

Proposed use/development:
(Provide details of proposed works and use).

Fuel Outlet

Location of Development:
(If the development includes more than one site, or is over another property include address of both Properties).

20 Interlaken Road Oatlands

Certificate of Title/s Volume Number/Lot Number:

CT44807/6

Land Owners Name:

C&M LIVESTOCK CONSULTING TASMANIA PTY LTD

Full Name/s or Full Business/Company Name

Applicant's Name:

E3planning pty ltd

Full Name/s or Full Business/ Company Name (ABN if registered business or company name)

Contact details:

PO Box 58
South Hobart Tas 7004:

Telephone or Mobile:
0438376840

Email address:
evan@e3planning.com.au

(Please note it is your responsibility to provide your correct email address and to check your email for communications from the Council.)

Details

Tax Invoice for application fees to be in the name of:
(if different from applicant)

TAS Petroleum Pty Ltd

Full Name/s or Full Business or Company Name and ABN if registered business or company name

As Above
Print email address

ABN

What is the estimated value of all the new work proposed

\$ 5 million



For Commercial Planning Permit Applications Only

Signage: Yes No

If yes, attach details: size, location and art work

week Business Details:	Existing hours of operation				Proposed hours of new operation 24 hours 7 days per			
	Hours	am	to	pm	Hours	am	to	pm
Weekdays								
Sat								
Sun								

Number of existing employees: Number of proposed new employees:

Traffic Movements:	Number of commercial vehicles serving the site at present	As per the attached TIA	Approximate number of commercial vehicles servicing the site in the future	As per the attached TIA
Number of Car Parking Spaces:	How many car spaces are currently provided	As per the attached TIA	How many new car spaces are proposed	As per the attached TIA

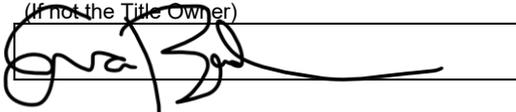
Is the development to be staged: Please tick ✓ answer
Yes No X

Please attach any additional information that may be required by Part 6.1 *Application Requirements* of the Tasmanian Planning Scheme – Southern Midlands.

Signed Declaration 

I/we as owner of the land or person with consent of the owner hereby declare that:

- I/we have read the Certificate of Title and Schedule of Easements for the land and I/we are satisfied that this application is not prevented by any restrictions, easements or covenants.
- I/we provide permission by or on behalf of the applicant for Council officers to enter the site to assess the application.
- The information given in this application is true and accurate. I/we understand that the information and materials provided with this application may be made available to the public. I/we understand that the Council may make such copies of the information and materials as, in its opinion, are necessary to facilitate a thorough consideration of the application.
- I/we have secured the necessary permission from the copyright owner to communicate and reproduce the plans submitted with the application for assessment. I/we indemnify the Southern Midlands Council for any claim or action taken against it regarding a breach of copyright in respect of any of the information or material provided.
- I/we declare that, in accordance with Section 52(1) of the Land Use Planning and Approvals Act 1993, that I have notified the owner of the intention to make this application. Where the subject property is owned or controlled by Council or the Crown, their consent is attached and the application form signed by the Minister of the Crown responsible and/or the General Manager of the Council.

Applicant Signature (if not the Title Owner)


Applicant Name (please print)
Evan Boardman

Date
20 August 2025



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Land Owner(s) Signature
The landowners have been notified of the making of the application as per the Land Use Planning and Approvals Act 1993

Land Owners Name (please print)

Date

Land Owner(s) Signature

Land Owners Name (please print)

Date

PRIVACY STATEMENT

The Southern Midlands Council abides by the Personal Information Protection Act 2004 and views the protection of your privacy as an integral part of its commitment towards complete accountability and integrity in all its activities and programs.

Collection of Personal Information: The personal information being collected from you for the purposes of the Personal Information Protection Act, 2004 and will be used solely by Council in accordance with its Privacy Policy. Council is collecting this information from you in order to process your application.

Disclosure of Personal Information: Council will take all necessary measures to prevent unauthorised access to or disclosure of your personal information. External organisations to whom this personal information will be disclosed as required under the Building Act 2000. This information will not be disclosed to any other external agencies unless required or authorised by law.

Correction of Personal Information: If you wish to alter any personal information you have supplied to Council please telephone the Southern Midlands Council on (03) 62545050. Please contact the Council's Privacy Officer on (03) 6254 5000 if you have any other enquires concerning Council's privacy procedures.

Information & Checklist Sheet

DEVELOPMENT / USE

Use this check list for submitting your application for a planning permit –Please do not attach the check list with your application

Submitting your application ✓

1. All plans and information required per Part 6.1 Application Requirements of the Tasmanian Planning Scheme i.e.: site plan showing all existing buildings, proposed buildings, elevation plans etc.
2. Copy of the current Certificate of Title, Schedule of Easements and Title Plan (Available from Service Tasmania Offices)
3. Any reports, certificates or written statements to accompany the Application (if applicable) required by the relevant zone or code.
4. Prescribed fees payable to Council

Information

If you provide an email address in this form then the Southern Midlands Council (“the Council”) will treat the provision of the email address as consent to the Council, pursuant to Section 6 of the Electronic Transactions Act 2000, to using that email address for the purposes of assessing the Application under the Land Use Planning and Approvals Act 1993 (“the Act”).

If you provide an email address, the Council will not provide hard copy documentation unless specifically requested.

It is your responsibility to provide the Council with the correct email address and to check your email for communications from the Council.

If you do not wish for the Council to use your email address as the method of contact and for the giving of information, please tick ✓ the box

Heritage Tasmania

If the Property is listed on the Tasmanian Heritage Register then the Application will be referred to Heritage Tasmania unless an Exemption Certificate has been provided with this Application. (Phone 1300 850 332 (local call cost) or email enquires@heritage.tas.gov.au)

TasWater

Depending on the works proposed Council may be required to refer the Application to TasWater for assessment (Phone 136992)

IMPORTANT: There is no connection between Planning approval and Building & Plumbing approvals.

Owners are to ensure that the work is either Low-Risk Building Work, Notifiable Building Work or Permit work in accordance with the Directors Determination – Categories of Building & Demolition Work v 1.4 dated 12 March 2021 prior to any building works being carried out on the land.

https://www.cbos.tas.gov.au/_data/assets/pdf_file/0014/405014/Directors-determination-categories-of-building-and-demolition-work-2021.pdf



Planning Scheme Amendment and Development Application

20 Interlaken Road Oatlands TAS 7120

Client: TAS PETROLEUM Pty Ltd

Author: Evan Boardman

Date: February 2026

PO Box 58 South Hobart TASMANIA 7004
0438 376 840 evan@e3planning.com.au
www.e3planning.com.au



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Authorised by

A handwritten signature in black ink, appearing to read "Evan Boardman".

Name: Evan Boardman 9 February 2026





1. INTRODUCTION

- 1.1. This is a planning report to support a Combined Planning Scheme Amendment and Development Application to provide for a Vehicle Fuel Outlet (the Proposal) at 20 Interlaken Road Oatlands (the Property).
- 1.2. The scheme amendment proposed is limited to a Site-Specific Qualification (SSQ).
- 1.3. The Proposal would improve the efficiency and effectiveness of the Midland Highway (the Highway) by providing a conveniently located vehicle fuel outlet adjacent to the Highway equidistant between Cambell Town and Kempton a distant of 87km. It would be located on a property (the Site) which was previously used as a material storage area for upgrades to the Highway.
- 1.4. The Proposal would introduce competition into fuel sales on the Midland Highway as all other outlets are owned by the same brand. Based upon the current midland highway pricing, once Tas Petroleum enters the market there will be a market adjustment. This is estimated to be 10 cents below current fuel prices.
- 1.5. The Proposal would not be in direct competition with the existing BP Service Station in Oatlands as no retail offering other than fuel would be provided nor would any motor vehicle repair or maintenance be offered. These would be expressly prohibited by the wording of the SSQ.
- 1.6. The strategic location of the Proposal provides access for travellers and transport operators, reducing the logistical challenges faced during long-haul routes. By situating the fuel outlet equidistant between Campbell Town and Kempton, it enhances service accessibility and caters to the growing demand for competitive fuel options in the region.





- 1.7. By providing a competitive fuel outlet, the Proposal addresses the current monopoly on fuel sales, offering tangible benefits to local businesses and motorists. The reduced fuel costs contribute to lowering operational expenses for freight companies and individual motorists, which can have a ripple effect on the broader economy.
- 1.8. Per annum it is estimated that motorists and trucking companies would save \$3.37 million.

2. Supporting Documentation

- 2.1. The following lists the supporting documentation for this Proposal, these documents should be considered in conjunction with this Report.
 - Property Titles – Appendix A
 - Plans and Elevations, Proposed Fuel Stop 20 Interlaken Road, Oatlands, Prime Design, 4 February 2026 Appendix B
 - 20 Interlaken Road, Oatlands, Traffic Impact Assessment, Midson Traffic, July 2025 Appendix C
 - Bushfire Hazard Management Report: Hazardous Use, Report for: Tas Petroleum, Property Location: 20 Interlaken Road, Oatlands, & Emergency Management Strategy Prepared by: Scott Livingston 28 April 2025 Appendix D
 - Stormwater Management Report, Proposed Fuel Stop 20 Interlaken Road Oatlands, Rare, 30 October 2025 Appendix E.
 - Flood Hazard Report, Proposed Fuel Stop 20 Interlaken Road Oatlands, Rare, 30 October 2025 Appendix F.
 - Letter from landowners C & M Livestock Consulting Tasmania Pty Ltd 4 June 2025 Appendix G





- Email from State Roads Department of State Growth, (Traffic Impact on the Interlaken/Midlands Highway junction acceptable) 12 August 2025 Appendix H
- 20 Interlaken TPC Form 1 Appendix I

3. Site Description

- 3.1. The Site is 20 Interlaken Road Oatlands as shown in Figures 2 and 3, it has a Property Title reference number of CT 44807/6.
- 3.2. The site is over 140 metres from the centreline of the Midland Highway.
- 3.3. Oatlands is situated approximately between 300m and 1.5kms from the site on the opposite (eastern side) of the Midland Highway.
- 3.4. The Site has an area of 1.7ha and has been cleared of all native vegetation. There are no significant natural values associated with the Site.
- 3.5. No rural activities or other uses are undertaken on the Site.
- 3.6. The principal need for the SSQ is that all land along the Midland Highway is zoned either Rural or Agriculture with vehicle fuel sales and service prohibited in both zones. This has created a situation where all uses which might be associated with the midland Highway prohibited.
- 3.7. A copy of the Property Title is included as Appendix A.
- 3.8. The site was previously used as a materials storage and construction area for the upgrading of the Midland Highway as can be seen in Figure 1.





Figure 1: Image of the Site showing it being used to store materials for the upgrading of the Midland Highway.



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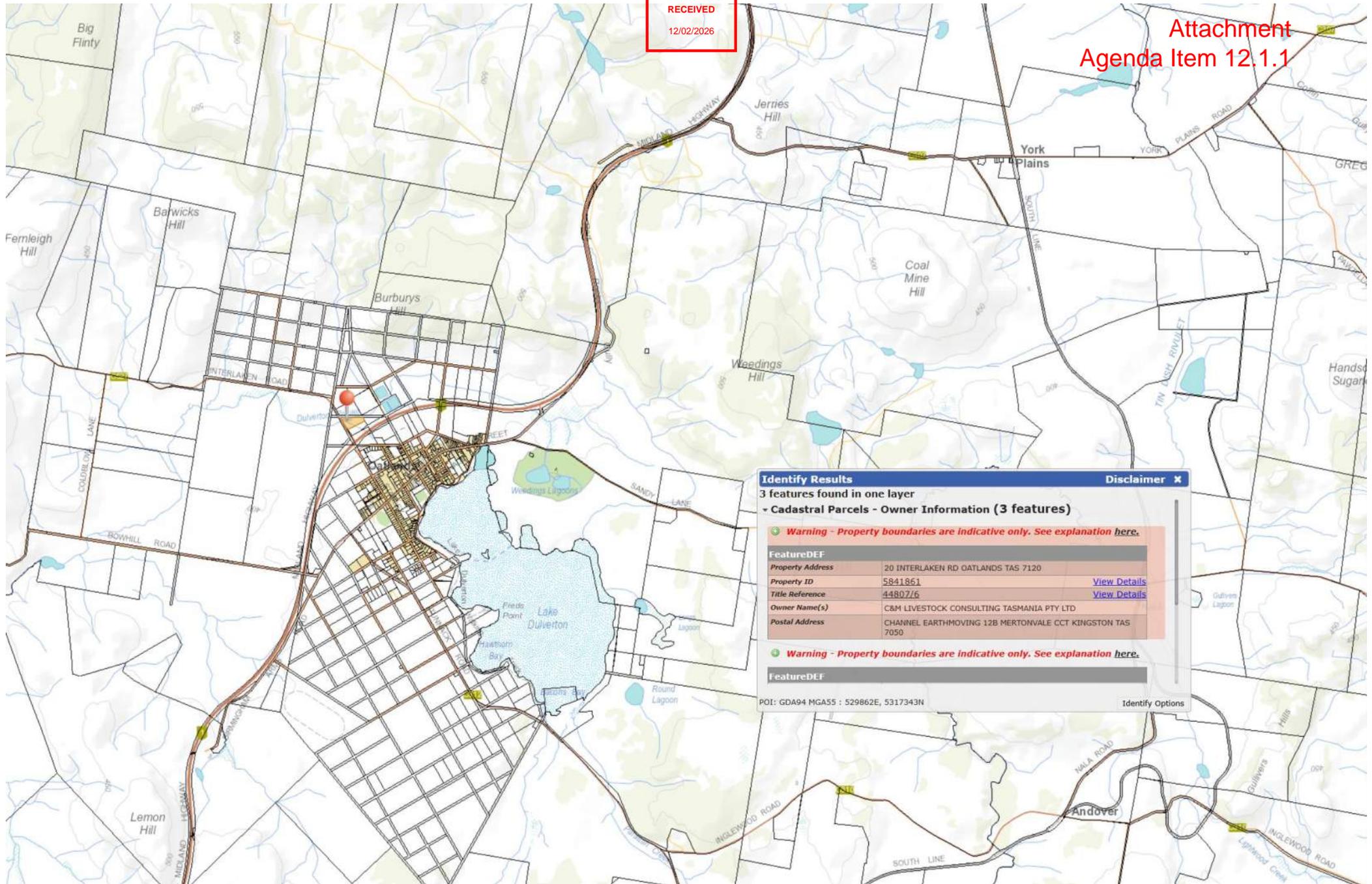


Figure 2: Site Location



Figure 3: Site Location

4. TASMANIAN PLANNING SCHEME – SOUTHERN MIDLANDS LOCAL PROVISIONS SCHEDULE (THE SCHEME)

- 4.1. The Site is zoned Rural under the Scheme as shown in Figure 4 and is subject to the *Natural Assets Code Waterway and Coastal Protection* as shown in Figure 5, the *Bushfire Prone Areas Code* as shown in Figure 6, the *Scenic Protection Code* as shown in Figure 7 and the *Signs Code*.

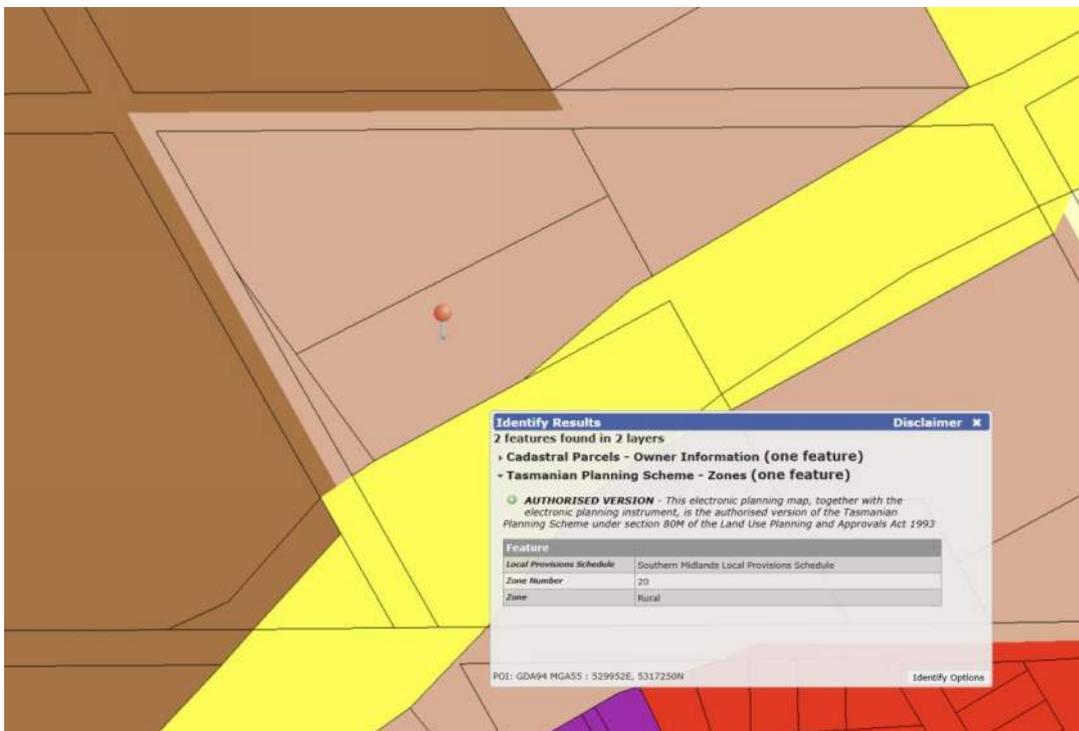


Figure 4: Rural Property Zoning as per the Scheme

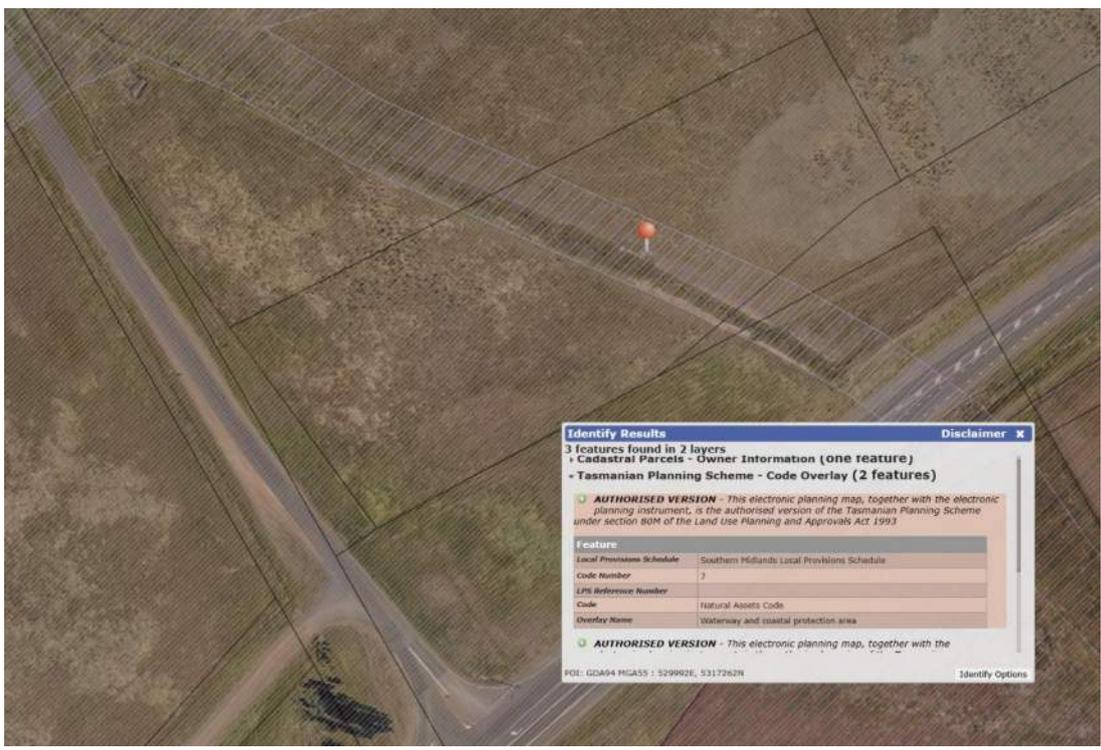


Figure 5: Natural Assets Code Waterway and Coastal Protection

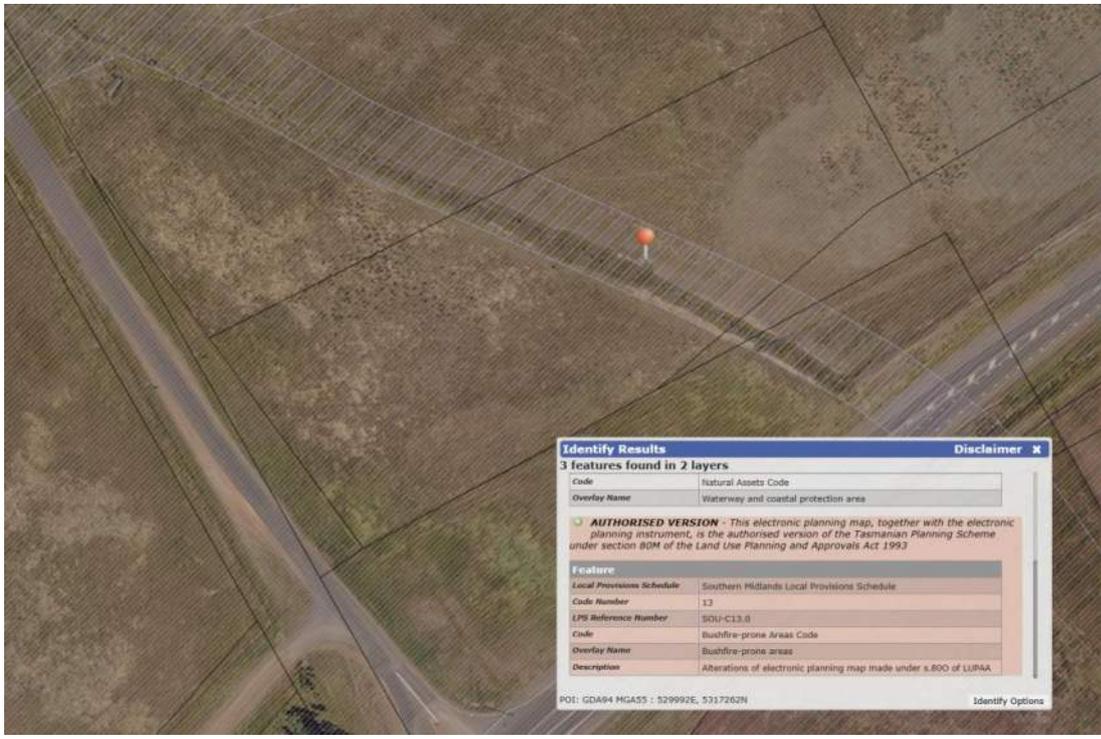


Figure 6: Bushfire Prone Areas Code



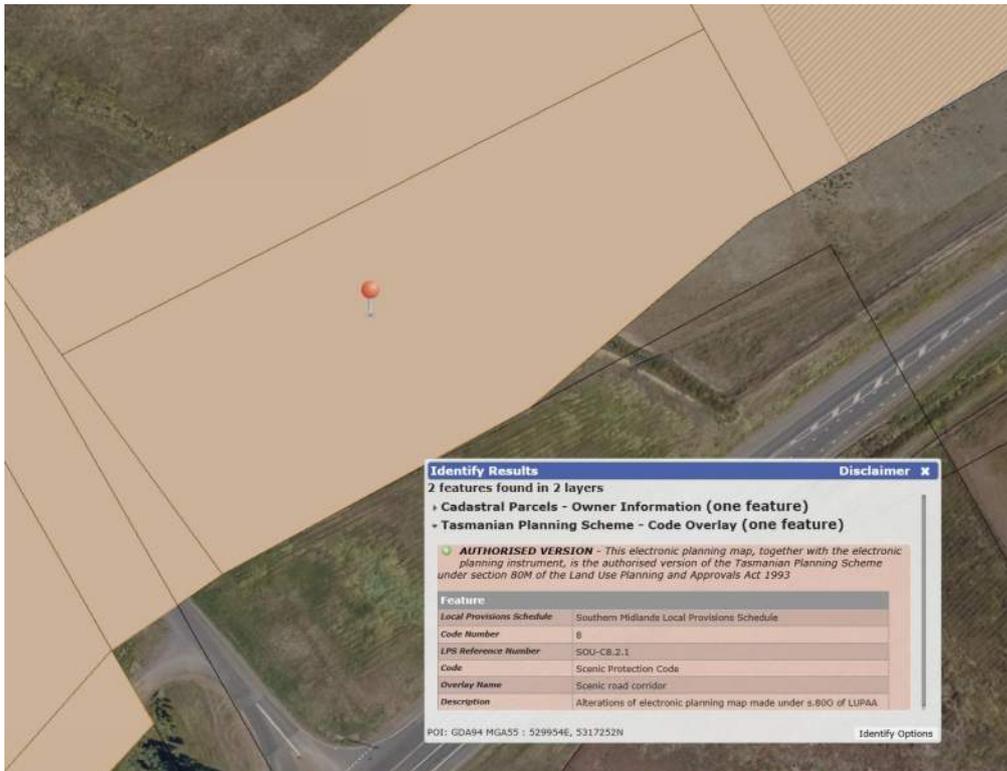


Figure 7: Scenic Protection Code

- 4.2. The relevant clauses from the Scheme that relate to the Proposal are considered is section 8 of this report.





5. DEVELOPMENT APPLICATION (THE PROPOSAL)

5.1. It is proposed to develop the Site for a vehicle fuel sales outlet and electrical vehicle charging station (the Proposal). A 3D render of the fuel sales outlet is shown in Figure 8. The Proposal comprises:

- 5.1..1. Undercover roofed area 19m x 13m – 247m²
- 5.1..2. Three electric vehicle charging stations
- 5.1..3. Eight fuel bowsers.

- 5.1..3.1. 6 x 10 hose dispensers for light/medium vehicles

- 5.1..3.2. 2 x 6 hose dispensers for heavy vehicles

- 5.1..3.3. 28 diesel hoses – 8 of these will service heavy vehicles.

- 5.1..3.4. 36 petrol hoses

- 5.1..3.5. 8 AdBlue hoses

- 5.1..4. Fuel storage tanks

- 2 x 110,000 L, split compartment underground tanks and 1 x 25,000 L underground tank. Compartments split into the following grades – *(Final design, capacities and grades may change due to operational requirements.)*

- 5.1..4.1. 70,000 litres diesel

- 5.1..4.2. 20,000 litres of premium diesel

- 5.1..4.3. 20.0kl – Premium Diesel

- 5.1..4.4. 20.0kl – Premium Diesel

- 5.1..4.5. 65.0kl – Unleaded 91

- 5.1..4.6. 22.5kl – Premium Unleaded 95

- 5.1..4.7. 22.5kl – Premium Unleaded 98

- 5.1..4.8. 25.0kl – AdBlue





5.2. Amenities block comprising

- 5.2..1.1. Three bathrooms with toilets and showers
- 5.2..1.2. Control room
- 5.2..1.3. Rest Area

5.3. Signage

- A canopy sign 2m x 800mm is proposed on the southern and western elevations

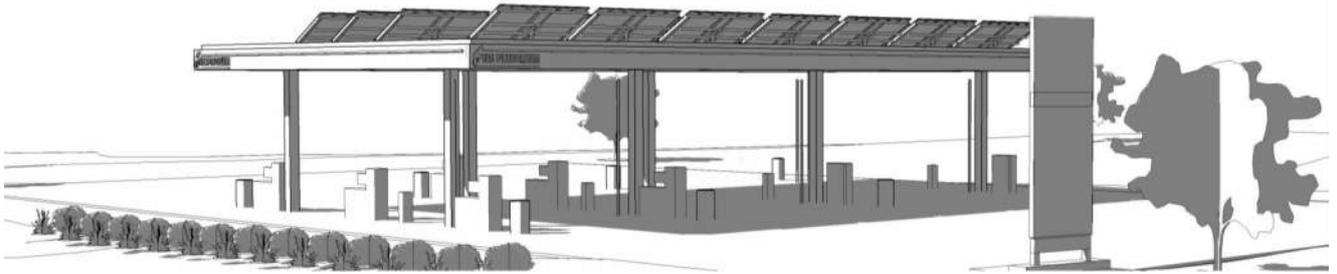


Figure 8: 3D render of the Proposal.

5.4. A copy of the Site Plan is shown in Figure 9.

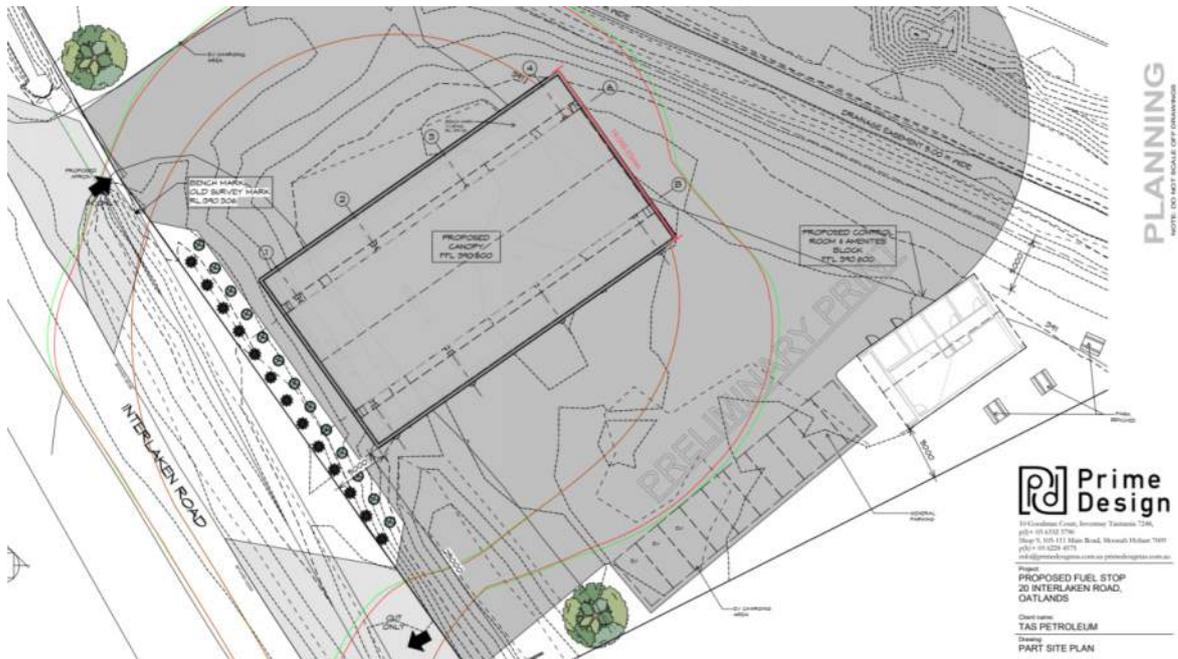


Figure 9: Site Plan of the proposal

- 5.5. The maximum height of the Proposal is 7.9m including the solar panel and 7 metres to the top of the roof.
- 5.6. The rest area would be entirely compliant with the *Austroads Guidelines for the Provision of Heavy Vehicle Rest Area Facilities Edition 1.1.*
- 5.7. Fuel Sales from the outlet are estimated at 1,000kL per month 20% Petrol and 80% Diesel.
- 5.8. The majority of fuel sold would be to service the heavy vehicle fleet the majority of whom would have existing fleet supply contracts with Tas Petroleum.
- 5.9. Tas Petroleum would own and operate the facility.
- 5.10. One part time employee would service the facility.
- 5.11. The facility would operate as a self-serve facility.
- 5.12. The facility would not offer any associated retail services or items.





- 5.13. A full copy of the Site Plan, Floor Plans and Elevations are included as Appendix B.
- 5.14. Access to the Proposal would be 150 metres from the Highway to the west off Interlaken Road. A full copy of the Traffic Impact Assessment is included as Appendix C.
- 5.15. Bushfire Risk has been addressed through a Bushfire Hazard Assessment, this is included within Appendix D and E.



6. TAS PETROLEUM

- 6.1. Tas Petroleum is a Tasmanian-owned fuel and lubricants distributor, providing bulk fuel delivery services across Tasmania, catering to industries like agriculture, transport, mining, and marine. They are headquartered in Bridgewater, Tasmania.

Fuel, Lubricant, Solvent and Adblue® Partners



Tas Petroleum has outlets across Tasmania as shown in Figure 7.

- 6.2. Their main retail customers are light vehicles and small commercial traffic. Larger account customers are heavy vehicle operators including Chas Kelly Transport, Searoad Logistics, Team Global Express, Orana Enterprises, Gradco, Padgetts, Fresh Freight Tasmania, SRT Logistics and Hazell Bros, etc.
- 6.3. The majority of the fuel sales are to larger commercial fleet operators which have accounts with Tas Petroleum. As can be seen from Figure 10, Tas Petroleum has no outlets between Longford in the north and Bridgewater in the South. Establishing a presence on the Midland Highway would enable Tas Petroleum to deliver improved fuel services to all Tasmanians.
- 6.4. It is the largest Tasmanian owned and operated fuel distributor employing over 55 Tasmanians.



Figure 10: Tas Petroleum outlets across Tasmania.

6.5. To address this gap, Tas Petroleum has identified the Midland Highway as a strategic location to establish a new fuel outlet. This corridor serves as a vital artery for freight and passenger movements across the state, making it a prime area for expanding services. By catering to heavy vehicle operators and other motorists traveling along this route, Tas Petroleum can enhance its reach and streamline fuel accessibility for commercial and retail customers alike.



7. MIDLAND HIGHWAY

- 7.1. The Midland Highway is the principal north south road corridor which connects Hobart and Launceston, it is 176km in length. Its route is shown in Figure 11.

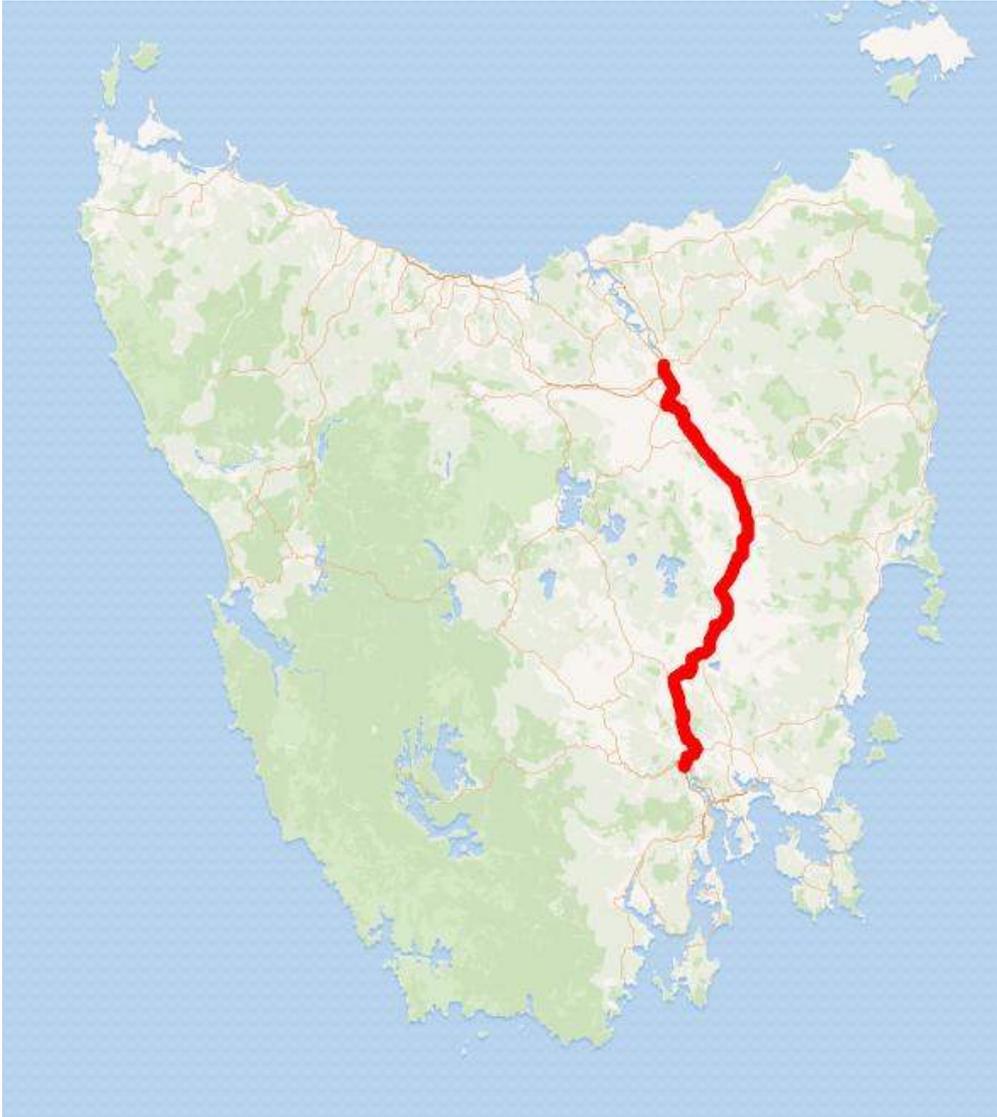


Figure 11: Midland Highway



- 7.2. The majority of towns and cities on the Highway have been bypassed with Campbell Town really the only town remaining on the Highway.
- 7.3. The Highway carries 4,500 to 5,000 vehicles a day between Kempton and Campbell Town and 7,500 between Campbell Town and Perth.ⁱⁱ
- 7.4. Figure 12 shows the existing fuel outlets on the Midland Highway.

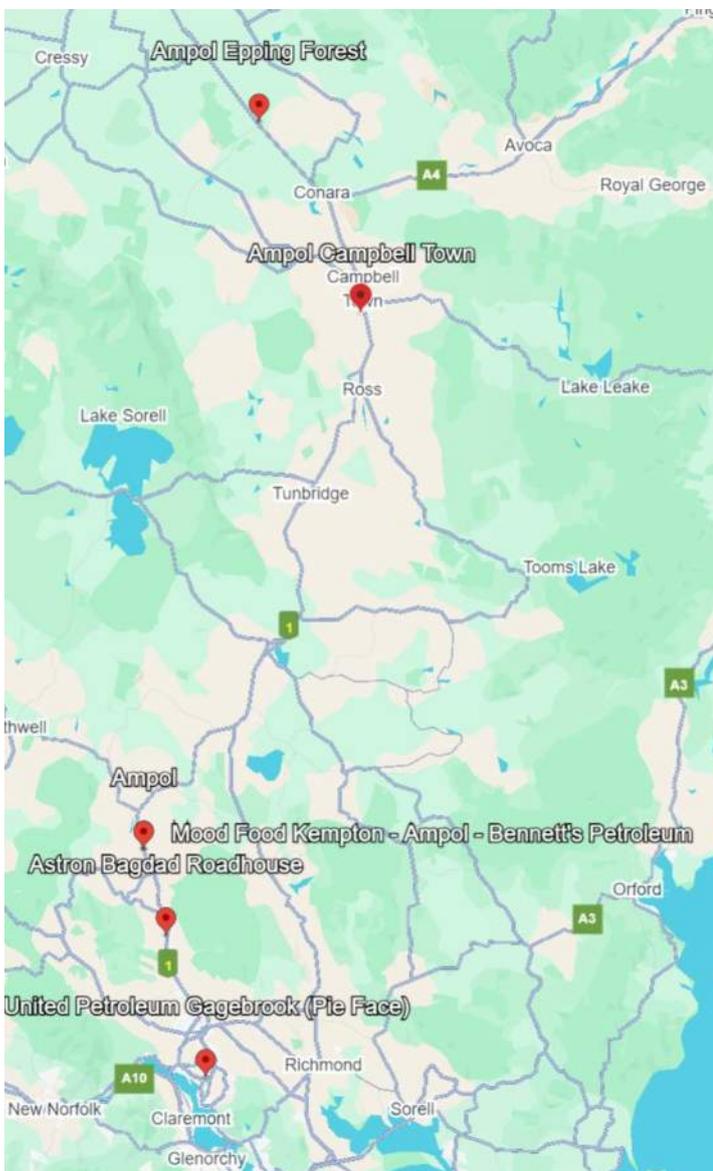


Figure 12: Map showing Fuel Outlets on the Midland Highway.



7.5. Table 1 lists the Fuel outlets.

Fuel Outlet	Operating Hours	Facilities	Truck Stop
Ampol Heritage Highway Roadhouse Bagdad	Mon-Fri 5:30am - 6:30pm Weekends 7am-7pm	Food/retail	No
Ampol Kempton	24 hours	Food/retail	Yes
Ampol Campbell Town	4am – 10pm	Food/retail	No
Ampol Campbell Town South	unmanned	Fuel only	Yes but no facilities other than a toilet.
Ampol Epping Forest	4am – 10pm	Food/retail	Not to Aust standard

Table 1: Fuel outlets on the Midland Highway and their facilities.





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- 7.6. Most of Tasmania's land freight is carried by road, with volumes highest between Burnie and Launceston, and over the Bridgewater Bridge.
- 7.7. The facilities along the Midlands Highway vary in terms of offerings and standards. While some outlets meet Australian standards and provide robust food and retail services, others fall short, particularly with respect to truck stops and rest areas which don't meet Austroads Standards catering to basic needs.
- 7.8. As can be seen from Table 1 only the Ampol Fuel Outlet at Kempton, offers a 24 hour a day service with a truck stop that meets Austroads Standards.
- 7.9. DSG estimates that 24% of vehicles on the Midlands Highway are heavy vehicles.
- 7.10. Heavy Rigid to B-Double vehicles use 35-45 litres of fuel per 100 km, while light vehicles consume 7-12 litres per 100 km.ⁱⁱⁱ



PO Box 58 South HOBART, Tasmania 7004

0438376840 evan@e3planning.com.au



8. FUEL SALES

- 8.1. It is estimated that Tasmania's Truck, Bus and Light Vehicle fuel consumption in 2024 was slightly more than 1 billion litres of petrol and diesel. ^{iv}
- 8.2. The estimated volume of fuel sold from major outlets on the Midland Highway between Kempton and Campbell Town is shown in Table 2.

Fuel Outlet	Fuel Sales
Kempton Ampol Bennetts	850 kilolitres per month
Epping Forest Ampol Bonneys	450 kilolitres per month
Campbell Town Bonneys	900 kilolitres per month
Total	2,200 kilolitres per month 26.4 million litres year annum

Table 2: Estimated sales from fuel outlets on the Midland Highway



9. DRAFT AMENDMENT (SSQ)

- 9.1. It is proposed to amend the Southern Midlands Local Provision Schedule (the Scheme) by inserting a Site-Specific Qualification (SSQ) into the table headed SOU-Site-specific Qualifications within the Scheme.
- 9.2. The SSQ would provide for the proposed Vehicle Fuel Sales and Service with the qualification that no other retail uses may be undertaken from the site.
- 9.3. Fuel Sales and Service are defined under the Scheme as *use of land primarily for the sale of motor vehicle fuel and lubricants, and if the land is so used, the use may include the routine maintenance of vehicles. An example is a service station.*
- 9.4. The proposed SSQ is shown in Table 3.

Reference Number	Site Reference	Folio of the Register	Description (modification, substitution or addition)	Relevant Clause in State Planning Provisions
SOU-20.6	20 Interlaken Road Oatlands	44807/6	An additional Discretionary Use Class for this site is: Vehicle Fuel Sales and Hire. With the qualification that <i>If not for selling or installing motor vehicle accessories or parts. Selling of food, drinks and other convenience items. Hiring of trailers or servicing or washing motor vehicles. Canopy and Blade Signs are discretionary if associated with a</i>	Rural Zone – clause 20.2 Use Table. Signs Code 1.6.1.





			<i>Vehicle Fuel Sales and Hire Class.</i>	
--	--	--	---	--

Table 3: Proposed SSQ wording.

- 9.5. The qualification is included within the Local Provision Schedule to ensure that operation of the proposed fuel sales outlet does not impact upon the retail hierarchy of Oatlands and in particular the existing BP Fuel Outlet.
- 9.6. There are five SSQ's within the Scheme as reproduced and shown in Figure 13. Of these, two provide for Vehicle Fuel Sales and Service as a discretionary use.

SOU-Site-specific Qualifications

Reference Number	Site reference	Folio of the Register	Description (modification, substitution or addition)	Relevant Clause in State Planning Provisions
SOU-20.1	3001 Midland Highway, Kempton	37224/1	An additional Discretionary Use Class for this site is: Vehicle Fuel Sales and Hire.	Rural Zone – clause 20.2 Use Table
SOU-20.2	1172 Midland Highway, Mangalore	112712/1 138003/1	An additional Discretionary Use Class for this site is: Vehicle Fuel Sales and Hire.	Rural Zone – clause 20.2 Use Table
SOU-20.3	21 Blackwell Road, Melton Mowbray	35615/1	An additional Discretionary Use Class for this site is: Hotel Industry.	Rural Zone – clause 20.2 Use Table
SOU-20.4	2120 Mudwalls Road, Colebrook	25976/1	An additional Discretionary Use Class for this site is: Hotel Industry.	Rural Zone – clause 20.2 Use Table
SOU-20.5	Whynyates Street, Oatlands	31884/1	A substitution for the qualification for the Discretionary Use Class of Service Industry for this site is: "If associated with Resource Development."	Rural Zone – clause 20.2 Use Table

Figure 13: Existing Site-Specific Qualifications from the Scheme.





- 9.7. As the proposed SSQ is virtually identical to two other SSQ's within the Scheme this is evidence that the SSQ if implemented would work and be effective.
- 9.8. Figures 14 and 15 show the properties subject to SSQ's that provide for vehicle fuel sales and service.

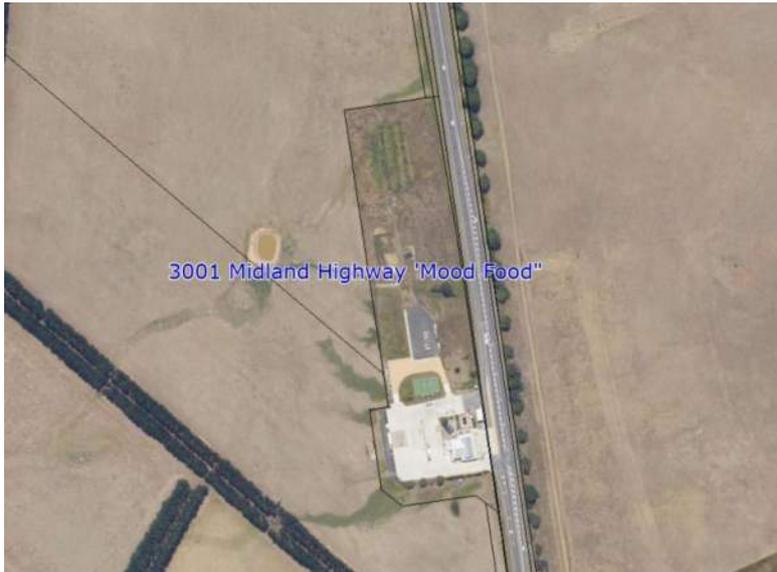


Figure 14 3001 Midland Highway Mood Food which is subject to an SSQ.



Figure 15: 1172 Midland Highway (Closed) which is subject to an SSQ.

- 9.9. The SSQ is required as Vehicle Fuel Sales and Service Use is prohibited along the length of the Highway. Land immediately adjacent to the Highway Corridor is zoned either Agriculture or Rural as shown in Figure 16.



Figure 16: Image showing zoning of land immediately adjacent to the Highway.

- 9.10. Figure 17 shows the Agriculture and Rural Zoning of the land adjacent to the Midland Highway in the vicinity of the Site and Oatlands to the east.



Figure 17: Image showing Agriculture and Rural zoning of land adjacent to the Midland Highway.

9.11. Neither of these zones provide for Vehicle Fuel Sales and Service within their respective use tables as reproduced from the Scheme and shown in Figures 18 and 19.



20.2 Use Table

Use Class	Qualification
No Permit Required	
Natural and Cultural Values Management	
Passive Recreation	
Resource Development	
Utilities	If for minor utilities.
Permitted	
Business and Professional Services	If for: (a) a veterinary centre; or (b) an agribusiness consultant or agricultural consultant.
Domestic Animal Breeding, Boarding or Training	
Educational and Occasional Care	If associated with Resource Development or Resource Processing.
Emergency Services	
Extractive Industry	
Food Services	If associated with Resource Development or Resource Processing.
General Retail and Hire	If associated with Resource Development or Resource Processing.
Manufacturing and Processing	If for the processing of materials from Extractive Industry.
Pleasure Boat Facility	If for a boat ramp.
Research and Development	If associated with Resource Development or Resource Processing.





Residential	If for: (a) a home-based business in an existing dwelling; or (b) alterations or extensions to an existing dwelling.
Resource Processing	
Storage	If for: (a) a contractors yard; (b) freezing and cooling storage; (c) grain storage; (d) a liquid, solid or gas fuel depot; or (e) a woodyard.
Utilities	If not listed as No Permit Required.
Visitor Accommodation	If for guests accommodated within an existing building.
Discretionary	
Bulky Goods Sales	If for: (a) a supplier for Extractive Industry, Resource Development or Resource Processing; (b) a garden and landscaping materials supplier; (c) a timber yard; or (d) rural supplies.
Business and Professional Services	If not listed as Permitted.
Community Meeting and Entertainment	
Crematoria and Cemeteries	
Custodial Facility	
Educational and Occasional Care	If not listed as Permitted.
Food Services	If not listed as Permitted.
General Retail and Hire	If not listed as Permitted.
Manufacturing and Processing	If not listed as Permitted.
Motor Racing Facility	
Pleasure Boat Facility	If not listed as Permitted.
Recycling and Waste Disposal	
Research and Development	If not listed as Permitted.

Figure 18: Rural Zone Use Table





21.2 Use Table

Use Class	Qualification
No Permit Required	
Natural and Cultural Values Management	
Passive Recreation	
Resource Development	If: (a) on land other than prime agricultural land; or (b) an agricultural use, excluding plantation forestry, on prime agricultural land if it is dependent on the soil as the growth medium or conducted in a manner which does not alter, disturb or damage the existing soil profile or preclude it from future use as a growth medium.
Utilities	If for minor utilities.
Permitted	
Food Services	If associated with Resource Development or Resource Processing.
General Retail and Hire	If associated with Resource Development or Resource Processing.
Pleasure Boat Facility	If for a boat ramp.
Residential	If for: (a) a home-based business in an existing dwelling; or (b) alterations or extensions to an existing dwelling.
Discretionary	





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Bulky Goods Sales	If: (a) a supplier for Extractive Industry, Resource Development or Resource Processing; (b) a garden and landscape supplier; or (c) a timber yard.
Domestic Animal Breeding, Boarding or Training	
Educational and Occasional Care	
Emergency Services	
Extractive Industry	
Food Services	If not listed as Permitted.
General Retail and Hire	If not listed as Permitted.
Manufacturing and Processing	If for: (a) the manufacturing of agricultural equipment; or (b) the processing of materials from Extractive Industry.
Research and Development	
Residential	If: (a) not restricted by an existing agreement under section 71 of the Act; and (b) not listed as Permitted.
Resource Development	If not listed as No Permit Required.
Resource Processing	
Storage	If for: (a) a contractors yard; (b) freezing and cooling storage; (c) grain storage; (d) a liquid, solid or gas fuel depot; or (e) a woodyard.
Tourist Operation	
Transport Depot and Distribution	If for the transport and distribution of agricultural produce and equipment.
Utilities	If not listed as No Permit Required.

Figure 19: Use Table Agriculture Zone



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9.12. This zoning restriction necessitates alternative solutions for accommodating Vehicle Fuel Sales and Service Uses while complying with regional planning standards. Given the Midland Highway's strategic importance as a major corridor linking Hobart and Launceston, it is essential to evaluate existing fuel outlet facilities and their operational capacity to meet demand across this stretch.

9.13. **Use Standards of the Rural Zone**

9.14. The Proposal does not involve any amendment to the Use Standards within the Rural Zone.

9.15. **Development Standards**

9.16. The Proposal does not involve any amendment to the Development Standards within the Rural Zone.

9.17. **Zone Purpose**

20.1 Zone Purpose The purpose of the Rural Zone is:

20.1.1 To provide for a range of use or development in a rural location:

(a) where agricultural use is limited or marginal due to topographical, environmental or other site or regional characteristics;

(b) that requires a rural location for operational reasons;

(c) is compatible with agricultural use if occurring on agricultural land; and

(d) minimises adverse impacts on surrounding uses.

20.1.2 To minimise conversion of agricultural land for non-agricultural use.

20.1.3 To ensure that use or development is of a scale and intensity that is appropriate for a rural location and does not compromise the function of surrounding settlements.

9.18. **Response**





- 9.19. The Proposal must be located within this rural location as it provides for a fuel outlet within the immediate vicinity of the Midlands Highway.
- 9.20. The Property has low agricultural value, being relatively small in area (less than 2ha) without any available water for irrigation or for agricultural uses.
- 9.21. All properties within the immediate vicinity are of a similar size and also have limited agricultural potential.
- 9.22. The only change to the Planning Scheme ordinance is the SSQ and if approved the only difference that would result would be that a fuel sales outlet without any other retail could be constructed on the property. All Use and Development Standards and Codes of the Scheme would still need to be addressed.
- 9.23. The only signs that could be considered for approval on the Property would be ones directly associated with a fuel sales outlet.

10. C1.0 SIGNS CODE

- 10.1. A Canopy sign is proposed as shown in Figure 20.
- 10.2. A Canopy Sign is defined under the Scheme as *means a [sign](#) attached to the perimeter of a canopy on a [building](#) for the purpose of shielding from the elements such as, signs on the fascia of a canopy over a [service station](#).*
- 10.3. A Canopy Sign is permissible within the Rural Zone if it complies with the following:
 - (a) have a minimum clearance above ground level of 2.4m; and
 - (b) not be closer than 450mm from a vertical projection of the kerb line of any [road](#).



10.4.

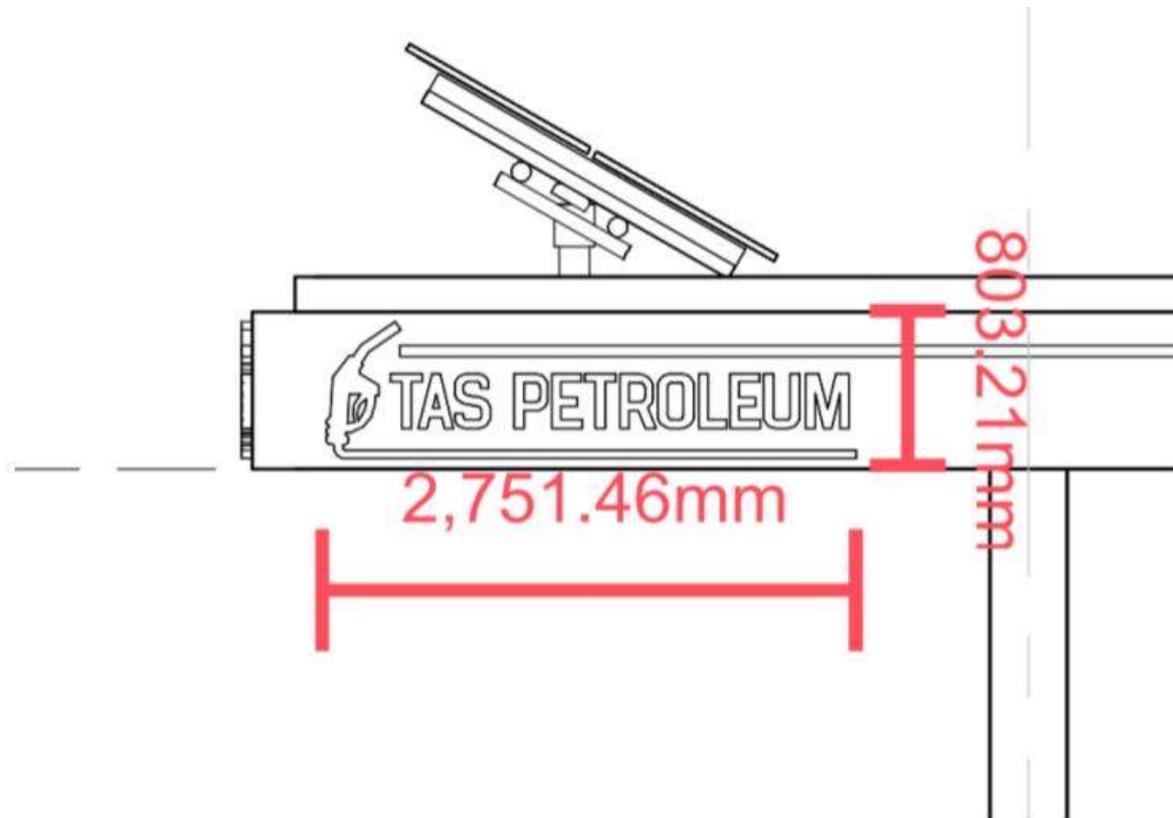


Figure 20: Proposed Canopy Signage appearance

10.5. The Canopy Sign is greater than 2.4m above ground level and greater than 450mm from the kerb line of Interlaken Road.

11. C2.0 PARKING AND SUSTAINABLE TRANSPORT CODE,

11.1. A Traffic Impact Assessment is attached to this report as Appendix C. It found as follows.

The development will generate a total of 400 vehicles per day with a peak of 50 vehicles per hour. The nature of the development will result in the majority of this traffic generation being 'linked trips', where a vehicle already travelling on the Midland Highway will divert into and out of the proposed development.



The traffic generation associated with the proposed development meets the requirements of Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme.

The development will provide a new access on Interlaken Road. The access has been designed to accommodate the swept paths of A-doubles and semi-trailers. The access arrangements will facilitate separated entry and exit driveways for the site.

The Midland Highway/ Interlaken Road intersection can safely accommodate 26m B-Double movements associated with the proposed development. Swept path analysis confirms that the existing channelised turn lanes provide adequate width and geometry for these vehicle types. The channelisation of entering left and right turn lanes provide appropriate separation of truck movements from through movements on the Midland Highway.

- Interlaken Road has sufficient width to accommodate two-way heavy vehicle traffic between the development site and the Midland Highway. The 6-7 metre sealed carriageway width exceeds the 5.5 metres required for two-way B-Double movements.
- The Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme is met as it does not trigger a need for parking. A total of 10 on-site parking spaces are provided. This includes 3 x EV spaces. The parking can be used for access to the on-site amenities provided, as well as vehicles accompanying vehicles that are being refuelled or recharged.

12. C7.0 NATURAL ASSETS CODE – WATERWAY AND COASTAL PROTECTION

- 12.1. The Property is subject to the Natural Assets Code Waterway and Coastal Protection Area and any development application within the area subject to the code would need to address the code.
- 12.2. A report similar to the *Stormwater Management Report, Proposed Fuel Stop 20 Interlaken Road Oatlands, Rare, 30 October 2025*





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12.3. The State Stormwater Strategy is relevant to this and the section on pollutants is reproduced below. ‘

1.2.1. Stormwater pollutants

Stormwater runoff collects a range of pollutants that have been deposited on the land surface including litter, pathogens, nutrients, hydrocarbons, heavy metals and pesticides. These pollutants not only affect receiving waters but also the way we use waterways for recreation and economic benefit. Table 2 provides more detail on the types of pollutants found in stormwater, their possible sources and their potential impacts.

12.4. This is addressed in the Stormwater Management Report, Proposed Fuel Stop 20 Interlaken Road Oatlands, Rare, 30 October 2025

13. C8.0 SCENIC PROTECTION COD

13.1. The Property is subject to the Scenic Protection Code Scenic Road Corridor. Table 4 assesses the development application against the relevant provisions of the Code.



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Relevant Provision	Response
A1 Destruction of exotic trees with a height more than 10m, native vegetation , or hedgerows within a scenic road corridor must not be visible from the scenic road .	No trees exotic or otherwise exist on the property.
A2 Buildings or works within a scenic road corridor must not be visible from the scenic road .	The only component of the development application within the Scenic Road Corridor would be the amenities block.





<p>P2 Buildings or works within a scenic road corridor must not cause an unreasonable reduction of the scenic value of the road corridor, having regard to:</p> <ul style="list-style-type: none"> a) the topography of the site b) proposed reflectance and colour of external finishes c) design and proposed location of the buildings or works d) the extent of any cut or fill required. e) any existing or proposed screening f) the impact on views from the road; and g) the impact on views from the road; and h) the purpose of any management objectives identified in the relevant Local Provisions Schedule. 	<p>The site is relatively flat.</p> <p>The external finishes of the amenities block will be finished in muted tones and if approved a condition of approval could specify this.</p> <p>The amenities block would be approximately 5.5 metres in height and would be over 170 metres from the Centreline of the Midland Highway. At this distance motorists travelling at 100km/hr on the Highway would be only just register a building.</p> <p>The extent of cut and fill is limited to the extent necessary to provide for flat parking surfaces.</p> <p>Screening is proposed along the boundary of the property facing the highway. A planting schedule is shown on the plans. This screen will limit the visibility of the amenities block from the highway.</p> <p>The impact on views to and from the highway are considered to be minor and would be outweighed by the benefit provided by public amenities and a compliant truck rest stop.</p>
--	--

Table 4: Assessment of the development application against the scenic road corridor

13.2. The development application complies with the provisions of the Scenic Protection Code.





14. SECTION 34 - ECONOMIC IMPACT

- 14.1. Section 34(2) of the *Land Use Planning and Approvals Act 1993* (the Act) requires that any proposed amendment to a Local Provision Schedule (LPS) demonstrate that it is of a significant social, economic or environmental benefit. The provision is reproduced below, and comments follow.

Section 32(4) An LPS may only include a provision referred to in [subsection \(3\)](#) in relation to an area of land if –

a use or development to which the provision relates is of significant social, economic or environmental benefit to the State, a region or a municipal area; or

- 14.2. The Midland Highway is the critical north south infrastructure corridor which provides the only transport options for light vehicles, passengers and heavy vehicles between Hobart and Launceston.
- 14.3. The SSQ would substantially improve the efficiency of the Highway by removing an 87km stretch of the Highway between Campbell Town and Kempton where there are no heavy vehicle fuel sales and no 24hour fuel sales to light passenger vehicles.
- Campbell Town to Oatlands is 47km.
 - Kempton to Oatlands is 40km.
- 14.4. 87km is a significant stretch of highway to traverse without any easily accessible fuel or truck stops.
- 14.5. Heavy vehicle operators are required to take mandatory rest stops and breaks as per the National Heavy Vehicle Regulator as shown in Table 5. This truck stop would greatly assist in enabling operators to meet these mandatory requirements, particularly as they would be offered by a fuel supplier other than Ampol.





TIME	WORK	REST
In any period of...	A driver must not work for more than a maximum of...	And must have the rest of that period off work with at least a minimum rest break of...
6 ¼ hours	6 hours work time	15 continuous minutes rest time
9 hours	8 ½ hours work time	30 minutes rest time in blocks of 15 continuous minutes
12 hours	11 hours work time	60 minutes rest time in blocks of 15 continuous minutes
24 hours	14 hours work time	7 continuous hours stationary rest time*
7 days	36 hours long/night work time**	No limit has been set
14 days	144 hours work time	24 continuous hours stationary rest time taken after no more than 84 hours work time and 24 continuous hours stationary rest time and 2 x night rest breaks# and 2 x night rest breaks taken on consecutive days.

Table 5: Basic Fatigue Management Mandatory requirements. [Work and rest requirements | NHVR](#)

14.6. The lack of adequate facilities for heavy vehicles and continuous fuel access along this stretch of the Midland Highway raises concerns around about logistical inefficiencies and safety. Drivers must plan their journeys to ensure they do not get stranded on this fuel-scarce section of the highway. This is compounded by the absence of 24-hour fuel sales, which further restricts accessibility during nighttime hours. Establishing fuel outlets at strategic intervals along the highway would not only address these deficiencies but also enhance the overall reliability of this crucial transport corridor. Such an initiative aligns with the broader regional planning goal of supporting infrastructure improvements with demonstrable social, economic, and environmental benefits.





- 14.7. The construction of a fuel outlet would minimise vehicles running out of fuel and people finding themselves in potentially dangerous situations on the roadside possibly late at night.
- 14.8. There is virtually no competition on the Highway with the Ampol brand having a Monopoly over fuel sales. Ampol branded outlets stretch along a 150km stretch or 2/3 of the entire length of the Highway.
- 14.9. Introducing competition into the fuel market would deliver the key benefits of:
 - 14.9..1. Making fuel prices more dynamic and responsive to consumer demand.
 - 14.9..2. Lower fuel prices from competitive pressure. Competition in the fuel market drives prices down due to fuel retailers attempting to undercut one another.
 - 14.9..3. In more competitive markets fuel prices tend to follow a weekly cycle, where retailers' lower prices to gain customers then raise them again when demand stabilises.





14.10. A comparison of fuel prices at the fuel outlets along the Midland Highway (all of which are Ampol outlets) and fuel outs in Hobart and Launceston where there is competition demonstrates this to be the case. This comparison is shown in Table 6; prices are as of 30 May 2025 and are sourced from Fuel Radar.^v

Fuel Station	Location	Fuel Price (Unleaded)
Ampol	Campbell Town	173.9c/litre
Mood Food Ampol	Kempton	175.6c/litre
Ampol	Epping Forest	173.9c/litre ¹
United	Sandy Bay	157.9c/litre
Ampol	Brooker Highway	161.7c/litre
United	York Park Launceston	157.9c/litre
Tas Petroleum	Invermay	159.9c/litre

Table 6: Fuel prices at Tasmanian fuel outlets.

14.11. As can be seen in Table 6 fuel prices on the Midland Highway are 17.7c/litre or 11% higher than the cheapest fuel offered for sale in Hobart and Launceston. Based upon Tas Petroleum's 20-year experience in the Tasmanian Fuel Industry they expect it will be possible to achieve a reduction in the price a fuel on the Midland Highway by at least 10 cents per litre.

14.12. In examining the potential economic benefits and costs that would be delivered by the Proposal the following assumptions are made:

14.12..1. Existing fuel sales on Midland Highway between Kempton and Epping Forest excluding urban centred service stations in Oatlands and Ross are 2,200kl/month as shown in Table 7.^{vi}





Fuel Station	Estimated fuel sales/month
Ampol Campbell Town	450KIL
Ampol Campbell Town South	450kL
Mood Food Ampol Kempton	850kL
Ampol Epping Forest	450kL
TOTAL	2,200kL

Table 7: Estimated fuel sales from fuel outlets on the Midland Highway

14.13. 24% of vehicles on the Midland Highway are Heavy Vehicles.

14.14. 60% of total fuel sold is diesel and the remaining petrol.

14.14..1. Half of the diesel is sold to freight companies who have contracts with existing suppliers, this would not change.

14.14..2. The new fuel outlet would sell approximately 1000kL/month.

14.14..3. Introducing competition in the Midland Highway fuel market would impact 18% of existing fuel sales^{vii} This would result in a reduction in fuel sales across the three existing outlets of 396kL/month. The Proposal would impact fuel sales from the Tas Petroleum outlets at Kingsmeadows, Westbury, Brighton, Cambridge. Tas Petroleum sells to heavy vehicle fleet operators under contract from these locations.

14.14..4. Total fuel sales when the outlet is operating would be 2,804kL/month.

14.14..5. Based upon existing fuel pricing, introducing competition would provide for a reduction in fuel prices on the Midland Highway by 10 cents per litre. This reduction would be felt across diesel and petrol.

14.14..6. Fuel price reductions of 10 cents per litre would be felt equally across diesel and petrol sales.





14.14..7. Table 7 shows the expected savings to customers purchasing fuel on the Midland Highway.

Fuel savings	Total Fuel Sold to be impacted	Saving to motorists/month	Savings to motorists/year
10 cents per litre	2,804kL/month	\$280,400	\$3.37 million

Table 7: Estimated fuel sales from fuel outlets on the Midland Highway

14.14..8. Economic benefits during construction are estimated by be approximately \$4.5 - \$5million.

14.14..9. Employment – 20 direct jobs would be created during construction of the fuel outlet.

14.14..10.The outlet would be unstaffed for most of the time; however, one full time employee would always be available the outlet is operating. This is comparable to a typical fuel outlet which has one staff member on site when operating. Between one and two staff would be required to be onsite during operating hours.

14.14..11.Non direct economic Benefits

14.14..12.Convenience – having more fuel outlets along the Midland Highway provides customers and vehicle operators with greater convenience and opportunities for refuelling.

14.14..13.Safety – having a 24-hour 7 day a week fuel outlet would reduce the likelihood of people running out of petrol on the Midland Highway, particularly at night.

14.14..14.Efficiency – another fuel outlet would improve the efficiency of the road network by providing critical infrastructure (fuel outlets)

14.14..15.Visitation – the proposed turnoff to the fuel outlet on Interlaken Road is immediately adjacent to Oatlands and it is expected that some customers would visit Oatlands.





14.14..16.Environmental - electric vehicle charging stations have become crucial components of modern transport corridors. Integrating these facilities within the Midland Highway would not only addresses the absence of 24-hour fuel availability but also aligns with global trends toward decarbonization and sustainable mobility solutions. The outlet would promote economic growth while reducing our reliance on fossil fuels. Strategic placement of these stations as proposed would ensure that both individual travellers and freight operators benefit from improved accessibility and efficiency, fostering resilience in Tasmania's transport network.

14.15. Costs

14.15..1. Competitors – there are three other fuel outlets on the Midland Highway that would be impacted by the Proposal. It is estimated that introducing competition into this section of the fuel market would reduce fuel sales by a maximum of 18%. Diesel sales are as unlikely to as effected, as many of their existing heavy vehicle fleet customers would continue to source fuel from these outlets.

14.15..2. There is an existing BP Fuel Outlet in Oatlands as shown in Figure 20, which sells fuel via two petrol bowsers, a limited retail offering and services vehicles. Its customers are principally local from Oatlands and surrounding farmers and landowners. Like most small service stations, it relies upon motor vehicle repair and retail as its main income source. Customers of the Proposal would have been very unlikely to have turned off the highway specifically to purchase fuel from this outlet. As the Proposal would not offer any retail or vehicle maintenance and repair it would not compete with the BP within Oatlands. No impact upon the existing BP outlet in Oatlands would occur because of the Proposal.





Figure 20: Image of BP service Station at Oatlands source: Google Streetview

14.16. The Proposal would improve the efficiency of the existing road network and improve services for users of the highway. From a strategic planning perspective, it is always preferable to maximise the efficiency of existing infrastructure.

14.17. Social benefits would be delivered through the convenience of having an additional outlet on the Midland Highway. This combined with cheaper fuel prices would benefit all people using the highway.

14.18. Getting vehicles as quickly, safely and efficiently to their destination would deliver environmental benefits by have vehicle on the highway for shorter periods.

(a) the area of land has particular environmental, economic, social or spatial qualities that require provisions, that are unique to the area of land, to apply to the land in substitution for, or in addition to, or modification of, the provisions of the SPPs.



- 14.19. The Site is the most conveniently located site on the Midland Highway to provide for a fuel outlet such as proposed.
- 14.20. It is equidistant between Campbell Town and Kempton.
- 14.21. There is an existing intersection with a right turn and left turn slot off the Midland Highway providing a safe and efficient access to the Proposal. State Growth is supportive of the Proposal.
- 14.22. There are no other sites as conveniently located equidistant between Campbell Town and Kempton with an existing access as the Interlaken Road Site.
- 14.23. No environmental constraints exist which would prevent the Site being operated for a vehicle fuel outlet.





15. PLANNING SCHEME ASSESSMENT

15.1. The Property is zoned Rural under the Scheme.

15.2. Use Categorisation

15.3. The proposed use would be best categorised under Table 6.2 of the Scheme as:

15.4. **Vehicle Fuel Sales and Service** use of land primarily for the sale of motor vehicle fuel and lubricants, and if the land is so used, the use may include the routine maintenance of vehicles. An example is a service station.

15.5. This use is prohibited within the Rural Zone, hence the reason for the SSQ.

15.6. If the SSQ is approved and inserted into the Scheme the Proposal would need to be assessed against the relevant provisions of the Scheme. The relevant provisions are highlighted in italics and comments follow.

15.7. *20.3 Use Standards 20.3.1 Discretionary use.*

15.8. *The Acceptable Solution is:*

A1

A use listed as Discretionary, excluding Residential, is for an alteration or extension to an existing use, if:

- (a) the gross floor area does not increase by more than 30% from that existing at the effective date; and
- (b) the development area does not increase by more than 30% from that existing at the effective date.





15.9. The Proposal cannot comply with the acceptable solution and must be assessed against the performance criteria. It must be noted however that the entire basis for the SSQ is to provide for a Vehicle Fuel Sales and Service facility. Any approval of the SSQ would entail a strategic planning assessment on whether a vehicle fuel sales and service outlet is appropriate in on the site.

P1

A use listed as Discretionary, excluding Residential, must require a rural location for operational reasons, having regard to:

(a) the nature, scale and intensity of the use;

15.10. The proposed use would be immediately accessible from the Midland Highway and its scale and intensity would be equivalent with the infrastructure necessary to efficiently operate a highway by supplying fuel, energy and electricity to vehicles including cars and trucks.

15.11. It could reasonably be expected that a fuel outlet would be located adjacent to the highway. At 1.7ha the site is relatively small and does not have sufficient area to undertake any reasonable or economically viable agricultural activities.

(b) the importance or significance of the proposed use for the local community;

15.12. The provision of fuel outlets conveniently located along the Midland Highway is crucial to provide for the needs of the local community.

15.13. There is currently a gap of 87km between fuel outlets on the Midland Highway between Kempton and Oatlands. There are fuel outlets at Ross and Oatlands, however these are not easily accessible and require drivers to divert substantially from the highway.





15.14. As the fuel outlet would not provide retail services or grocery items it would not compete with or impact upon the retail hierarchy of the surrounding area of Oatlands.

(c) whether the use supports an existing agricultural use;

15.15. The SSQ is proposed to provide for the development of a fuel services outlet at this location. A fuel outlet providing fuel 24 hours 7 days a week would improve convenience to surrounding agricultural uses.

15.16. As considered in the economic impact assessment, improving competition for fuel supplies on the Midland Highway is expected to place downward pressure upon fuel prices with Tas Petroleum intending to offer diesel and unleaded 10 cents less than their existing competitors at Kempton, Campbell Town and Epping Forest.

(d) whether the use requires close proximity to infrastructure or natural resources; and

15.17. The site is immediately accessible from the Midland Highway and would provide fuel and electricity to vehicles, in particular trucks.

15.18. The Traffic Impact Assessment included with this submission demonstrates that the existing road network and Interlaken Road are appropriate and safe for the proposed use.

15.19. For the Midland Highway to operate as effectively and efficiently as possible fuel outlets for drivers using it should be located as close to and as easily accessible to the highway as possible.

(e) whether the use requires separation from other uses to minimise impacts.

15.20. There are no other uses within the surrounding area that would be impacted by the Proposal. Impacts from vehicles including noise and other emissions are already felt by the surrounding area through the operating of the highway.





- 15.21. **C2.0 Parking and Sustainable Transport Code** – The relevant provisions of this code are addressed in the Traffic Impact Assessment prepared by Keith Midson.
- 15.22. **C3.0 Road and Railway Assets Code** The relevant provisions of this code are addressed in the Traffic Impact Assessment prepared by Keith Midson.
- 15.23. **C7.0 Waterway and Coastal Protection Code**
- 15.24. Table 8 provides a planning response to the matters raised by the Code.





P1.1 Buildings and works within a waterway and coastal protection area must avoid or minimise adverse impacts on natural assets, having regard to:	Response
(a) impacts caused by erosion, siltation, sedimentation and runoff;	A Stormwater Management Plan and erosion Management Plan would be prepared and submitted to Council prior to construction commencing.
(b) impacts on riparian or littoral vegetation;	The site contains no vegetation.
(c) maintaining natural streambank and streambed condition, where it exists;	No streams are on the site and accordingly no impact upon any streambeds would result from the Proposal.
(d) impacts on in-stream natural habitat, such as fallen logs, bank overhangs, rocks and trailing vegetation;	No streams are present on the Property.
(e) the need to avoid significantly impeding natural flow and drainage;	There is no natural flow of water across the site. All stormwater generated from hardpaved surfaces will be directed to appropriate onsite detention basin/s.
(f) the need to maintain fish passage, where known to exist;	N/A
(g) the need to avoid land filling of wetlands;	No wetlands are on the site.
(h) the need to group new facilities with existing facilities, where reasonably practical; (i) minimising cut and fill; (j) building design that responds to the particular size, shape, contours or slope of the land;	The site is adjacent to the Midland Highway and provides for an easily accessible location of the fuel outlet such as proposed.





(k) minimising impacts on coastal processes, including sand movement and wave action;	The site is not near the coast.
(l) minimising the need for future works for the protection of natural assets, infrastructure and property;	N/A
(m) the environmental best practice guidelines in the Wetlands and Waterways Works Manual; and	The stormwater management plan and the erosion management control plan would be prepared in accordance with this document.
(n) the guidelines in the Tasmanian Coastal Works Manual	No works on the coast are proposed.

Table 8: Response to matters raised by the Code.

15.25. The exact location of the Septic Tank will be determined during detailed engineering designs, and the entire wastewater system will be designed by a suitably qualified practitioner. No designs have yet been undertaken as no Planning Permit has been issued outlining any of Council's requirements that must be met prior to the Fuel Outlet being operational.

15.26. Once a planning permit has been issued an application for a Special Plumbing Permit will be made to Council.

15.27. Although no designs have yet been prepared the Septic Tank and absorption areas will be located outside the Waterway and Coastal Protection Area.



16. C13.0 BUSHFIRE-PRONE AREAS CODE

- 16.1. This code is addressed in the Bushfire Hazard Assessment and the Emergency Management Plan prepared by Scott Livingston,
- 16.2. The Emergency Management Strategy has been endorsed by Alex Duncan from TAS Fire.
- 16.3. As the Fuel Sales Outlet will have a fuel storage capacity that exceeds manifest quantities is therefore a Hazardous Use under the Bushfire Code C13.5.2. Fuel tanks will have a combined capacity of 223,200L. The site will have 8 bowsers, under a canopy, a control room and amenities building and 3 EVC charging bays.
- 16.4. The Bushfire Hazard Management Report: Hazardous Use found the following with respect to Bushfire and Hazardous Use.

A hazard management area for BAL 12.5 rating of the bowsers, fuel tanks is well inside the proposed hardstand area and no additional fuel management is required. The hardstand provides compliant access. A static water supply will be required prior to storage of fuel in excess of manifest quantities unless a hydrant can be provided within 120m hose lay of all buildings and tanks.

An Emergency Plan endorsed by Tasmania Fire Service must be in place prior to storage of hazardous materials in excess of manifest quantities. The plan must be updated annually and supplied to TFS while the Bushfire Prone Areas Overlay includes the site.

17. SOU-C8.2.1 SCENIC PROTECTION CODE

- 17.1. As the building is partly located within the Scenic Road Corridor, this code needs to be considered.
- 17.2. This code would need to be considered if an alternative proposal for a fuel sales outlet on the Property was proposed.





17.3. Table 9 provides a planning response to the matters raised within the Code.

Objective	Response
a) Buildings or works within a scenic road corridor must not cause an unreasonable reduction of the scenic value of the road corridor, having regard to: (a) the topography of the site;	The topography of the site is relatively flat.
(b) proposed reflectance and colour of external finishes;	The exterior finishes of the building will be painted in low reflective neutral tones.
c) design and proposed location of the buildings or works;	Buildings and future works will appear as a piece of transport infrastructure within the road Corridor. Fuel Outlets are necessary along and within Road Corridors to provide for the efficient movement of vehicles. give off the appearance of N/A.
d) the extent of any cut or fill required.	No significant cut and fill in proposed.
e) any existing or proposed screening;	Landscaping is proposed along the boundary of the property facing toward the Midland Highway.
f) the impact on views from the road; and	The landscape when viewed from the road would still appear rural with a vital piece of transport infrastructure (a fuel outlet) within it.
g) the purpose of any management objectives identified in the relevant Local Provisions Schedule.	These management objectives are considered in Table 10 below.





Table 9: Response to relevant matters within the Scenic Protection Area Code.

17.4. SOU - Table C8.2 Scenic Road Corridors contains the relevant Management Objectives from the Local Provisions Schedule and is reproduced below and comments follow.

Management Objective	Response
(a) To minimise native vegetation clearance adjacent to the road by setting works and development back from the road.	The Property does not contain any native vegetation and accordingly no vegetation clearance would result from the Proposal.
(b) To reduce visibility of works and development through vegetation screening and natural topography.	Vegetation screening is proposed along the southern boundary of the Property to reduce the visibility of the buildings and signage. The areas to be vegetated are shown on the landscaping Plan which is included within the Plans and Elevations prepared by Prime Design.
(c) To minimise removal, clearance or modification of hedgerows, avenue plantings and other exotic plantings	No hedgerows, avenue plantings and or other exotic plantings would be impacted by the proposal as there are none on the Property.
(d) To maintain and continue the tradition of planting avenues of trees and related exotic plantings.	There is no scope on the Property to plant avenues of trees and related exotic plantings.
(e) To encourage (and implement) a long-term strategy for the ongoing replacement of avenue trees and related highway plantings.	This clause is not relevant to an assessment of the Proposal.
(f) To avoid signage that is unnecessary, excessive in size or	The signage proposed would be directly associated with fuel





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<p>otherwise unreasonably interferes with the landscape character in which they are located.</p>	<p>sales outlet and is necessary to ensure that the motorists on the Midland Highway know that there is a fuel sales outlet that they can access. Due to the distance from the highway (140 metres) its visibility within the landscape would be relatively limited. The landscape character of the area would not be significantly impacted by the proposal. It is a reasonable expectation that fuel sales outlet would have accompanying signage.</p>
--	--

Table 10: Response to the relevant Management Objectives within SOU - Table C8.2 Scenic Road Corridors.





- 17.5. A Landscaping Plan is included within the Plans and Elevations prepared by Prime Design.

18. NATURAL ASSETS CODE – WATERWAY AND COASTAL PORTOTECTION AREA

- 18.1. A Stormwater Management Report is included with this Proposal it found:

Due to the presence of the potential for hydrocarbon spills from re-fuelling activities, it is proposed to install a stormwater treatment/oil water separator device to capture hydrocarbons and prevent discharge to the receiving environment. A SPEL Purceptor or similar device is proposed as has been used in previous similar developments with sizing to be determined by SPEL (or other manufacturer) in accordance with relevant standards/guidelines. Sufficient available space is present to enable installation of varying sized systems as required.

The re-fuelling areas as well as tank fill points are to be suitably bunded to ensure containment of hydrocarbon spills, with all stormwater from bunded areas to be directed to the proposed treatment system.

- 18.2. As the Property is subject to the Code A Flood Hazard Management Report is included as a supporting document to this Proposal, it found.

The proposed development site has been identified by Statewide Flood Hazard mapping within LISTmap as being situated within areas of 1% AEP flood hazard and therefore subject to the flood-prone areas code of the Tasmanian Planning Scheme.

The proposed development consists of the construction of new hardstand areas for vehicle fuel sales including an overhead canopy and amenities/control room building. The existing site has been assessed as having a predominantly low risk to flooding of H1 hazard across the proposed development footprint.





It is recommended that the floor level for the proposed hardstand area be located above the indicated flood level of RL 390.6m AHD and that a high-level overflow be constructed for flows within the existing artificial drainage course to bypass the site.

The proposed development works are considered acceptable in regard to flood risk for the site.

19. SOUTHERN TASMANIAN REGIONAL LAND USE STRATEGY 2010-2030 (STRLUS)

19.1. STRLUS is the most comprehensive strategic planning review ever undertaken which will direct the future development of Southern Tasmania over the next five years.

19.2. Any amendments to planning schemes must be capable of satisfying the relevant provisions and policies of STRLUS. Relevant section of STRLUS and highlighted in italics and comments follow.

19.3. Section 13 of STRLUS deals with Land Use Transport and Integration

Demand for transport infrastructure is derived from the community's need to travel and to move freight. The relative location of different land uses (for example where people live in relationship to places for employment and shopping) is a significant determinant of transport demand, cost and modal choice. Improved integration of transport and land use planning is both a major challenge and critical factor in the development of efficient and liveable urban areas and becoming a more environmentally sustainable community in the face of a changing climate.

Freight movement is focused on the road network.

19.4. There is nowhere on the Midland Highway for an 87km stretch where there is 24-hour fuel available.





13.1 Maximising the efficiency of freight and public transport corridors and assets including maintaining and improving existing key public transport corridors to facilitate reliable, frequent public transport services;

19.5. The Proposal would improve the efficiency of freight movement on the Highway.

PI 2.3 *Identify, protect and manage existing and future infrastructure corridors and sites.*

19.6. The Midland Highway 10-Year Action Plan has resulted in a \$565 million investment aimed at improving road safety and infrastructure. The Australian and Tasmanian governments have jointly funded various upgrades, including road widening, new overtaking lanes, and safety barriers.

19.7. To further align with the objectives of STRLUS, the Proposal emphasizes maximizing connectivity and efficiency. It prioritizes the development of strategic infrastructure that not only supports freight movement but also enhances public transport accessibility. By addressing the current gaps, such as the absence of 24-hour fuel availability along the Midland Highway, the Proposal aims to strengthen regional transport resilience and foster a more sustainable integration of land use and transportation systems.

LUTI 1.7 *Protect major regional and urban transport corridors through planning schemes as identified in Maps 3 & 4.*

19.8. The Midland Highway is identified within STRLUS as a regional transport Corridor.

19.9. The Proposal would foster protect the Corridor as it would provide greater choice for motorists when purchasing fuel, improve accessibility to it at a reduced price. access, such as electric vehicle charging stations, could address regional gaps like the lack of 24-hour fuel availability along key stretches of the Midland Highway.





WR 1 *Protect and manage the ecological health, environmental values and water quality of surface and groundwater, including waterways, wetlands and estuaries*

- 19.10. This matter has been addressed through the provision of two separate reports which accompany the proposal. A Flood Hazard Report and a Stormwater Management Report prepared by Rare Consulting. These are included as Appendices E and F. The following summarises the findings and recommendations from these reports. The proposal would not impact upon the
- 19.11. Due to the presence of the potential for hydrocarbon spills from re-fuelling activities, it is proposed to install a stormwater treatment/oil water separator device to capture hydrocarbons and prevent discharge to the receiving environment. A SPEL Purceptor or similar device is proposed as has been used in previous similar developments with sizing to be determined by SPEL (or other manufacturer) in accordance with relevant standards/guidelines. Sufficient available space is present to enable installation of varying sized systems as required.
- 19.12. The re-fuelling areas as well as tank fill points are to be suitably bunded to ensure containment of hydrocarbon spills, with all stormwater from bunded areas to be directed to the proposed treatment system.
- 19.13. The proposed development site has been identified by Statewide Flood Hazard mapping within LISTmap as being situated within areas of 1% AEP flood hazard and therefore subject to the flood-prone areas code of the Tasmanian Planning Scheme.
- 19.14. The proposed development consists of the construction of new hardstand areas for vehicle fuel sales including an overhead canopy and amenities/control room building. The existing site has been assessed as having a predominantly low risk to flooding of H1 hazard across the proposed development footprint.
- 19.15. It is considered that the relevant section of STRLUS have been addressed within the attached reports.





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MRH 1 *Minimise the risk of loss of life and property from bushfires.*

- 19.16. An *Emergency Management Strategy* to demonstrate how risk to employees and visitors to the site will be managed to a tolerable level through contextualised emergency procedures for the proposal as prepared by Scott Livingston forms part of this application and this *Emergency Management Strategy* has been endorsed by Alex Duncan, Planning and Assessment Officer at the Tasmanian Fire Service.
- 19.17. It is considered that the detailed response contained within this report addresses and responds to MR1 of STRLUS.
- 19.18. A Bushfire Hazard Management Plan is attached to supporting document.

CV 4 *Recognise and manage significant cultural landscapes throughout the region to protect their key values.*

19.19. This clause is addressed in sections 10.26 and 10.27 of the of the Development Application and is reproduced below to satisfy clause CV4 of STRLUS.

10.26. **SOU-C8.2.1 Scenic Protection Code**

10.27. Table 9 provides a planning response to the matters raised by the Code.

Objective	Response
a) Minimise native vegetation clearance adjacent to the road by setting works and development back from the road.	There is no native vegetation on the Property which could be cleared.
b) Reduce visibility of works and development through vegetation screening and natural topography.	A landscaping plan will be prepared and provided to the satisfaction of Council as part of any development approval.
c) Minimise removal, clearance or modification of hedgerows, avenue plantings and other exotic plantings.	No hedgerows are present on the site.





d) Maintain and continue the tradition of planting avenues of trees and related exotic plantings.	A landscaping plan will be prepared and provided to the satisfaction of Council as part of any development approval.
e) Encourage (and implement) a long term strategy for the ongoing replacement of avenue trees and related highway plantings.	N/A.
f) Avoid signage that is unnecessary, excessive in size or otherwise unreasonably interferes with the landscape character in which they are located.	A signage plan will be submitted to Council if the application is approved.

Table 9: Response to relevant matters within the Scenic Protection Area Code.

19.20. Fuel Outlets and Service Stations are vital pieces of infrastructure to provide for movement of vehicles along transport corridors, and they must be located within the corridor for efficiency. It can be expected that fuel outlets would be visible within the scenic protection code area.

AC 1 *Focus employment, retail and commercial uses, community services and opportunities for social interaction in well-planned, vibrant and accessible regional activity centres that are provided with a high level of amenity and with good transport links with residential areas.*

AC 1.1 *Implement the Activity Centre Network through the delivery of retail, commercial, business, administration, social and community and passenger transport facilities.*

19.21. Oatlands would continue to be the focus for employment, retail and commercial uses, community services and opportunities for social interaction.

19.22. A necessary fuel outlet would not create a new centre which would compete with Oatlands.

19.23. No sale of any retail products outside of fuel would be afforded by the Proposal.





19.24. Locating a Fuel Outlet on the Midland Highway would assist in implementing the Activity Centre by getting people goods and services from one centre to the other in a more efficient manner.

WR 1 *Protect and manage the ecological health, environmental values and water quality of surface and groundwater, including waterways, wetlands and estuaries*

WR 1.1 *Ensure use and development is undertaken in accordance with the State Policy on Water Quality Management*

19.25. This matter has been addressed through the provision of two separate reports which accompany the proposal. A Stormwater Management Report and a Flood Hazard Report prepared by Rare Consulting, Appendices E and F. The following summarises the findings and recommendations from these reports. The proposal would not impact upon the

19.26. Due to the presence of the potential for hydrocarbon spills from re-fuelling activities, it is proposed to install a stormwater treatment/oil water separator device to capture hydrocarbons and prevent discharge to the receiving environment. A SPEL Purceptor or similar device is proposed as has been used in previous similar developments with sizing to be determined by SPEL (or other manufacturer) in accordance with relevant standards/guidelines. Sufficient available space is present to enable installation of varying sized systems as required.

19.27. The re-fuelling areas as well as tank fill points are to be suitably bunded to ensure containment of hydrocarbon spills, with all stormwater from bunded areas to be directed to the proposed treatment system.

19.28. The proposed development site has been identified by Statewide Flood Hazard mapping within LISTmap as being situated within areas of 1% AEP flood hazard and therefore subject to the flood-prone areas code of the Tasmanian Planning Scheme.





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MRH 1 *Minimise the risk of loss of life and property from bushfires.*

MRH 1.1 *Provide for the management and mitigation of bushfire risk at the earliest possible stage of the land use planning process (rezoning or if no rezoning required; subdivision) by the identification and protection (in perpetuity) of buffer distances or through the design and layout of lots.*

19.29. This is addressed in the Bushfire Hazard Management Plan contained as Appendix D.

19.30. An Emergency Management Strategy to demonstrate how risk to employees and visitors to the site will be managed to a tolerable level through contextualised emergency procedures for the proposal as prepared by Scott Livingston. This forms part of this application and this Emergency Management Strategy has been endorsed by Alex Duncan, Planning and Assessment Officer at the Tasmanian Fire Service.

19.31. It is considered that the detailed response contained within this report addresses and responds to MRH 1 of STRLUS.

CV 4 *Recognise and manage significant cultural landscapes throughout the region to protect their key values.*

CV 4.1 *State and local government, in consultation with the community, to determine an agreed set of criteria for determining the relative significance of important landscapes and key landscape values.*

CV 4.2 *Ensure the key values of regionally significant landscapes are not significantly compromised by new development through appropriate provisions within planning schemes.*

19.32. This matter is addressed in sections 14 of this report and is reproduced below to satisfy clause CV4 of STRLUS.

Objective	Response
-----------	----------



PO Box 58 South HOBART, Tasmania 7004

0438376840 evan@e3planning.com.au



Buildings or works within a scenic road corridor must not cause an unreasonable reduction of the scenic value of the road corridor, having regard to: (a) the topography of the site;	The topography of the site is relatively flat.
(b) proposed reflectance and colour of external finishes;	The exterior finishes of the building will be painted in low reflective neutral tones.
c) design and proposed location of the buildings or works;	Buildings and future works will appear as a piece of transport infrastructure within the road Corridor. Fuel Outlets are necessary along and within Road Corridors to provide for the efficient movement of vehicles. give off the appearance of N/A.
d) the extent of any cut or fill required.	No significant cut and fill in proposed.
e) any existing or proposed screening;	Landscaping is proposed along the boundary of the property facing toward the Midland Highway.
f) the impact on views from the road; and	The landscape when viewed from the road would still appear rural with a vital piece of transport infrastructure (a fuel outlet) within it.
g) the purpose of any management objectives identified in the relevant Local Provisions Schedule.	These management objectives are considered in Table 10 below.





19.33. Fuel Outlets and Service Stations are vital pieces of infrastructure to provide for movement of vehicles along transport corridors, and they must be located within the corridor for efficiency. It can be expected that fuel outlets would be visible within the scenic protection code area.

AC 1 *Focus employment, retail and commercial uses, community services and opportunities for social interaction in well-planned, vibrant and accessible regional activity centres that are provided with a high level of amenity and with good transport links with residential areas.*

19.34. Oatlands would continue to be the focus for employment, retail and commercial uses, community services and opportunities for social interaction.

19.35. A necessary fuel outlet would not create a new centre which would compete with Oatlands.

19.36. No sale of any retail products outside of fuel would be afforded by the Proposal.

19.37. Locating a Fuel Outlet on the Midland Highway would assist in implementing the Activity Centre by getting people goods and services from one centre to the other in a more efficient manner.

19.38. 3. Section 34(2)(f) Land Use Planning & Approvals Act 1993 – 'have regard to the strategic plan.'

20. CONCLUSION

20.1. The Proposal addresses critical gaps along the Midland Highway by ensuring the availability of 24-hour fuel services and introducing infrastructure like electric vehicle charging stations.

20.2. These enhancements aim to maximize connectivity, improve the efficiency of freight and public transport corridors, and align with the strategic objectives outlined in the Southern Tasmania Regional Land Use Strategy (STRLUS).





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- 20.3. The Proposal supports regional transport resilience and facilitates sustainable integration between land use and transportation systems. By filling an 87-kilometre stretch lacking essential services, the initiative fosters economic benefits, enhances accessibility, and delivers improved choices for motorists, including fuel at reduced prices saving motorists and fleet operators up to \$3.37 million per annum.
- 20.4. If approved the SSQ and the development application would represent a significant step forward in strengthening infrastructure, safety, and sustainability within the Midland Highway Corridor.
- 20.5. It would provide a dedicated truckers lounge, toilets, shower facilities. Picnic tables and spacious rest areas for travelling families and commercial vehicle operators.



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Bibliography

Tasmanian Heavy Vehicle Driver Rest Area Strategy, State Growth, August 2020

Fuel radar accessed 26 May 2025 <https://fuelradar.com.au/>

Midland Highway 10 Year Action Plan

Southern Integrated Transport Plan 2010 Southern Tasmanian Councils Authority

Southern Tasmania Regional Land Use Strategy 2010–2035

Survey of Motor Vehicle Use, Australia Estimates of; kilometres travelled, tonne-kilometres travelled, tonnes carried and fuel use. Includes sub annual estimates. Australian Bureau of Statistics 2024

Tasmanian Greenhouse Gas Emissions Report 2024 August 2024
Renewables, Climate and Future Industries Tasmania Department of State Growth

Tasmanian Integrated Freight Strategy

Why Australia's Petrol Stations Keep Changing Names By Ally Burnie Last updated May 08, 2025 Why Australia's Petrol Stations Keep Changing Names (2025) - Fuel Card Comparison





Tasmanian Integrated Freight Strategy

Midland Highway 10 Year Action Plan

Survey of Motor Vehicle Use, Australia Estimates of; kilometres travelled, tonne-kilometres travelled, tonnes carried and fuel use. Includes sub annual estimates. Australian Bureau of Statistics 2024

Why Australia's Petrol Stations Keep Changing Names By Ally Burnie Last updated May 08, 2025 [Why Australia's Petrol Stations Keep Changing Names \(2025\) - Fuel Card Comparison](#)

Southern Tasmania Regional Land Use Strategy 2010–2035

Southern Integrated Transport Plan 2010 Southern Tasmanian Councils Authority

Tasmanian Greenhouse Gas Emissions Report 2024 August 2024 Renewables, Climate and Future Industries Tasmania Department of State Growth

Fuel radar accessed 26 May 2025 <https://fuelradar.com.au/>



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Appendix A
Property Title



PO Box 58 South HOBART, Tasmania 7004

0438376840 evan@e3planning.com.au

SEARCH OF TORRENS TITLE

VOLUME 44807	FOLIO 6
EDITION 6	DATE OF ISSUE 19-Oct-2024

SEARCH DATE : 21-May-2025
SEARCH TIME : 04.00 PM

DESCRIPTION OF LAND

Town of OATLANDS
Lot 6 on Diagram 44807
Being the land sixthly described in Conveyance No. 66/2501
Excepting thereout Part of Lot 5 (D20462)
Derivation : Part of 4A-1R-26Ps. Gtd. to T. Burbury.
Prior CT 4702/92

SCHEDULE 1

M889797 TRANSFER to C&M LIVESTOCK CONSULTING TASMANIA PTY LTD
Registered 20-Jun-2021 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
66/4538 GRANT OF EASEMENT - BURDENING EASEMENT: Right of
Drainage (appurtenant to The Crown) over the land
marked Drainage Easement shown passing through Lot 6
on Diagram No. 44807

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

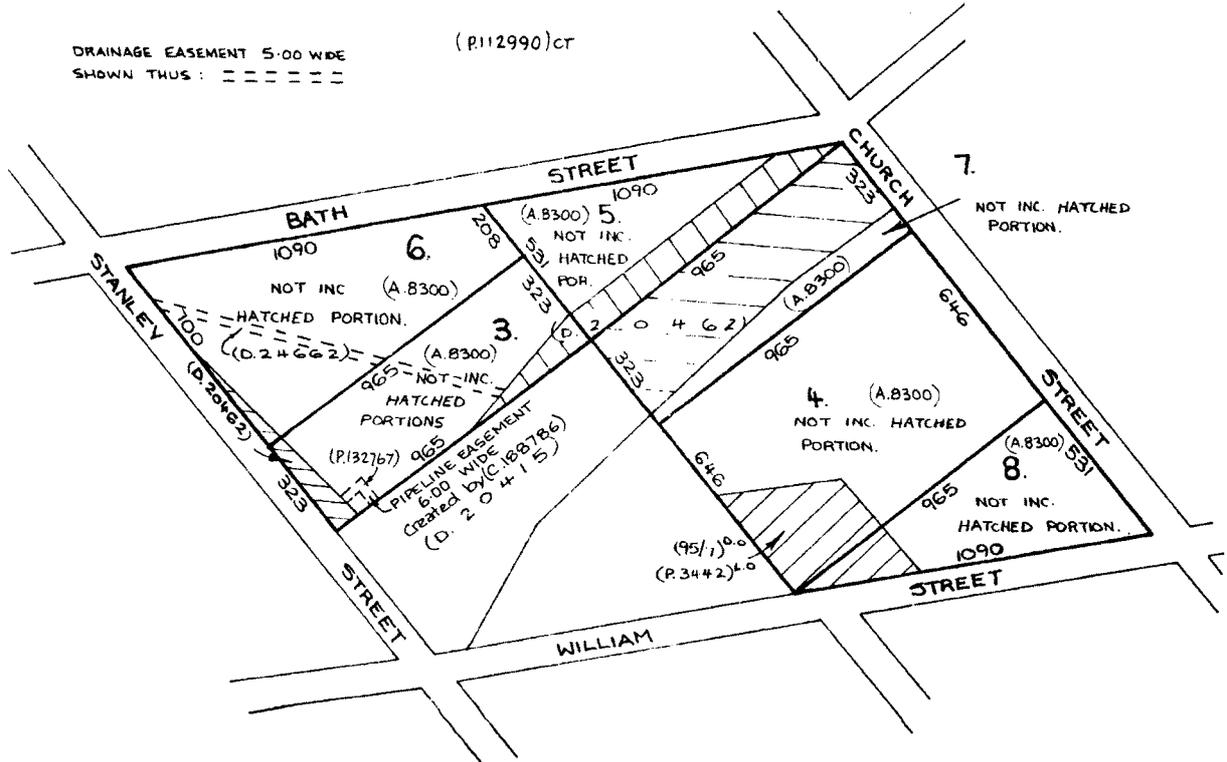
APPROVED 18 SEP 1990 <i>Michael Pina</i> RECORDER OF TITLES	CONVERSION PLAN CONVERTED FROM 66/2501 (3RDLY & 4THLY DESC)	REGISTERED NUMBER D. 44807
FILE NUMBER A 8300	GRANTEE SEE REVERSE SIDE FOR GRANTEES.	DRAWN <i>AH</i> 26/6/90

SKETCH BY WAY OF ILLUSTRATION ONLY

CITY/TOWN OF OATLANDS (SEC. E)
LAND DISTRICT OF
PARISH OF
LENGTHS ARE IN METRES. NOT TO SCALE.
LENGTHS IN BRACKETS IN LINKS/FEET & INCHES.

EXCEPTED LANDS

- LOT 6: PART OF LOT 5 (D.20462)
- LOT 3: PART OF LOTS 5 & 9 (D.20462)
- LOT 5: PART OF LOT 9 (D.20462)
- LOT 7: PART OF LOT 9 (D.20462)
- LOT 4: PART OF (1-1-1⁶/₁₀) (95/7) 0.0 NOT. 43/6244
- LOT 8: PART OF (1-1-1⁶/₁₀) (95/7) 0.0 NOT. 43/6244





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Appendix B

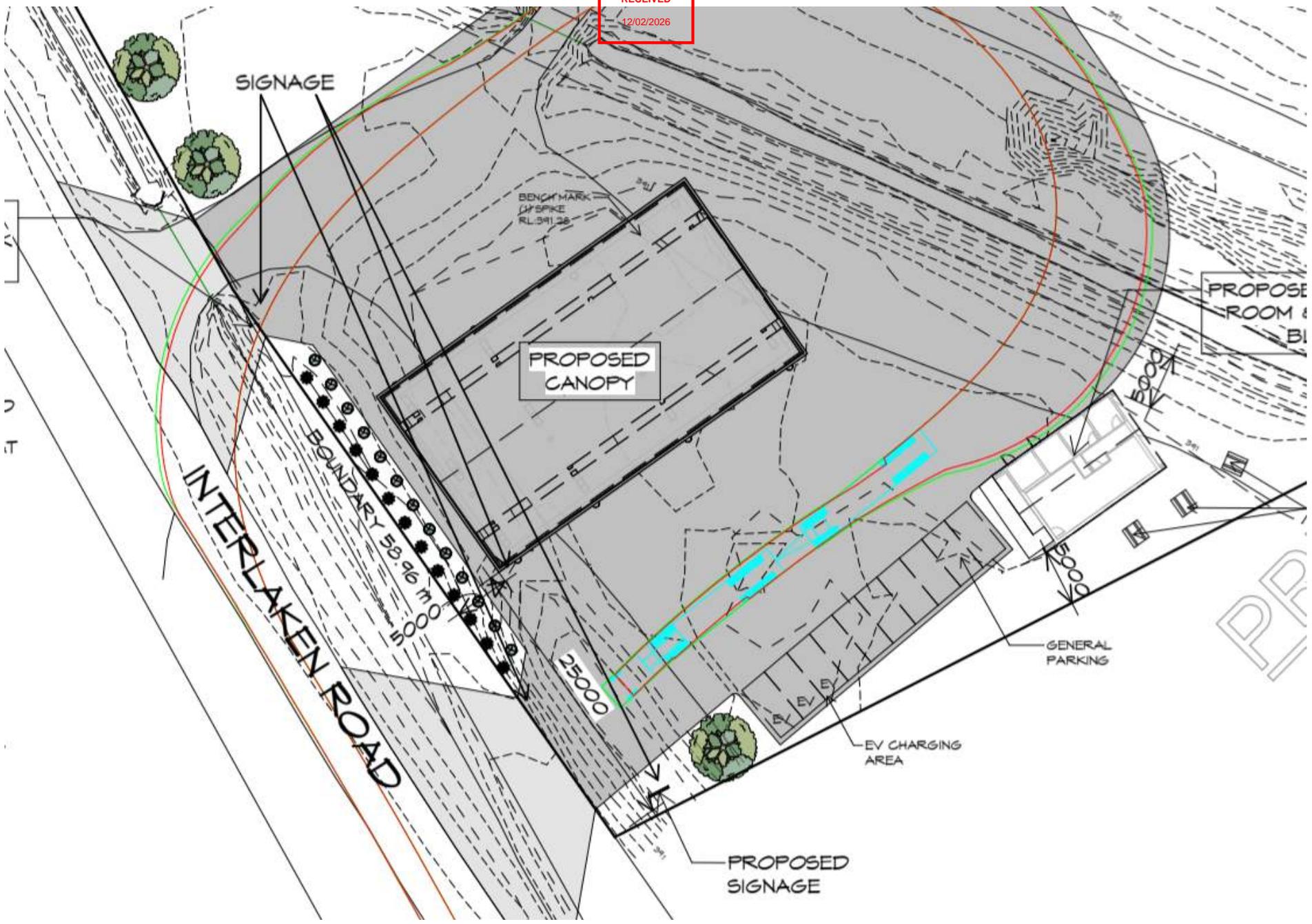
Development Application Drawings Site Plan, Elevations and Floor Plan



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0438376840 evan@e3planning.com.au

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PROPOSED FUEL STOP 20 INTERLAKEN ROAD, OATLANDS TAS PETROLEUM

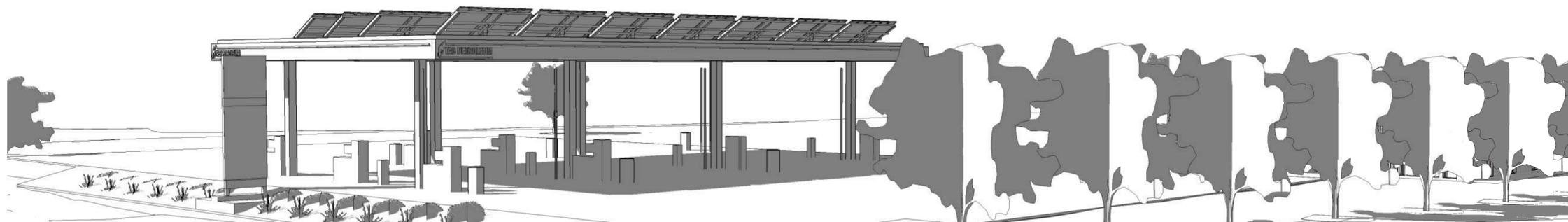
PD24266

BUILDING DRAWINGS

No	DRAWING
01	SITE PLAN
02	PART SITE PLAN
03	SITE LANDSCAPING PLAN
04	LOCALITY PLAN
05	FLOOR PLAN
06	ELEVATIONS
07	ELEVATIONS
08	ROOF PLAN

BUILDING DRAWINGS - AMENITIES

No	DRAWING
A01	FLOOR PLAN



PLANNING



10 Goodman Court , Invermay Launceston 7248
p() +03 6332 3790
Shop 9, 105-111 Main Road, Moonah Hobart 7009
p(h)+03 6228 4575
info@ primedesigntas.com.au primedesigntas.com.au
Accredited Building Practitioner: Frank Geskus -No CC246A

FEBRUARY 2026

BATH STREET (UNFORMED)

BOUNDARY 219.27 m

BOUNDARY 41.84 m

LINE OF SCENIC ROAD CORRIDOR

BENCH MARK
OLD SURVEY MARK
RL:390.306

PROPOSED
CANOPY

PROPOSED CONTROL
ROOM & AMENITIES
BLOCK

BOUNDARY 136.56 m

DRAINAGE EASEMENT
5.00 m WIDE

INTERLAKEN ROAD

BOUNDARY 32.41 m

BOUNDARY 32.96 m

24000

EV CHARGING
AREA

GENERAL
PARKING

PARK
BENCHES



PLANNING
NOTE: DO NOT SCALE OFF DRAWINGS

SITE PLAN

1 : 500

SURVEY NOTES

1. THIS PLAN HAS BEEN PREPARED BY SURVEY PLUS FROM A COMBINATION OF EXISTING RECORDS AND FIELD SURVEY FOR THE PURPOSES OF SHOWING THE PHYSICAL FEATURES OF THE LAND AND SHOULD NOT BE USED FOR ANY OTHER PURPOSE.
2. TITLE BOUNDARIES SHOWN WERE NOT VERIFIED BY SURVEY PLUS AT THE TIME OF THIS SURVEY.
3. SERVICES SHOWN ON THIS PLAN WERE LOCATED WHERE POSSIBLE BY FIELD SURVEY. THEY ARE NOT A COMPLETE PICTURE OF SERVICES ON SITE. ALL SERVICE LOCATIONS ARE TO BE VERIFIED BEFORE COMMENCEMENT OF ANY WORK ON SITE, IN PARTICULAR THOSE SERVICES NOT PREVIOUSLY LOCATED THROUGH FIELD SURVEY.
4. SURVEY PLUS CAN NOT ACCEPT LIABILITY WHATSOEVER FOR LOSS OR DAMAGE CAUSED TO ANY UNDERGROUND SERVICE WHETHER SHOWN BY OUR SURVEY OR NOT.
5. THIS NOTE IS AN INTEGRAL PART OF THIS PLAN/DATA. REPRODUCTION OF THIS PLAN OR ANY PART OF IT WITHOUT THIS NOTE BEING INCLUDED IN FULL WILL RENDER THE INFORMATION SHOWN ON SUCH A REPRODUCTION INVALID AND NOT SUITABLE FOR USE WITHOUT PRIOR AUTHORITY OF SURVEY PLUS.
6. HORIZONTAL DATUM IS MGA (GDA94).
7. VERTICAL DATUM IS AHD.
8. CONTOUR INTERVAL IS 0.2 METRE, INDEX IS 1.0 METRE.
9. SURVEY BY ROBOTIC TOTAL STATION AND GPS.
10. DUE TO THE AGE OF TITLE SURVEY IF ANY CONSTRUCTION WORKS ARE TO BE UNDERTAKEN ON OR NEAR THE TITLE BOUNDARY OR PRESCRIBED SETBACKS A RE-MARK SURVEY BY A REGISTERED LAND SURVEYOR WILL BE REQUIRED.
11. IMPORTED DATA SHOWN ON THIS PLAN WAS OBTAINED FOR PUBLIC AVAILABLE DATA FROM VARIOUS GOVERNMENT AUTHORITIES. THIS INFORMATION IS PROVIDED FOR GUIDANCE ONLY. THE ACCURACY OF ANY IMPORTED DATA IS PER THE ACCURACY QUOTED BY THE SOURCE AND IS IN NO WAY GUARANTEED BY SURVEY PLUS. USERS MUST NOT RELY ON THIS DATA FOR ON-GROUND LOCATION OF BOUNDARIES AND/OR SERVICES.
 - LIST DATA IMPORT
 - TasWater-SewerLateralLine
 - TasWater-SewerMain
 - TasWater-SewerMaintenanceHole
 - TasWater-SewerPressurisedMain
 - TasWater-WaterHydrant
 - TasWater-WaterLateralLine
 - TasWater-WaterMain
12. 3D DATA TURNED OFF IN LAYER CONTROL.
 - 3D TIN
 - MAJOR CONTOUR 3D
 - MINOR CONTOUR 3D

GENERAL NOTES

- CHECK & VERIFY ALL DIMENSIONS & LEVELS ON SITE
- WRITTEN DIMENSIONS TO TAKE PREFERENCE OVER SCALED
- ALL WORK TO BE STRICTLY IN ACCORDANCE WITH NCC 2022, ALL S.A.A. CODES & LOCAL AUTHORITY BY-LAWS
- ALL DIMENSIONS INDICATED ARE FRAME TO FRAME AND DO NOT ALLOW FOR WALL LININGS
- CONFIRM ALL FLOOR AREAS
- ALL PLUMBING WORKS TO BE STRICTLY IN ACCORDANCE WITH A.S. 3500, NCC 2022 & APPROVED BY COUNCIL INSPECTOR
- BUILDER/PLUMBER TO ENSURE ADEQUATE FALL TO SITE CONNECTION POINTS IN ACCORDANCE WITH A.S. 3500 FOR STORMWATER AND SEWER BEFORE CONSTRUCTION COMMENCES
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE ENGINEER'S STRUCTURAL DRAWINGS
- ALL WINDOWS AND GLAZING TO COMPLY WITH A.S. 1288 & A.S. 2047
- ALL SET OUT OF BUILDINGS & STRUCTURES TO BE CARRIED OUT BY A REGISTERED LAND SURVEYOR AND CHECKED PRIOR TO CONSTRUCTION
- IF CONSTRUCTION OF THE DESIGN IN THIS SET OF DRAWINGS DIFFER FROM THE DESIGN AND DETAIL IN THESE AND ANY ASSOCIATED DOCUMENTS BUILDER AND OWNER ARE TO NOTIFY DESIGNER
- BUILDER'S RESPONSIBILITY TO COMPLY WITH ALL PLANNING CONDITIONS
- BUILDER TO HAVE STAMPED BUILDING APPROVAL DRAWINGS AND PERMITS PRIOR TO COMMENCEMENT OF CONSTRUCTION
- CONSTRUCTION TO COMPLY WITH AS 3959, READ IN CONJUNCTION WITH BUSHFIRE ATTACK LEVEL (BAL) ASSESSMENT REPORT.



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Project:
**PROPOSED FUEL STOP
20 INTERLAKEN ROAD,
OATLANDS**

Client name:
TAS PETROLEUM

Drawing:
SITE PLAN

Drafted by: D.D.H. Approved by: Approver

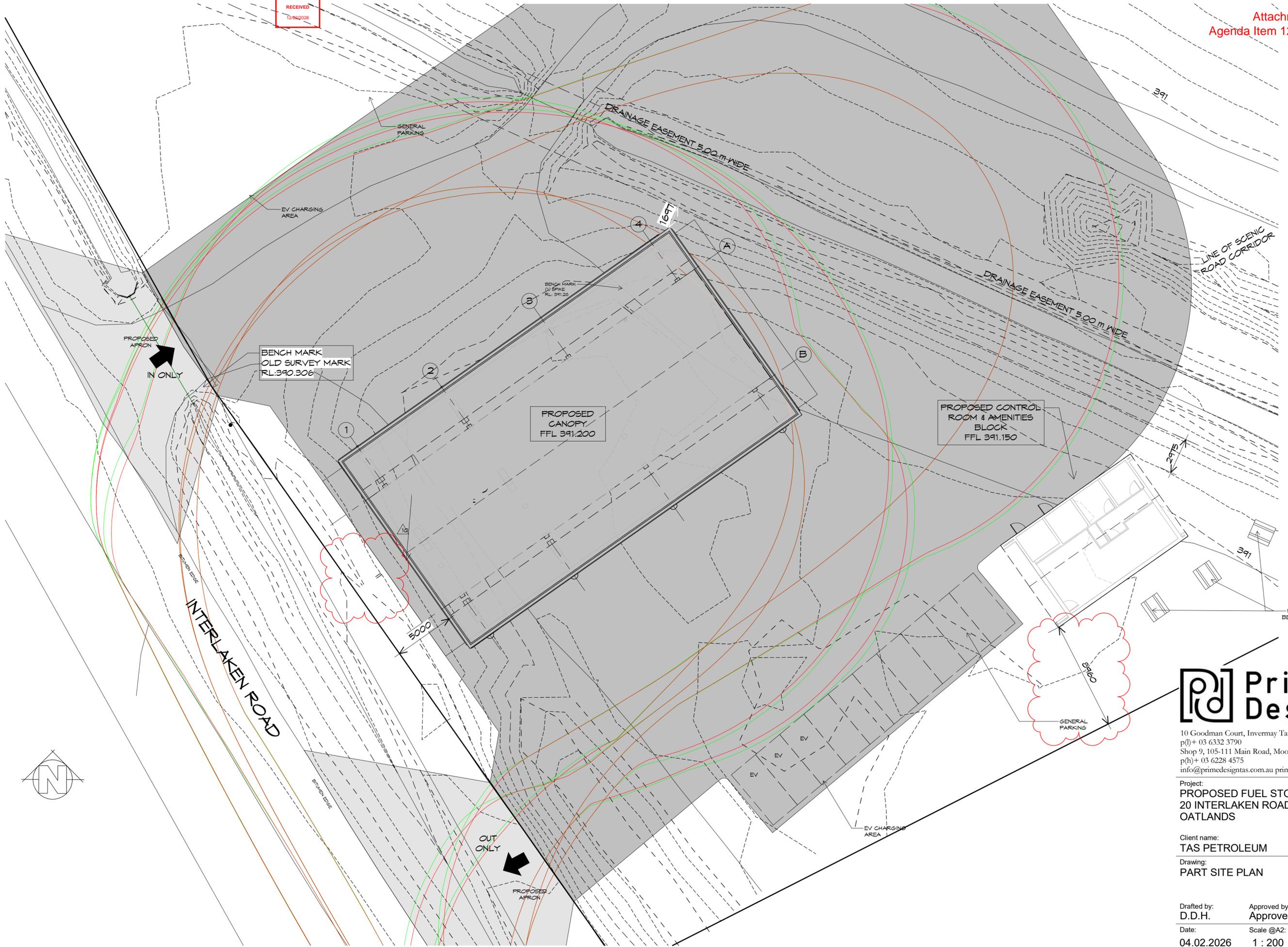
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Project:
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20 INTERLAKEN ROAD,
OATLANDS**

Client name:
TAS PETROLEUM

Drawing:
PART SITE PLAN

Drafted by: D.D.H. Approved by: Approver

Date: 04.02.2026 Scale @A2: 1 : 200

Project/Drawing no: PD24266 -02 Revision: 10



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PART SITE PLAN

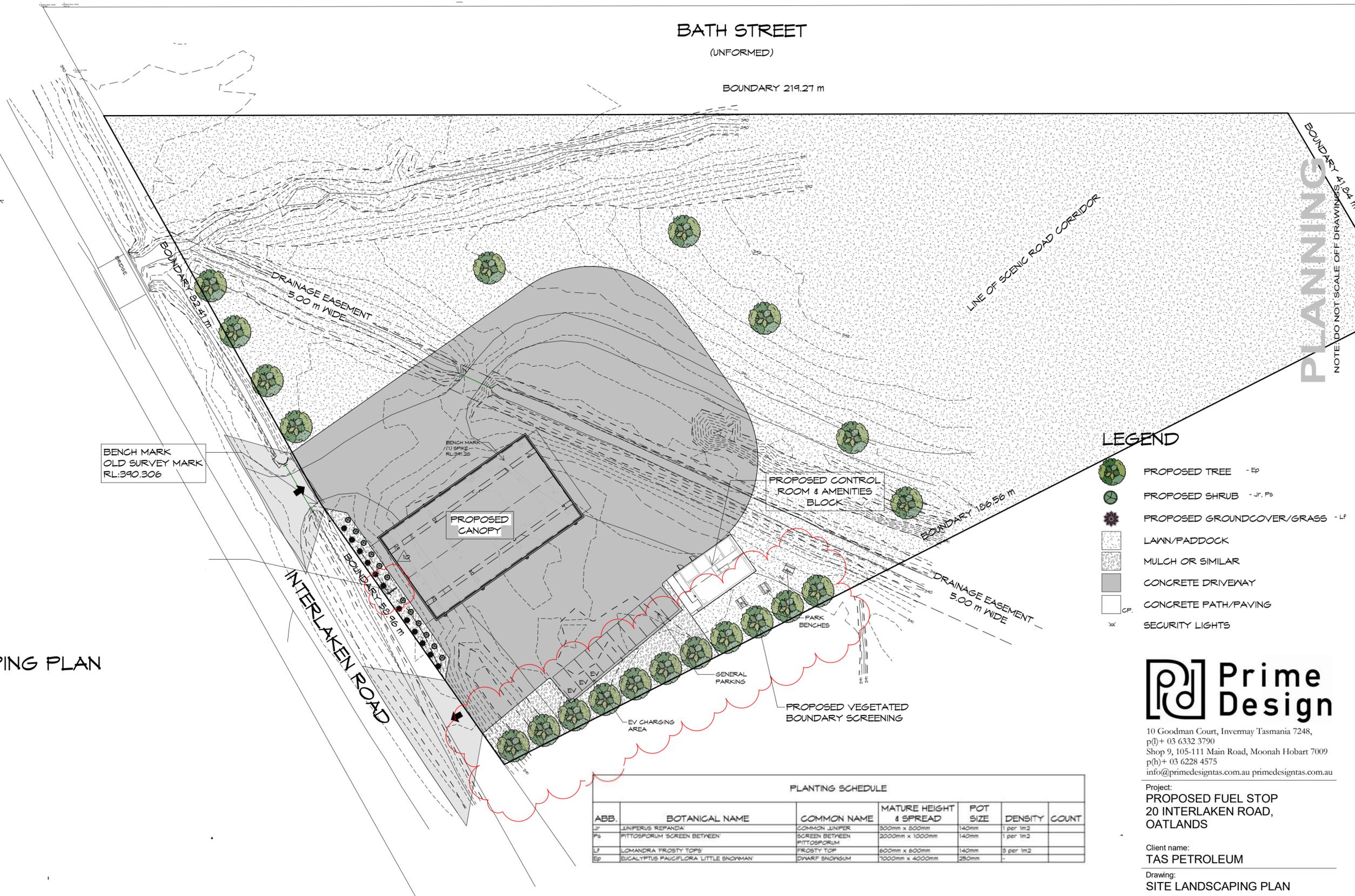
1 : 200



BATH STREET
(UNFORMED)

BOUNDARY 219.27 m

BOUNDARY 41.94 m
PLANNING
NOTE: DO NOT SCALE OFF DRAWINGS



BENCH MARK
OLD SURVEY MARK
RL:390.306

PROPOSED CONTROL
ROOM & AMENITIES
BLOCK

PROPOSED
CANOPY

LEGEND

- PROPOSED TREE - Ep
- PROPOSED SHRUB - Jr, Ps
- PROPOSED GROUND COVER/GRASS - Lf
- LAWN/PADDOCK
- MULCH OR SIMILAR
- CONCRETE DRIVEWAY
- CONCRETE PATH/PAVING
- SECURITY LIGHTS

SITE LANDSCAPING PLAN

1:500

PLANTING SCHEDULE

ABB.	BOTANICAL NAME	COMMON NAME	MATURE HEIGHT & SPREAD	POT SIZE	DENSITY	COUNT
Jr	JUNIFERUS REPANDA	COMMON JUNIFER	300mm x 300mm	140mm	1 per 1m ²	
Ps	PITTOSPORUM SCREEN BETWEEN	SCREEN BETWEEN	2000mm x 1000mm	140mm	1 per 1m ²	
Lf	LOMANDRA FROSTY TOPS	FROSTY TOP	600mm x 600mm	140mm	3 per 1m ²	
Ep	EUCALYPTUS PAUCIFLORA LITTLE SNOWMAN	DWARF SNOWSUM	1000mm x 4000mm	250mm	-	



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OATLANDS**

Client name:
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Drawing:
SITE LANDSCAPING PLAN

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Date: 04.02.2026 Scale @A2: As indicated

Project/Drawing no: PD24266 -03 Revision: 10



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LOCALITY PLAN

1 : 2000

THIS SITE IS ZONED RURAL AND REQUIRES A BUSHFIRE ASSESSMENT.
DEVELOPMENT IS NOT OVER 100m FROM UNMANAGED BUSH/GRASSLANDS GREATER THAN 1 HECTARE.

REFER TO BUSHFIRE ASSESSMENT REPORT FOR MANAGEMENT PLAN



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Project:
**PROPOSED FUEL STOP
 20 INTERLAKEN ROAD,
 OATLANDS**

Client name:
TAS PETROLEUM

Drawing:
LOCALITY PLAN

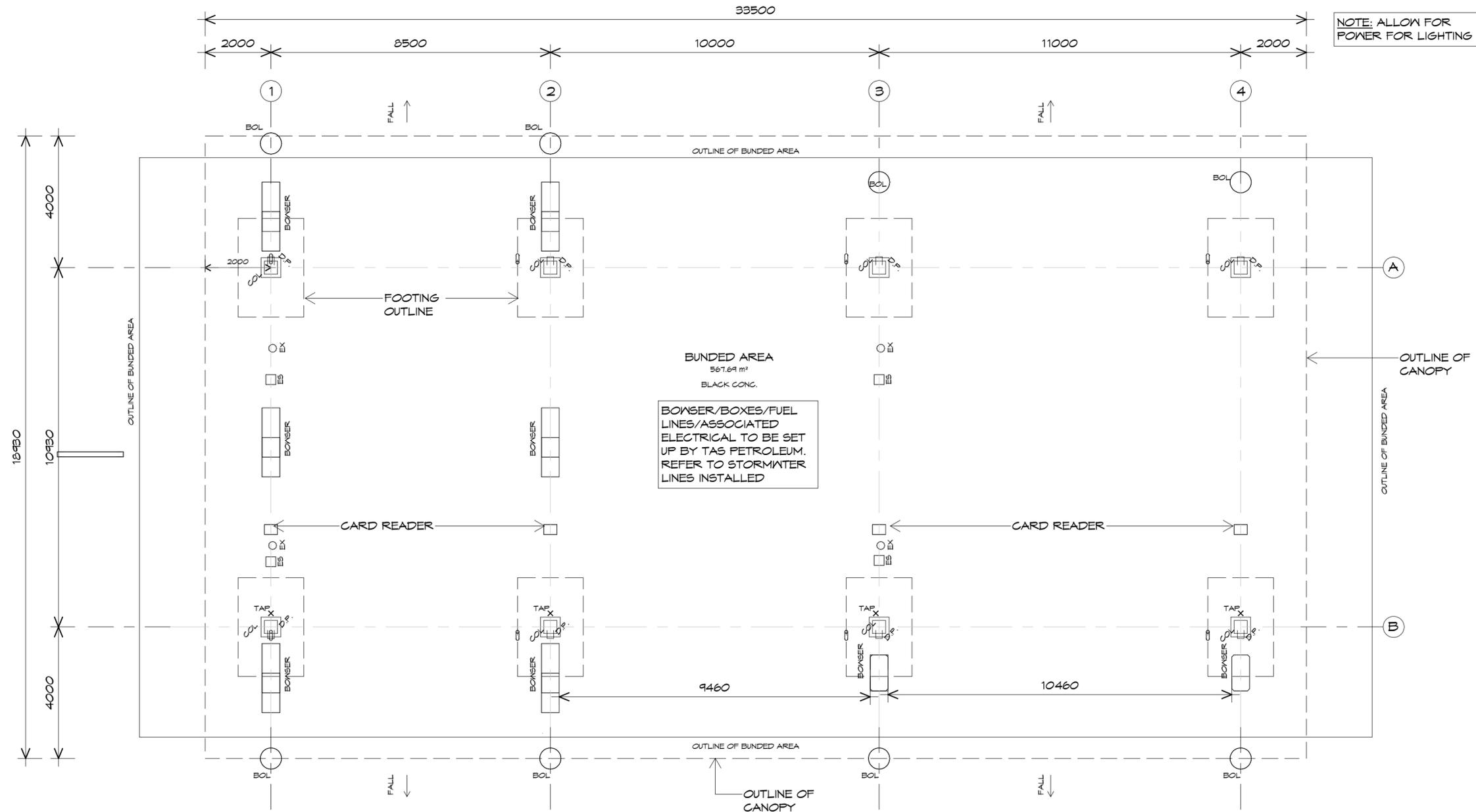
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Date: 04.02.2026 Scale @A2: 1 : 2000

Project/Drawing no: PD24266 -04 Revision: 10



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CANOPY FLOOR PLAN

1 : 100

CANOPY AREA 634.15 m2 (68.26 SQUARES)

PLANNING

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Client name:
TAS PETROLEUM

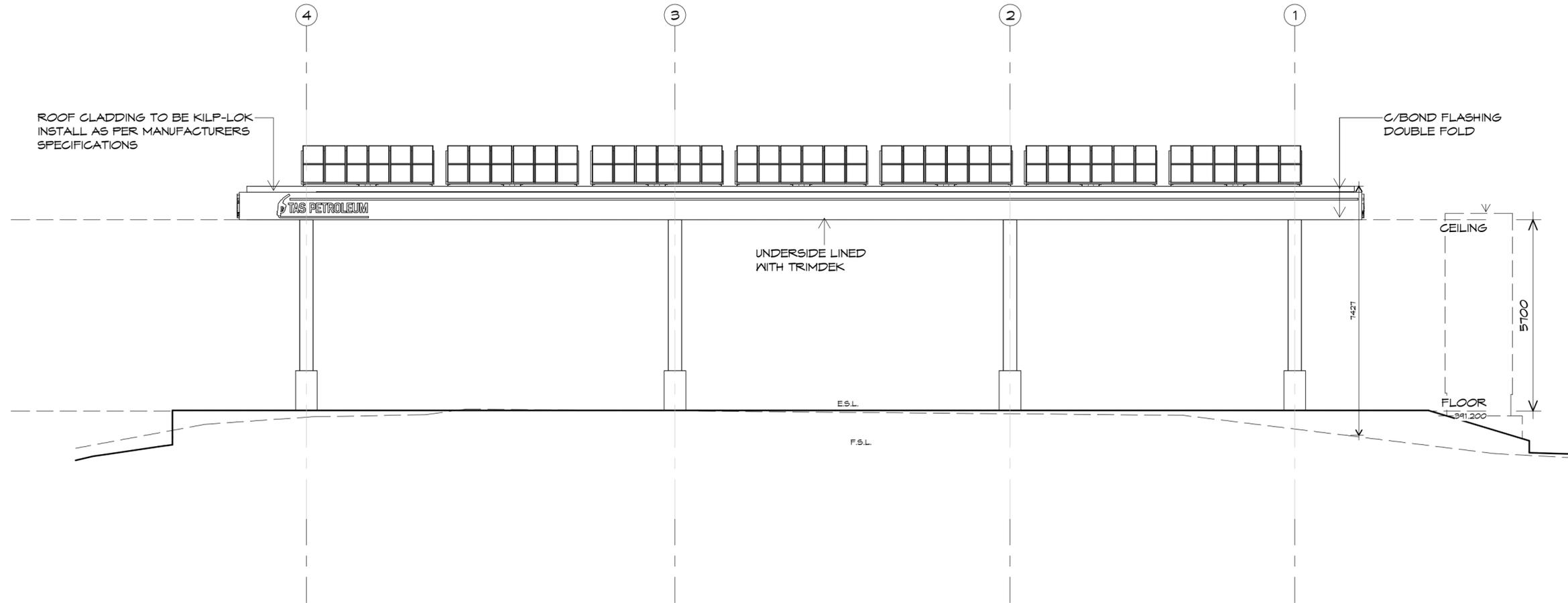
Drawing:
FLOOR PLAN

Drafted by: D.D.H. Approved by: Approver
Date: 04.02.2026 Scale @A2: 1 : 100

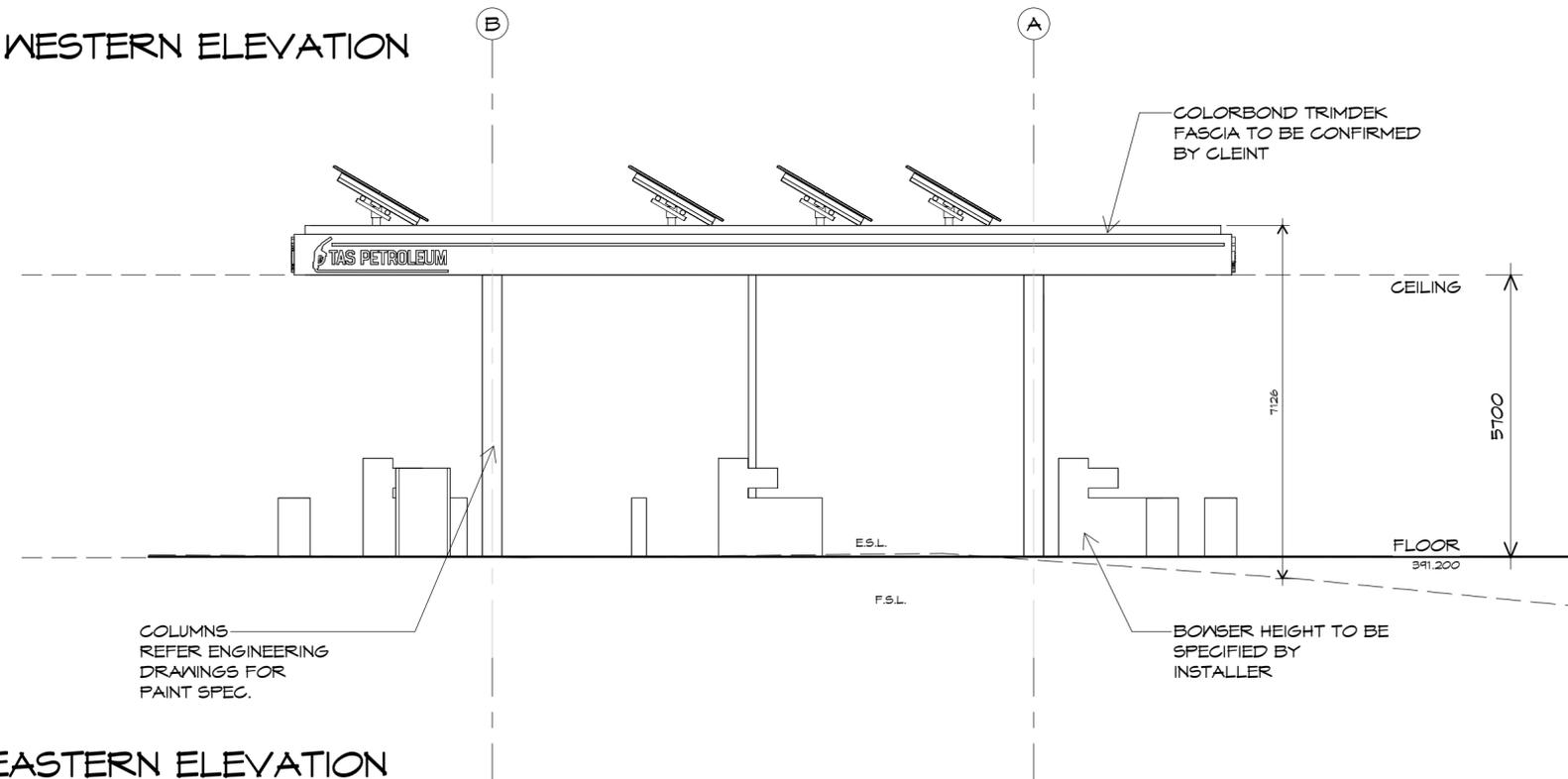
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NORTH WESTERN ELEVATION
1 : 100



NORTH EASTERN ELEVATION
1 : 100

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Client name:
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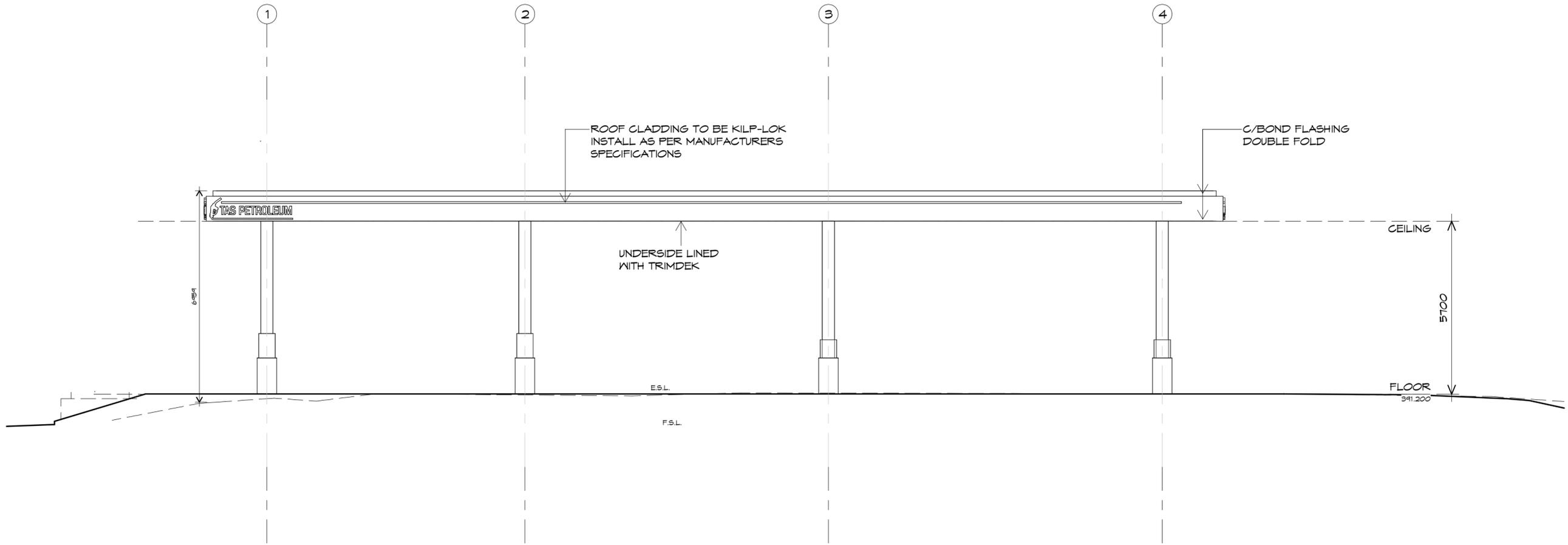
Drawing:
ELEVATIONS

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Date: 04.02.2026	Scale @A2: 1 : 100

Project/Drawing no: PD24266 -06	Revision: 10
Accredited building practitioner: Frank Geskus -No CC246A	

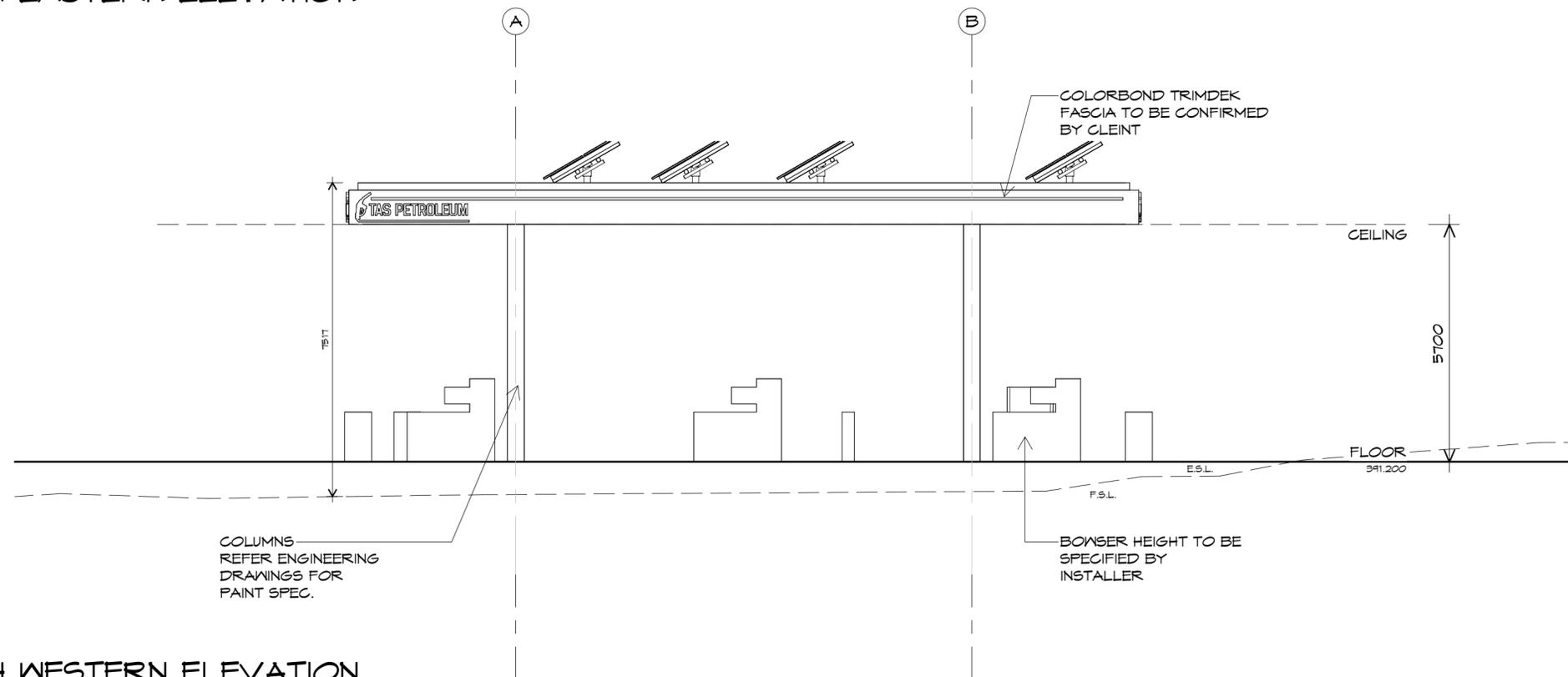


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SOUTH EASTERN ELEVATION

1 : 100



SOUTH WESTERN ELEVATION

1 : 100

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Project:
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OATLANDS**

Client name:
TAS PETROLEUM

Drawing:
ELEVATIONS

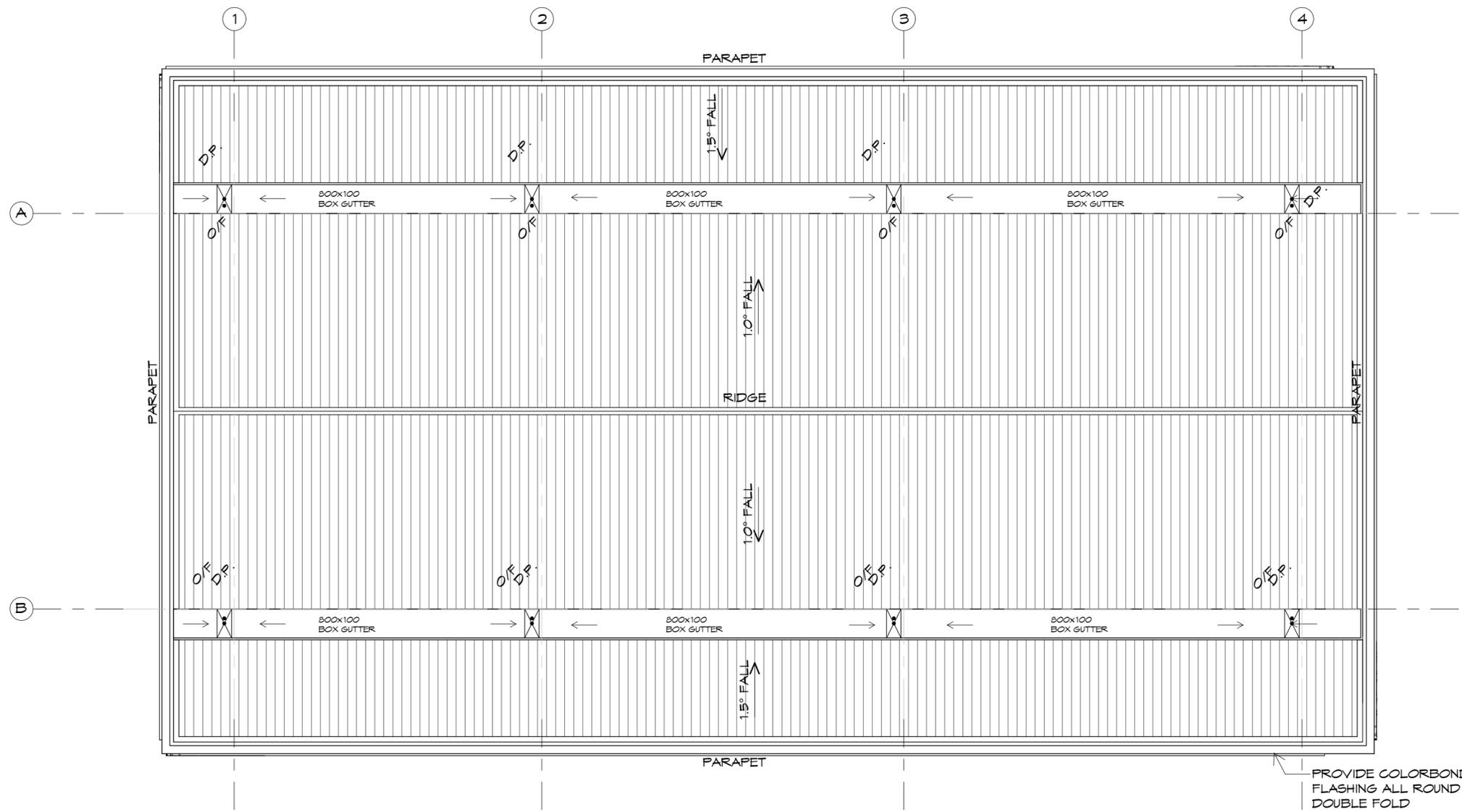
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ROOF PLAN
1 : 100

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Project:
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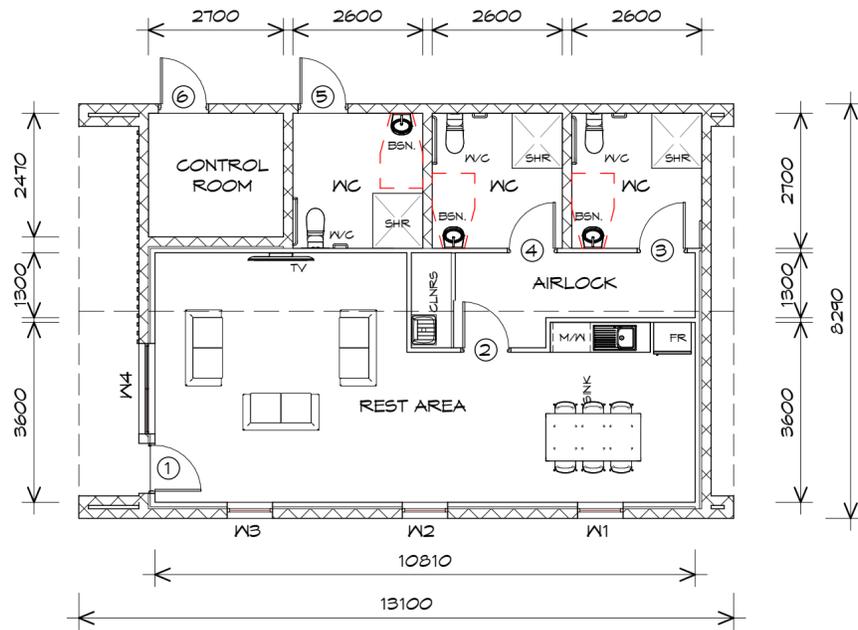
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Date: 04.02.2026 Scale @A2: 1 : 100

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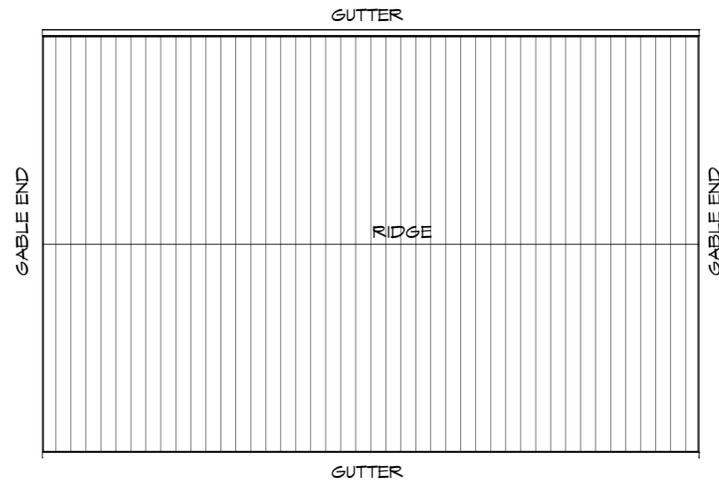
AMENITIES PLAN

1 : 100

CONTROL ROOM AREA 94.92 m² (10.22 SQUARES)
TOTAL AREA 94.92 10.22

NOTE:
FLOOR AREAS INCLUDE TO EXTERNAL FACE OF BUILDING, UNLESS OTHERWISE STATED.

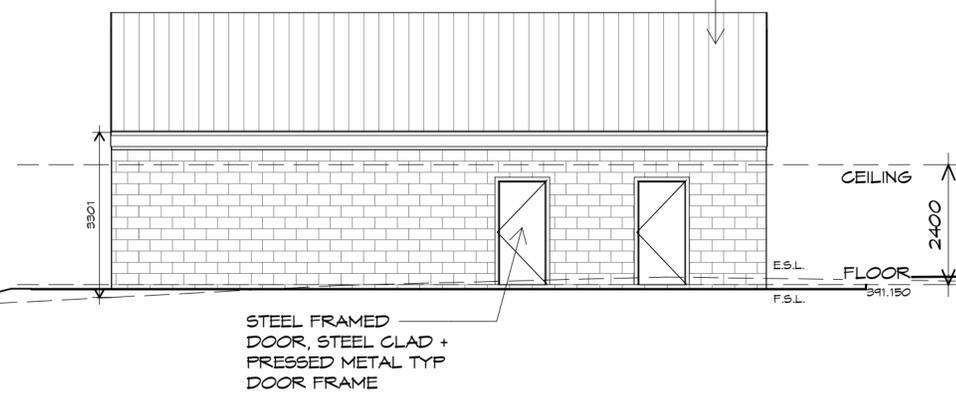
ROOF CLADDING
COLORBOND TRIMDEK
TO CLIENTS SPECS.



ROOF PLAN

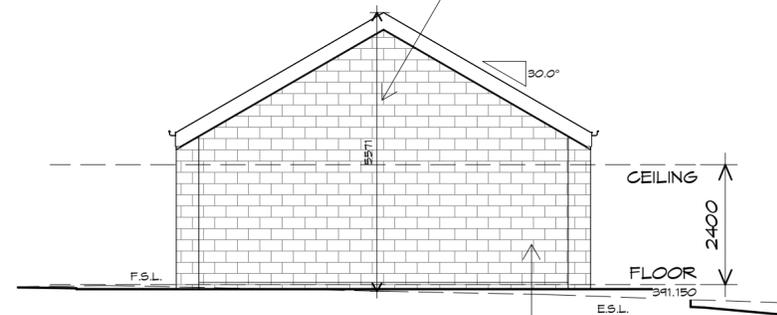
1 : 100

ROOF FRAMING
PREFABRICATED ROOF TRUSSES
@ 900 CRS MAX
BRACING BY OTHERS



AMENITIES NORTH WESTERN ELEVATION

1 : 100



AMENITIES NORTH EASTERN ELEVATION

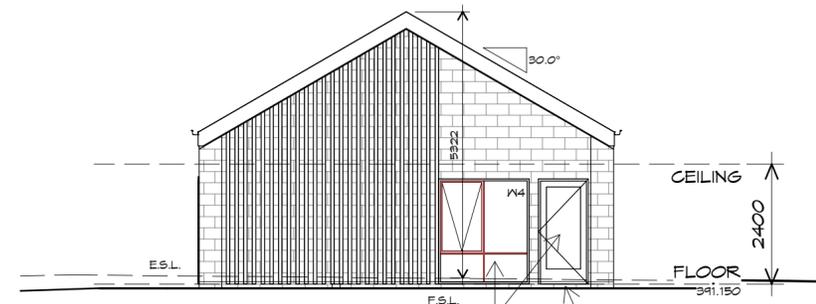
1 : 100

BLOCKWORK
CONCRETE BLOCK
FLUSH JOINTS, STRETCHER BOND
REFER ENGINEER FOR
ARTICULATION JOINTS
ALL MASONRY TO COMPLY
WITH NCC 2022 PART F3D5
REFER TECH DRY BLOCKS
MORTAR & CONC. INFILL TO HAVE
XYDEX ADDITIVE AS PER ISLAND
BLOCK & PAVING SPEC TO
ENSURE WATER TIGHT BUILDING



AMENITIES SOUTH EASTERN ELEVATION

1 : 100



AMENITIES SOUTH WESTERN ELEVATION

1 : 100



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Client name:
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Drawing:
FLOOR PLAN

Drafted by: D.D.H. Approved by: Approver

Date: 04.02.2026 Scale @A2: 1 : 100

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Attachment
Agenda Item 12.1.1
E3 Planning

Environment • Economy • Evolution

Appendix C

Traffic Impact Assessment



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0438376840 evan@e3planning.com.au



Prime Design
20 Interlaken Road, Oatlands
Traffic Impact Assessment
July 2025



CELEBRATING 15 YEARS
2008 - 2023

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1. Introduction

1.1 Background

Midson Traffic were engaged by Prime Design to prepare a traffic impact assessment for a proposed fuel station development at 20 Interlaken Road, Oatlands.

1.2 Traffic Impact Assessment (TIA)

A traffic impact assessment (TIA) is a process of compiling and analysing information on the impacts that a specific development proposal is likely to have on the operation of roads and transport networks. A TIA should not only include general impacts relating to traffic management, but should also consider specific impacts on all road users, including on-road public transport, pedestrians, cyclists and heavy vehicles.

This TIA has been prepared in accordance with the Department of State Growth (DSG) publication, *Traffic Impact Assessment Guidelines*, August 2020. This TIA has also been prepared with reference to the Austroads publication, *Guide to Traffic Management*, Part 12: *Integrated Transport Assessments for Developments*, 2020.

Land use developments generate traffic movements as people move to, from and within a development. Without a clear understanding of the type of traffic movements (including cars, pedestrians, trucks, etc), the scale of their movements, timing, duration and location, there is a risk that this traffic movement may contribute to safety issues, unforeseen congestion or other problems where the development connects to the road system or elsewhere on the road network. A TIA attempts to forecast these movements and their impact on the surrounding transport network.

A TIA is not a promotional exercise undertaken on behalf of a developer; a TIA must provide an impartial and objective description of the impacts and traffic effects of a proposed development. A full and detailed assessment of how vehicle and person movements to and from a development site might affect existing road and pedestrian networks is required. An objective consideration of the traffic impact of a proposal is vital to enable planning decisions to be based upon the principles of sustainable development.

This TIA also addresses the relevant clauses of C2.0, *Parking and Sustainable Parking Code*, and C3.0, *Road and Railway Assets Code*, of the Tasmanian Planning Scheme – Southern Midlands, 2021.

1.3 Statement of Qualification and Experience

This TIA has been prepared by an experienced and qualified traffic engineer in accordance with the requirements of Council's Planning Scheme and The Department of State Growth's, *Traffic Impact Assessment Guidelines*, August 2020, as well as Council's requirements.

The TIA was prepared by Keith Midson. Keith's experience and qualifications are briefly outlined as follows:

- 29 years professional experience in traffic engineering and transport planning.
- Master of Transport, Monash University, 2006
- Master of Traffic, Monash University, 2004

- Bachelor of Civil Engineering, University of Tasmania, 1995
- Engineers Australia: Fellow (FIEAust); Chartered Professional Engineer (CPEng); Engineering Executive (EngExec); National Engineers Register (NER)

1.4 Project Scope

The project scope of this TIA is outlined as follows:

- Review of the existing road environment in the vicinity of the site and the traffic conditions on the road network.
- Provision of information on the proposed development with regards to traffic movements and activity.
- Identification of the traffic generation potential of the proposal with respect to the surrounding road network in terms of road network capacity.
- Review of the parking requirements of the proposed development. Assessment of this parking supply with Planning Scheme requirements.
- Traffic implications of the proposal with respect to the external road network in terms of traffic efficiency and road safety.

1.5 Subject Site

The subject site is located at 20 Interlaken Road, Oatlands. The site is currently a vacant lot.

The subject site and surrounding road network is shown in Figure 1.

Figure 1 Subject Site & Surrounding Road Network



Image Source: LIST Map, DPIPW

1.6 Reference Resources

The following references were used in the preparation of this TIA:

- Tasmanian Planning Scheme – Southern Midlands, 2021 (Planning Scheme)
- Austroads, *Guide to Traffic Management, Part 12: Integrated Transport Assessments for Developments*, 2020
- Austroads, *Guide to Road Design, Part 4A: Unsignalised and Signalised Intersections*, 2021
- Department of State Growth, *Traffic Impact Assessment Guidelines*, 2020
- Transport NSW, *Guide to Traffic Impact Assessment*, 2024 (TIA Guide)
- Australian Standards, AS2890.1, *Off-Street Parking*, 2004 (AS2890.1)

2. Existing Conditions

2.1 Transport Network

For the purposes of this report, the transport network consists of Interlaken Road and Midland Highway.

2.1.1 Interlaken Road

Interlaken Road (Route C527) is a two-way sealed rural collector road that provides connectivity between the Midland Highway and the Lakes region of central Tasmania, specifically serving communities around Lake Crescent and Lake Sorell. It carries a traffic volume of approximately 1,000 vehicles per day.

It serves as a key freight and tourist route connecting Oatlands to Interlaken and surrounding agricultural areas. It carries a mix of vehicle types including passenger vehicles, agricultural machinery, and heavy vehicles servicing the agricultural sector. It connects to the Midland Highway at a T-junction with the highway having priority. A T-junction for Stanley Street is located opposite Interlaken Road in a staggered arrangement (the Stanley Street is located to the south of Interlaken Road junction). Both Interlaken Road and Stanley Street have channelised right and left turn lanes on the highway.

Interlaken Road has the following characteristics in the vicinity of the subject site:

- A sealed carriageway with a width of approximately 6-7 metres.
- Unsealed shoulders on both sides, typically 0.5 - 1.0 metres wide.
- Posted speed limit of 80 km/h in the rural sections.
- Generally flat to rolling terrain with good sight distances.
- Functions as a rural collector road, supporting both local access and through traffic.

Interlaken Road adjacent to the subject site is shown in Figure 2.

A dedicated truck turning area has recently been constructed opposite the site. The turning area is utilised as a truck rest stop facility. This is shown in Figure 3.

Figure 2 Interlaken Road



Figure 3 Interlaken Road Truck Turn Facility



2.1.2 Midland Highway

The Midland Highway is Tasmania's primary north-south transport corridor, connecting Hobart and Launceston. In the vicinity of Interlaken Road, the highway has the following characteristics:

- A two-way rural highway with a sealed carriageway width of approximately 14-metres.
- Two 3.8-metre traffic lanes (one in each direction); central 3.2-metre channelised right turn lanes into Interlaken Road and Stanley Street; 3.2 metre channelised left turn lanes into Interlaken Road and Stanley Street.
- Sealed shoulders approximately 1.5 - 2.5 meters wide on both sides.
- Posted speed limit of 110 km/h.
- Generally flat to gently undulating terrain providing good forward sight distance.

The highway carries a volume of approximately 4,500 vehicles per day to the south of the Interlaken Road junction. Peak traffic flows are approximately 500 vehicles per hour (PM peak). It carries approximately 20.4% heavy vehicles.

2.2 Road Safety Performance

Crash data can provide valuable information on the road safety performance of a road network. Existing road safety deficiencies can be highlighted through the examination of crash data, which can assist in determining whether traffic generation from the proposed development may exacerbate any identified issues.

Crash data was obtained from the Department of State Growth for a 5 year period between 1st January 2020 to 31st December 2024 for Interlaken Road between Midland Highway and Coldblow Lane.

Two crashes were reported during this time:

- 2:15am, Monday 7th February 2022 – 'other-curve' single vehicle crash resulting in property damage only.
- 3:06pm, Sunday 3rd March 2024 – no crash type recorded (single vehicle crash) resulting in property damage only.

The crash data does not indicate that there are any pre-existing road safety deficiencies in the transport network that may be exacerbated by traffic generated by the proposed development. No crashes were reported at the Midland Highway/ Interlaken Road junction.

3. Proposed Development

3.1 Development Proposal

The proposed development involves the construction of an unmanned 24-hour fuel station. The proposed development is primarily for trucks but will also be accessible by passenger vehicles. Three EV charging stations are provided, as well as 7 on-site parking spaces.

The site will be unmanned with all sales self-serve using a card reader. The development will include 8 bowsers available for all vehicle types, with two fuel bowsers that are accessible for large trucks. Access to the fuel station will be via two accesses on Interlaken Road. The development includes a control room and amenities.

The site has been designed to accommodate A-double trucks.

The proposed development is shown in Figure 4 and Figure 5.

Figure 4 Proposed Development Site Layout

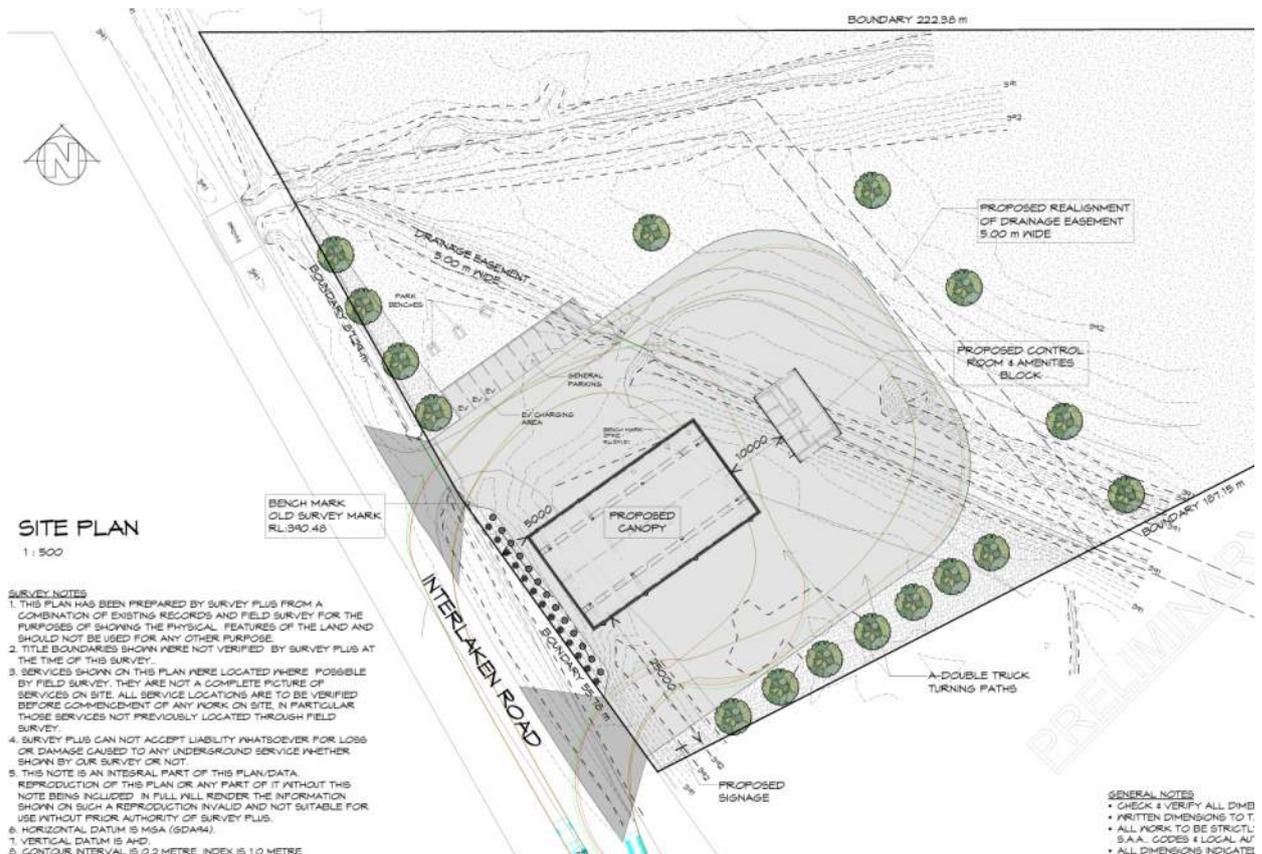
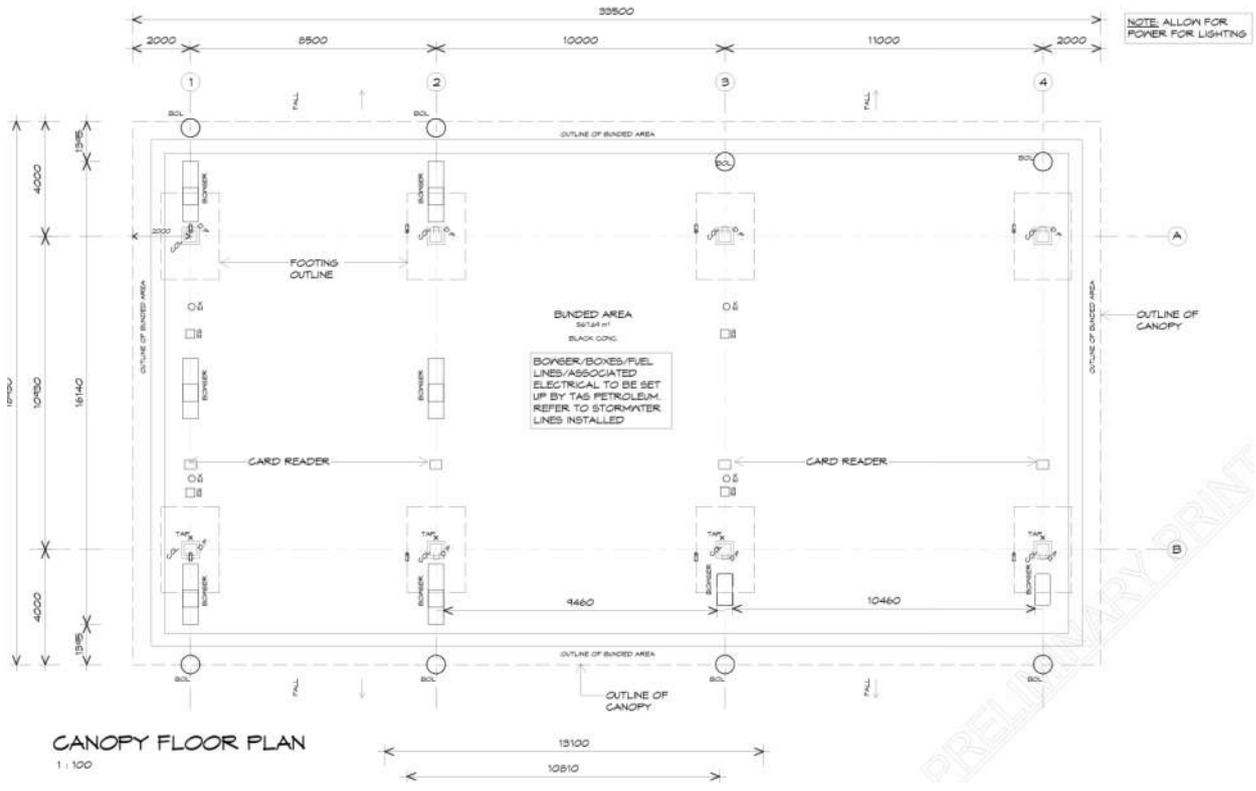


Figure 5 Proposed Development Canopy Floorplan



4. Traffic Impacts

4.1 Trip Generation

4.1.1 TIA Guide Traffic Generation Assessment

Trip generation rates were sourced from the TIA Guide as follows:

- Daily traffic generation (D) = $29.042(N)^2 + 222.58(N) + 1668.3$
- Site peak hour generation = $0.0819(D) + 46.302$
- AM peak hour generation = $0.2815(N)^2 + 14.047(N) + 16.715$
- PM peak hour generation = $0.0205(S) + 88.52$

Where N refers to the number of service channels and S refers to the total site area.

This results in the following traffic generation:

- Daily traffic generation = 1,836 vehicles per day
- Site peak hour generation = 197 vehicles per hour
- AM peak hour generation = 94 vehicles per hour
- PM peak hour generation = 185 vehicles per hour

It is noted that the proposed development will be unmanned, without provision of a convenience store on the site. The survey data provided in the TIA Guide relate to urban refuelling sites that have higher traffic flow on the adjacent road network and an attached convenience store. The traffic volume on the Midland Highway near the subject site is 4,500 vehicles per day, the traffic generation would represent approximately 40% of the AADT. On this basis the actual traffic generation is likely to be lower than the calculations provided above.

4.1.2 Comparison Site Traffic Generation

A similar example of the type of refuelling station proposed is the Campbell Town South Diesel Stop located on the corner of Midland Highway and Torless Street (site address is 184 Main Street, Campbell Town). This truck fuelling station was approved in 2016 and is shown in Figure 2. It operates safely and efficiently in a similar nature to the proposed development, noting that it is located on a major highway (Midland Highway), which carries approximately 6,500 vehicles per day near the site. The Campbell Town site is likely to generate a similar amount of traffic generation compared to the proposed development at 26A Tannery Road. The Midland Highway has a posted speed limit of 60-km/h adjacent to the Campbell Town site.

In this regard, there are strong comparisons between the Campbell Town site and the proposed development.

To better understand the likely traffic generation associated with the proposed development, transaction data was obtained from the Ampol self-service fuel station in Campbell Town. The Campbell Town site has been designed to cater for trucks, however cars can access the site and refuel in a similar manner to the proposed development. The site contains four bowser pump locations. Data was obtained for 2022, with total transactions summarised in Table 1.

Table 1 Campbell Town Ampol – Total Monthly Transactions 2022

Month	Total Transactions	Average Daily Transactions
January	2,789	90
February	2,873	103
March	3,028	98
April	2,594	86
May	2,653	86
June	2,335	78
July	2,322	75
August	2,384	77
September	2,316	77
October	2,483	80
November	2,573	86
December	2,273	73

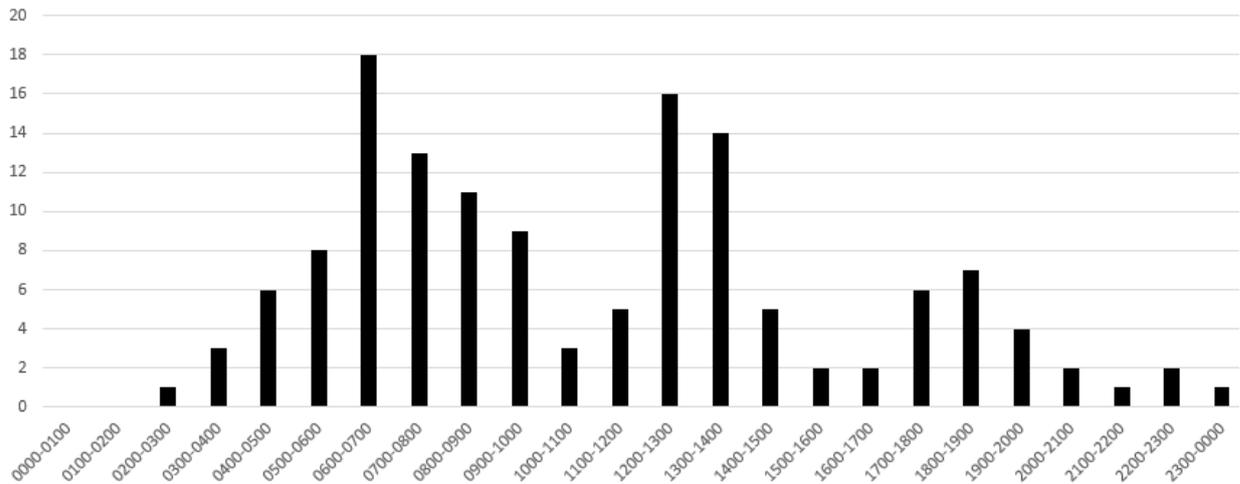
It can be seen that average daily transactions vary between 73 and 103. Full data was examined for the busiest month (February 2022). Daily transactions varied between 40 and 140 in February 2022, with an 85th percentile highest number of transactions being 134 per day. This equates to an 85th percentile daily traffic generation of 268 vehicles per day (two-way movements – 1 transaction equates to 1 entry movement and 1 exit movement).

Analysis of the peak day in February (Thursday 24th February 2022) indicates that the peak number of transactions per hour is summarised in Figure 1. It can be seen that peak periods do not correspond to commuter peak periods, although this is likely to be a function of the location being towards the middle of the Midland Highway. The peak number of transactions was 18 between 6:00am and 7:00am. The average hourly number of transactions was 6 (12 x two-way vehicle movements). This equates to a peak generation at the Campbell Town site of 36 vehicles per hour (two-way movements).

It is noted that the transactions do not differentiate between vehicle types. Whilst it is not possible to provide an accurate breakdown of transactions associated with cars and trucks. It was noted that the

majority of transactions were for large quantities of fuel, therefore demonstrating that trucks were the dominant users of the site.

Figure 6 Hourly Transactions February 2022



Comparing this to proposed development. The TIA Guide traffic generation of the development was estimated in the TIA to be 1,836 vehicles per day with a peak of 197 vehicles per hour. In light of the data obtained from the Campbell Town site, the TIA traffic generation is likely to be a significant over-estimate. Given the similarities between the sites, it would be a safe assumption that the peak generation would be in the order of 50 vehicles per hour (two-way movements), with a daily generation of 400 vehicles per day (two-way movements). This is on the assumption that whilst the proposed development has more bowsters, it is located away from the Highway in a location that has less traffic volume than the Campbell Town site.

Therefore for the purposes of this report, the proposed development is assumed to have a traffic generation of 400 vehicles per day, with a peak of 50 vehicles per hour. This traffic generation estimate should be considered an upper limit as it is based on peak transaction data from a similar site (rather than average data).

Figure 7 Comparison Site: Campbell Town Truck Refuelling Station



4.2 Trip Assignment

Due to the site's proximity to the Midland Highway, the majority of movements at the site's access will be right-in/ left-out.

4.3 Access Impacts

The proposed development relies on two accesses providing a one-way entry and exit configuration. The entry driveway will be constructed at the appropriate location of an existing access, and the exit driveway will be constructed towards the eastern boundary of the site's frontage on Interlaken Road.

For the new exit driveway, the Acceptable Solution A1.2 of Clause C3.5.1 of the Planning Scheme states "*For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority*". The proposed development proposes two accesses (one entry and one exit driveway) and therefore the Acceptable Solution A1.2 of Clause C3.5.1 of the Planning Scheme is not met.

For the existing access, the Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme states "*Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not*

increase by more than the amounts in Table C3.1". In this case the existing access currently generates no traffic, therefore the increase in traffic at the access will exceed the requirements of Table C3.1 (20% or 40 vehicle movements per day, whichever is greater).

The Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme states:

"Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:

- (a) any increase in traffic caused by the use;*
- (b) the nature of the traffic generated by the use;*
- (c) the nature of the road;*
- (d) the speed limit and traffic flow of the road;*
- (e) any alternative access to a road;*
- (f) the need for the use;*
- (g) any traffic impact assessment; and*
- (h) any advice received from the rail or road authority".*

The following is relevant with respect to the Interlaken Road accesses:

- a. Increase in traffic. The traffic generation of the proposed development each access will be approximately 400 vehicles per day, with a peak of 50 vehicles per hour. The traffic generation can be absorbed in the surrounding road network without loss of operational efficiency.
- b. Nature of traffic. The traffic will be consistent with the existing traffic currently utilising Interlaken Road. The nature of the development is such that it will attract existing traffic that would normally be travelling on the Midland Highway (short diversion utilising Interlaken Road).
- c. Nature of road. Interlaken Road is a low-volume rural road.
- d. Speed limit and traffic flow. Interlaken Road has a speed limit of 80-km/h and traffic flow in the order of 1,000 vehicles per day. The speed limit and traffic flow of Interlaken Road can sufficiently absorb the traffic generation at the proposed access.
- e. Alternative access. No alternative access is available or considered necessary.
- f. Need for use. The accesses are required to provide vehicular access to the proposed development.
- g. Traffic impact assessment. This report documents the findings of a traffic impact assessment. Importantly there is spare capacity to absorb the traffic generation associated with the proposed development.

Based on the above assessment, the accesses meet the requirements of Performance Criteria P1 of Clause C3.5.1.

4.4 Sight Distance

Australian Standards, AS2890.1, provide the sight distance requirements for commercial driveways. Sight distance requirements are lower for commercial driveways compared to road junctions.

The minimum sight distance requirements for an access driveway in a 80-km/h frontage road is 105 metres (the desirable sight distance is 111 metres). The available sight distance exceeds 200 metres in both directions along Interlaken Road.

4.5 Pedestrian Impacts

The proposed development is an automated fuel station with no associated convenience store. It will not generate pedestrian movements to or from the surrounding road network.

4.6 Midland Highway Intersection Assessment

The proposed development will result in increased heavy vehicle movements at the Midland Highway/Interlaken Road intersection. Although this intersection was not included in the recent Midland Highway safety upgrades, an assessment has been undertaken to determine its suitability for accommodating 26m B-Double truck movements.

4.6.1 Intersection Configuration

The existing intersection comprises of the following:

- 3.8-metre traffic lanes in each direction on the Midland Highway
- 3.2-metre channelised right turn lane from the highway into Interlaken Road
- 3.2-metre channelised left turn lane from the highway into Interlaken Road
- Corresponding channelised lanes for Stanley Street in a staggered arrangement

4.6.2 26m B-Double Assessment

Swept path analysis has been undertaken for 26m B-Double movements at the intersection, as shown in Figure 8. The analysis demonstrates:

- Left turn movements from the Midland Highway into Interlaken Road can be accommodated within the existing channelised left turn lane and available pavement width.
- Right turn movements from Interlaken Road onto the Midland Highway can be accommodated within the existing intersection geometry.
- The 3.2-metre channelised turn lanes provide adequate width for 26m B-Double swept paths.

- Shoulder areas provide additional manoeuvring space where required.

4.6.3 Priority Arrangements

The existing priority arrangement with the Midland Highway having priority and channelised left turn lanes (CHL) is appropriate for the increased truck traffic.

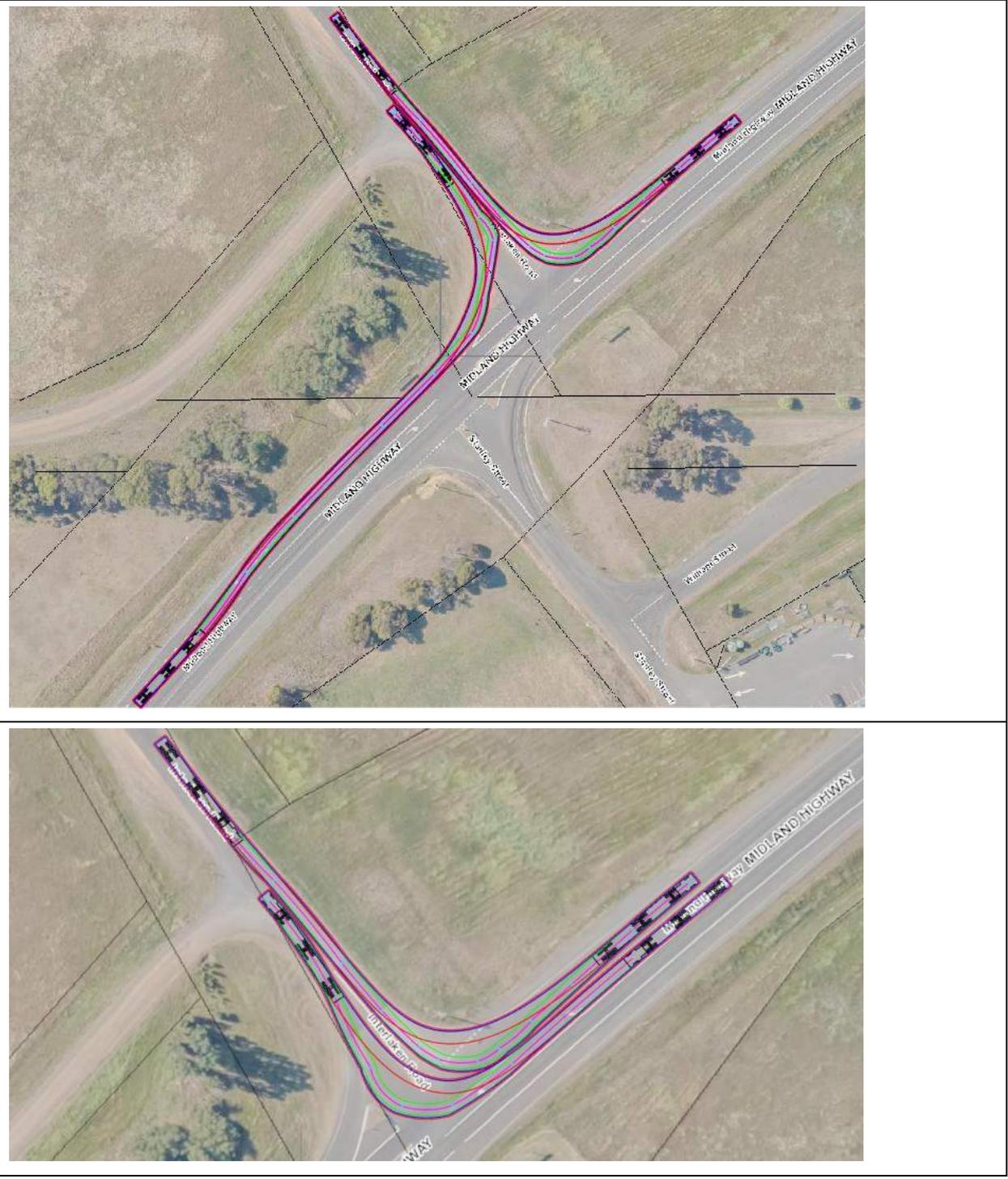
The channelised configuration provides:

- Dedicated space for turning movements separate from through traffic.
- Reduced conflict points compared to conventional intersections.
- Improved sight lines for both turning and through movements.

4.6.4 Traffic Volume Impact

The additional truck traffic generated by the development (estimated at 50% of the 50 vehicles per hour peak generation, or approximately 25 trucks per hour) represents a manageable increase that can be accommodated within the existing intersection capacity. This equates to less than one additional truck movement every two minutes during peak periods.

Figure 8 26m B-Double Swept Paths – Midland Hwy/ Interlaken Rd



4.7 Interlaken Road Impacts

An assessment has been undertaken to confirm that Interlaken Road can accommodate two-way heavy vehicle movements between the development site and the Midland Highway.

4.7.1 Road Width Assessment

Interlaken Road has the following characteristics:

- Sealed carriageway width of approximately 6.0 to 7.0 metres.
- Unsealed shoulders of typically 0.5 to 1.0 metres on both sides.
- Total available width of approximately 6.5 to 8.0 metres including shoulders

4.7.2 Two-Way Heavy Vehicle Movements

The available road width is adequate for two-way movements of 26m B-Doubles. Standard design vehicle widths are:

- 26m B-Double: 2.5 metres width.
- Required clearance between vehicles: minimum 0.5 metres.
- Total width required for two-way B-Double movements: 5.5 metres.

With a sealed carriageway of 6-7 metres, there is sufficient width for two 26m B-Doubles to pass safely. The unsealed shoulders provide additional manoeuvring space if required. It is noted that the swept paths shown in Figure 8 demonstrate the passage of B-Double vehicles travelling in opposing directions on Interlaken Road.

4.7.3 Existing Heavy Vehicle Use

Interlaken Road currently carries agricultural vehicles and trucks servicing the rural area. The recent construction of a dedicated truck turning facility opposite the site (shown in Figure 3) demonstrates the road's suitability for heavy vehicle use and the recognition by authorities of this function.

4.8 Road Safety Impacts

There are no significant detrimental road safety impacts foreseen for the proposed development. This is based on the following:

- The existing road safety performance of the road network does not indicate that there are any current road safety deficiencies that might be exacerbated by the proposed development (noting that only two crashes have been reported in Interlaken Road in the most recent five-year period).
- Adequate sight distance is available at the proposed site access on Interlaken Road in relation to the prevailing vehicle speeds in accordance with Australian Standards requirements.



-
- The additional traffic generated by the proposed development (noting an estimated peak of 50 vehicles per hour, which equates to an average of slightly less than 1 vehicle per minute) can be readily absorbed by the surrounding road network.

5. Parking Assessment

5.1 Parking Provision

The proposed development provides a total of 7 on-site car parking spaces, as well as 3 x EV charging station parking spaces.

5.2 Planning Scheme Requirements

The Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme states:

"The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:

- (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;*
- (b) the site is contained within a parking precinct plan and subject to Clause C2.7;*
- (c) the site is subject to Clause C2.5.5; or*
- (d) it relates to an intensification of an existing use or development or a change of use where:*
 - (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or*
 - (ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:*

$$N = A + (C - B)$$

N = Number of on-site car parking spaces required

A = Number of existing on site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1

C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1".

Table C2.1 requires 4 parking spaces per service bay for 'vehicle fuel sales and service'. In this case the proposed development is an automated fuel station that does not have a service component and therefore no parking spaces are required (no service bays provided).

The parking provision therefore meets the requirements of Table C2.1 and the requirements of Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme.

5.3 Car Parking Layout

The on-site car parking will be utilised by visitors to the site to use the amenities, or to wait for an accompanying vehicle to refuel or recharge.

The Acceptable Solution A1.1 of Clause C2.6.2 of the Planning Scheme states:

"Parking, access ways, manoeuvring and circulation spaces must either:

(a) comply with the following:

(i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6;

(ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;

(iii) have an access width not less than the requirements in Table C2.2;

(iv) have car parking space dimensions which satisfy the requirements in Table C2.3;

(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;

(vi) have a vertical clearance of not less than 2.1m above the parking surface level; and

(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or

(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6".

The development was assessed against A1.1(b). The relevant Australian Standards associated with the development is AS2890.1. The assessment is provided in the following sections.

5.3.1 Parking Grade

Section 2.4.6 of AS2890.1 states that the maximum grades within a car park shall be:

- Measured parallel to the angle of parking 1 in 20 (5%)
- Measured in any other direction 1 in 16 (6.25%)

The grades of the parking spaces are effectively level, thus complying with the AS2890.1 grade requirements.

5.3.2 Parking Dimensions

AS2890.1 defines the parking as User Class 2, *Medium term parking*. This accounts for the infrequent use of the parking within the development, as well as the time required for the electric charging within the EV spaces.

User Class 2 requires the following minimum parking dimensions:

- Space width 2.5 metres
- Space length 5.4 metres
- Aisle width 5.8 metres

The car park layout is as follows:

- Space width 2.7 metres
- Space length 5.4 metres
- Aisle width > 20 metres

The parking therefore complies with the dimensional requirements of AS2890.1.

5.3.3 AS2890.1 Assessment Summary

The parking space dimensions and manoeuvring areas comply with the requirements of AS2890.1. The development therefore complies with the requirements of Acceptable Solution A1.1(b) of Clause C2.6.2 of the Planning Scheme.

5.4 Vehicle Manoeuvring

The fuel station will cater for various vehicle types. The site will be accessible for vehicles up to A-double trucks.

The swept paths of 36 metre A-doubles were tested. The swept paths are shown in Figure 9. It can be seen that A-double movements can be accommodated within the site.

6. Conclusions

This traffic impact assessment (TIA) investigated the traffic and parking impacts of a proposed unmanned refuelling station development at 20 Interlaken Road, Oatlands.

The key findings of the TIA are summarised as follows:

- The development will generate a total of 400 vehicles per day with a peak of 50 vehicles per hour. The nature of the development will result in the majority of this traffic generation being 'linked trips', where a vehicle already travelling on the Midland Highway will divert into and out of the proposed development.
- The traffic generation associated with the proposed development meets the requirements of Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme.
- The development will provide a new access on Interlaken Road. The access has been designed to accommodate the swept paths of A-doubles and semi-trailers. The access arrangements will facilitate separated entry and exit driveways for the site.
- The Midland Highway/ Interlaken Road intersection can safely accommodate 26m B-Double movements associated with the proposed development. Swept path analysis confirms that the existing channelised turn lanes provide adequate width and geometry for these vehicle types. The channelisation of entering left and right turn lanes provide appropriate separation of truck movements from through movements on the Midland Highway.
- Interlaken Road has sufficient width to accommodate two-way heavy vehicle traffic between the development site and the Midland Highway. The 6-7 metre sealed carriageway width exceeds the 5.5 metres required for two-way B-Double movements.
- The Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme is met as it does not trigger a need for parking. A total of 10 on-site parking spaces are provided. This includes 3 x EV spaces. The parking can be used for access to the on-site amenities provided, as well as vehicles accompanying vehicles that are being refuelled or recharged.

Based on the findings of this report the proposed development is supported on traffic grounds.

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Attachment
Agenda Item 12.1.1



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Taroona TAS 7053

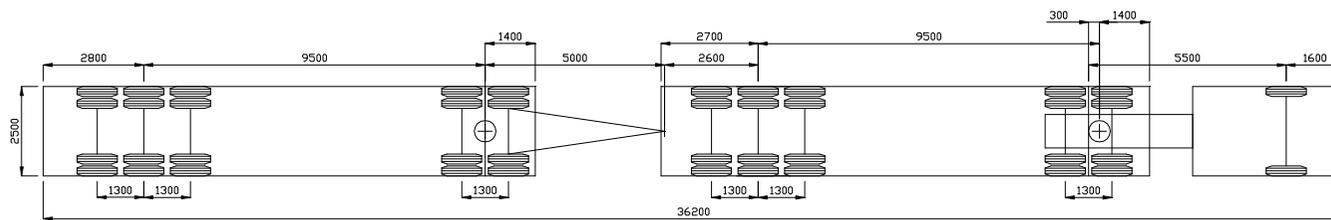
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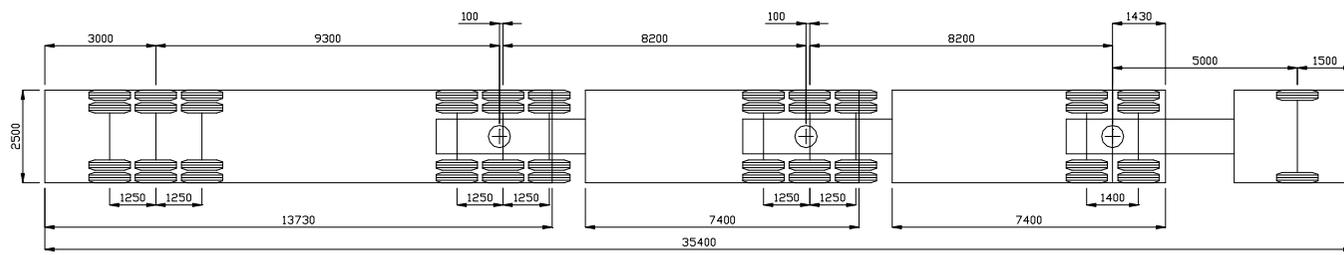
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Document Status

Revision	Author	Review	Date
0	Keith Midson	Zara Kacic-Midson	11 February 2025
1	Keith Midson	Zara Kacic-Midson	18 March 2025
2	Keith Midson	Zara Kacic-Midson	4 July 20255



A-double (Type I) (36.2 m)



B-triple (35.4 m)



-
- i [Guidelines for the Provision of Heavy Vehicle Rest Area Facilities \(Edition 1.1\)](#)
 - ii Midland Highway 10 Year Action Plan, Department of State Growth
 - iii [Appendix E Detailed fuel consumption coefficients \(uninterrupted flow\) | Australian Transport Assessment and Planning](#)
 - iv [Survey of Motor Vehicle Use, Australia, 12 Months ended 30 June 2020 | Australian Bureau of Statistics](#)
 - v Fuel Prices Near Launceston, TAS 7250 | FuelRadar
 - vi Pers comm Ben Fenton Tas Petroleum
 - vii Pers comm Ben Fenton





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Attachment
Agenda Item 12.1.1
E3 Planning

Environment • Economy • Evolution

Appendix D
Bushfire Report



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0438376840 evan@e3planning.com.au

Emergency Management Strategy: Fuel Stop 20 Interlaken Road, Oatlands

This document broadly identifies the proposed emergency management arrangements that will be formed into a detailed Bushfire Emergency Plan, for the purposes of a planning permit for a Hazardous Use (Fuel Stop with storage capacity in excess of manifest quantities.)

The purpose of this emergency management strategy is to demonstrate how risk to employees and visitors to the site will be managed to a tolerable level through contextualised emergency procedures. The emergency management strategy informs the more detailed Bushfire Emergency Plan, which is required at the building permit stage.

Facility

Unmanned Fuel Stop 20 Interlaken Road, Oatlands, CT 44807/6.

8 bowsers, under canopy area., and in ground fuel tanks. Amenities block & Control Room. 3 EV charging stations. The control room has no hazardous materials, and relies on electrical controls of fuel flows.

The site will be required to meet requirements of the Bushfire Hazard Management Plan including water supply and hazard management areas, in addition to fire and emergency requirements under other regulations for building approvals.

Fuel Storage Capacity (to be confirmed)

Underground tanks (2) located to the west of bowsers

	Type	tank (L)	Total (L)
petrol	91	66000	107,000
	95	20500	
	98	20500	
diesel	Std	71200	116,200

223,200

Occupancy characteristics

The site is unlikely to regularly have staff on site, occasional staff and contractors may be on site for site maintenance and deliveries. Clients are likely to have 24 hr access to refuelling facilities.

Emergency management structure and capability

Bushfire Emergency Plan to establish structure, chief, deputy wardens. This is likely to include staff not normally on site. Offsite (remote) control / shutdown of fuel pumping to be included in construction planning.

The building(s) and/or site vulnerability

- Canopy, (including bowsers, amenities / control room building and underground fuel tanks will have at least BAL 12.5 separation from bushfire prone vegetation and be maintained in perpetuity.
- Bushfire Emergency Plan, to be endorsed by Tasmania Fire Service, prior to storage of hazardous material in excess of manifest quantities.
- Compliance with AS1940, *The Storage and Handling of Flammable and Combustible Liquids*.
- Staff training as appropriate.

Possible bushfire scenarios

The fire direction under high and above fire danger ratings is likely to be from the northwest. To the northwest the land is grassland with forest around 4km. Forest occurs within 600m to the north. The surrounding land in all directions is grassland. The major bushfire threat to the facility will be from smoke and ash, with adequate onsite hazard management and building protection measures onsite ignition is considered unlikely from bushfires.

Primary and contingency bushfire safety options,

- Primary response is to evacuate for all staff / contractors / clients and closure of fuel flows to bowsers to prevent filling of vehicles, this will require remote shutdown capability.
- Shelter in place possible in the amenities building for limited numbers.

Firefighter access, firefighting services, and firefighter protection;

- Oatlands Fire Station is located at 18 Church Street Oatlands, (2 min east by vehicle)
- The site will have good access to all facilities / site.
- Insite fire equipment may include extinguishers and hose reel, noting the site will not be staffed and clients are expected to evacuate the site.
- Amenities building provides potential (limited space) onsite shelter.
- The site is not currently serviced by a reticulated water supply, a 225mm reticulation main runs 90m to the south of the lot and may provide a suitable water supply subject to negotiations with TasWater and inclusion of a new main to service the facility. If no reticulated supply is available a static supply will be required within a 90m hoselay of all areas of the canopy, tank farm and amenities building. Hydrants are available within the Oatlands township around 500m by road from the site.

Risk analysis

The most likely threat from bushfire to the site under high bushfire conditions is from fires in the forested areas to the northwest and north of the site, these are likely to carry ash and

under severe and higher rating burning embers. The nearest forest area in those directions is 600m from the site. The site may also be impacted by grass fire, all hazardous use points, bowsers and tanks will have in well in excess of BAL 12.5 separation from grassland.

Fires in those areas should give adequate warning times before becoming extensive enough to impact the site. Under known bushfire activity in the area Fuel Stop operators should monitor the situation and the site. Maintenance of grounds and building should minimise the potential fuel/ignition points on site.

Figure 4: fire history, shows the recorded fire history of the area, this shows no fire history to the northeast and south within 10km of the site, several fires have occurred in the 5-10km to the west.

Figure 5: TasVeg Flammability, shows surrounding land to be moderate high flammability.

Bushfire Protection Measures

- Remote shutdown of fuel supply to bowsers to be included in Construction plans.
- Fire Protection requirements to meet or exceed standard for Service Stations.
- Consider installation of a closed sign visible to clients at site entries to be activated if bowsers are shutdown.
- Bushfire Action Plan and offsite refuge map to be displayed at payment point and amenities building.

Proposed Emergency Plan:

- High or greater Bushfire Forecast or known fire in the vicinity: Chief Warden to monitor incident closely, brief staff on situation, review Emergency Plan.
- Ability to close fuel supply to bowsers.
- Consider installation of a closed sign visible to clients at site entries to be activated if
- Cancel fuel deliveries.
- Clients to evacuate at own recognisance.

Potential Offsite Refuge:

- Oatlands, 2 minutes east by vehicle.

Scott Livingston
BFP #105

14/4/2025, Version 2

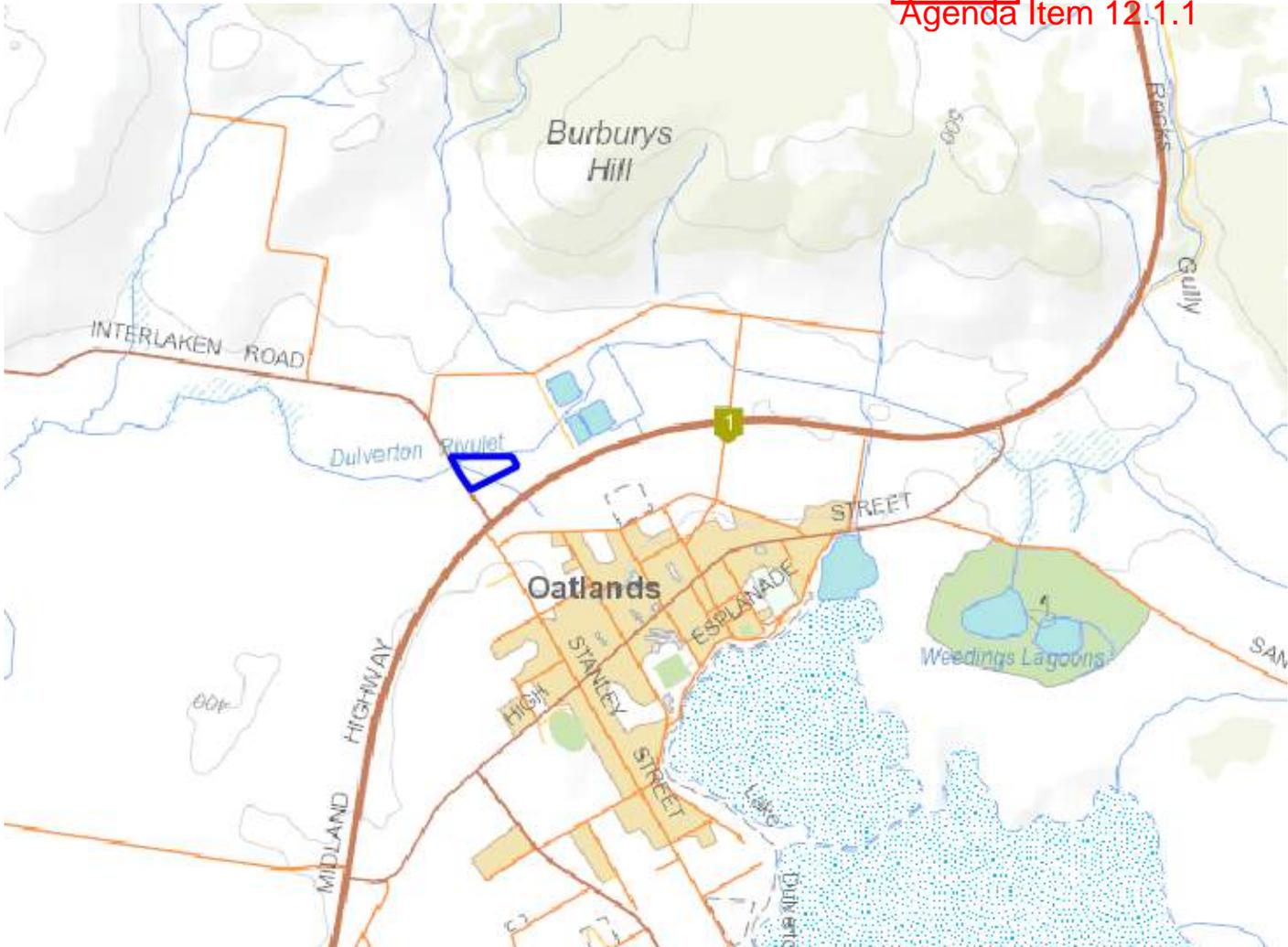
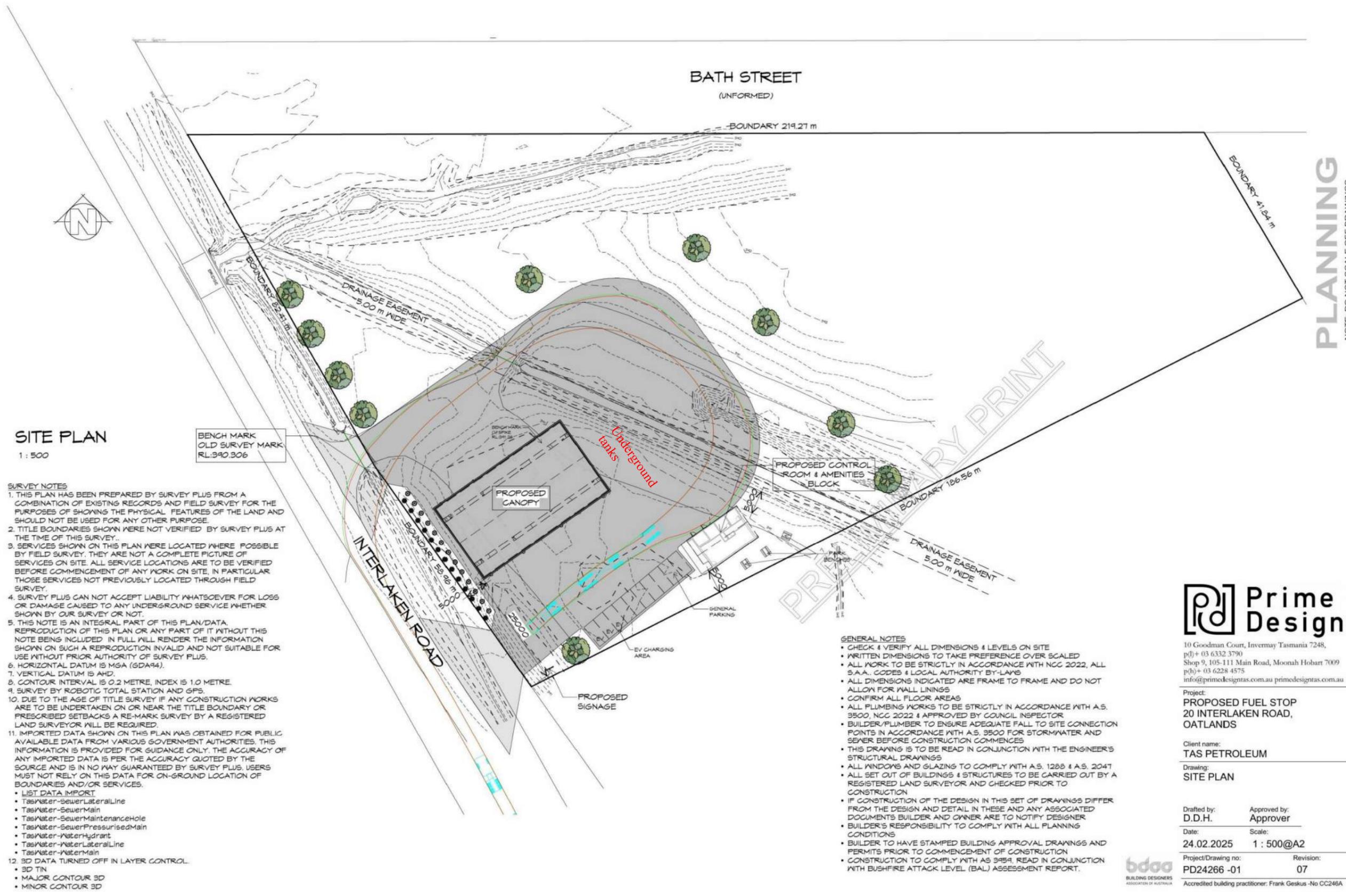


Figure 1: Location



Figure 2: Aerial Image (Google Earth)



PLANNING
NOTE: DO NOT SCALE OFF DRAWINGS

SITE PLAN
1 : 500

BENCH MARK
OLD SURVEY MARK
RL:390.306

- SURVEY NOTES**
1. THIS PLAN HAS BEEN PREPARED BY SURVEY PLUS FROM A COMBINATION OF EXISTING RECORDS AND FIELD SURVEY FOR THE PURPOSES OF SHOWING THE PHYSICAL FEATURES OF THE LAND AND SHOULD NOT BE USED FOR ANY OTHER PURPOSE.
 2. TITLE BOUNDARIES SHOWN WERE NOT VERIFIED BY SURVEY PLUS AT THE TIME OF THIS SURVEY.
 3. SERVICES SHOWN ON THIS PLAN WERE LOCATED WHERE POSSIBLE BY FIELD SURVEY. THEY ARE NOT A COMPLETE PICTURE OF SERVICES ON SITE. ALL SERVICE LOCATIONS ARE TO BE VERIFIED BEFORE COMMENCEMENT OF ANY WORK ON SITE, IN PARTICULAR THOSE SERVICES NOT PREVIOUSLY LOCATED THROUGH FIELD SURVEY.
 4. SURVEY PLUS CAN NOT ACCEPT LIABILITY WHATSOEVER FOR LOSS OR DAMAGE CAUSED TO ANY UNDERGROUND SERVICE WHETHER SHOWN BY OUR SURVEY OR NOT.
 5. THIS NOTE IS AN INTEGRAL PART OF THIS PLAN/DATA. REPRODUCTION OF THIS PLAN OR ANY PART OF IT WITHOUT THIS NOTE BEING INCLUDED IN FULL WILL RENDER THE INFORMATION SHOWN ON SUCH A REPRODUCTION INVALID AND NOT SUITABLE FOR USE WITHOUT PRIOR AUTHORITY OF SURVEY PLUS.
 6. HORIZONTAL DATUM IS MGA (GDA94).
 7. VERTICAL DATUM IS AHD.
 8. CONTOUR INTERVAL IS 0.2 METRE, INDEX IS 1.0 METRE.
 9. SURVEY BY ROBOTIC TOTAL STATION AND GPS.
 10. DUE TO THE AGE OF TITLE SURVEY IF ANY CONSTRUCTION WORKS ARE TO BE UNDERTAKEN ON OR NEAR THE TITLE BOUNDARY OR PRESCRIBED SETBACKS A RE-MARK SURVEY BY A REGISTERED LAND SURVEYOR WILL BE REQUIRED.
 11. IMPORTED DATA SHOWN ON THIS PLAN WAS OBTAINED FOR PUBLIC AVAILABLE DATA FROM VARIOUS GOVERNMENT AUTHORITIES. THIS INFORMATION IS PROVIDED FOR GUIDANCE ONLY. THE ACCURACY OF ANY IMPORTED DATA IS PER THE ACCURACY QUOTED BY THE SOURCE AND IS IN NO WAY GUARANTEED BY SURVEY PLUS. USERS MUST NOT RELY ON THIS DATA FOR ON-GROUND LOCATION OF BOUNDARIES AND/OR SERVICES.
 - LIST DATA IMPORT
 - TasWater-SewerLateralLine
 - TasWater-SewerMain
 - TasWater-SewerMaintenanceHole
 - TasWater-SewerPressurisedMain
 - TasWater-WaterHydrant
 - TasWater-WaterLateralLine
 - TasWater-WaterMain
12. 3D DATA TURNED OFF IN LAYER CONTROL.
- 3D TIN
 - MAJOR CONTOUR 3D
 - MINOR CONTOUR 3D

- GENERAL NOTES**
- CHECK & VERIFY ALL DIMENSIONS & LEVELS ON SITE
 - WRITTEN DIMENSIONS TO TAKE PREFERENCE OVER SCALED
 - ALL WORK TO BE STRICTLY IN ACCORDANCE WITH NCC 2022, ALL S.A.A. CODES & LOCAL AUTHORITY BY-LAWS
 - ALL DIMENSIONS INDICATED ARE FRAME TO FRAME AND DO NOT ALLOW FOR WALL LININGS
 - CONFIRM ALL FLOOR AREAS
 - ALL PLUMBING WORKS TO BE STRICTLY IN ACCORDANCE WITH A.S. 3500, NCC 2022 & APPROVED BY COUNCIL INSPECTOR
 - BUILDER/PLUMBER TO ENSURE ADEQUATE FALL TO SITE CONNECTION POINTS IN ACCORDANCE WITH A.S. 3500 FOR STORMWATER AND SEWER BEFORE CONSTRUCTION COMMENCES
 - THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE ENGINEER'S STRUCTURAL DRAWINGS
 - ALL WINDOWS AND GLAZING TO COMPLY WITH A.S. 1288 & A.S. 2047
 - ALL SET OUT OF BUILDINGS & STRUCTURES TO BE CARRIED OUT BY A REGISTERED LAND SURVEYOR AND CHECKED PRIOR TO CONSTRUCTION
 - IF CONSTRUCTION OF THE DESIGN IN THIS SET OF DRAWINGS DIFFER FROM THE DESIGN AND DETAIL IN THESE AND ANY ASSOCIATED DOCUMENTS BUILDER AND OWNER ARE TO NOTIFY DESIGNER
 - BUILDER'S RESPONSIBILITY TO COMPLY WITH ALL PLANNING CONDITIONS
 - BUILDER TO HAVE STAMPED BUILDING APPROVAL DRAWINGS AND PERMITS PRIOR TO COMMENCEMENT OF CONSTRUCTION
 - CONSTRUCTION TO COMPLY WITH AS 3159, READ IN CONJUNCTION WITH BUSHFIRE ATTACK LEVEL (BAL) ASSESSMENT REPORT.



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info@primedesigntas.com.au primedesigntas.com.au

Project:
**PROPOSED FUEL STOP
20 INTERLAKEN ROAD,
OATLANDS**

Client name:
TAS PETROLEUM

Drawing:
SITE PLAN

Drafted by:
D.D.H.

Approved by:
Approver

Date:
24.02.2025

Scale:
1 : 500@A2

Project/Drawing no:
PD24266 -01

Revision:
07



Accredited building practitioner: Frank Geskus -No CC246A

Figure 3: Site Plan

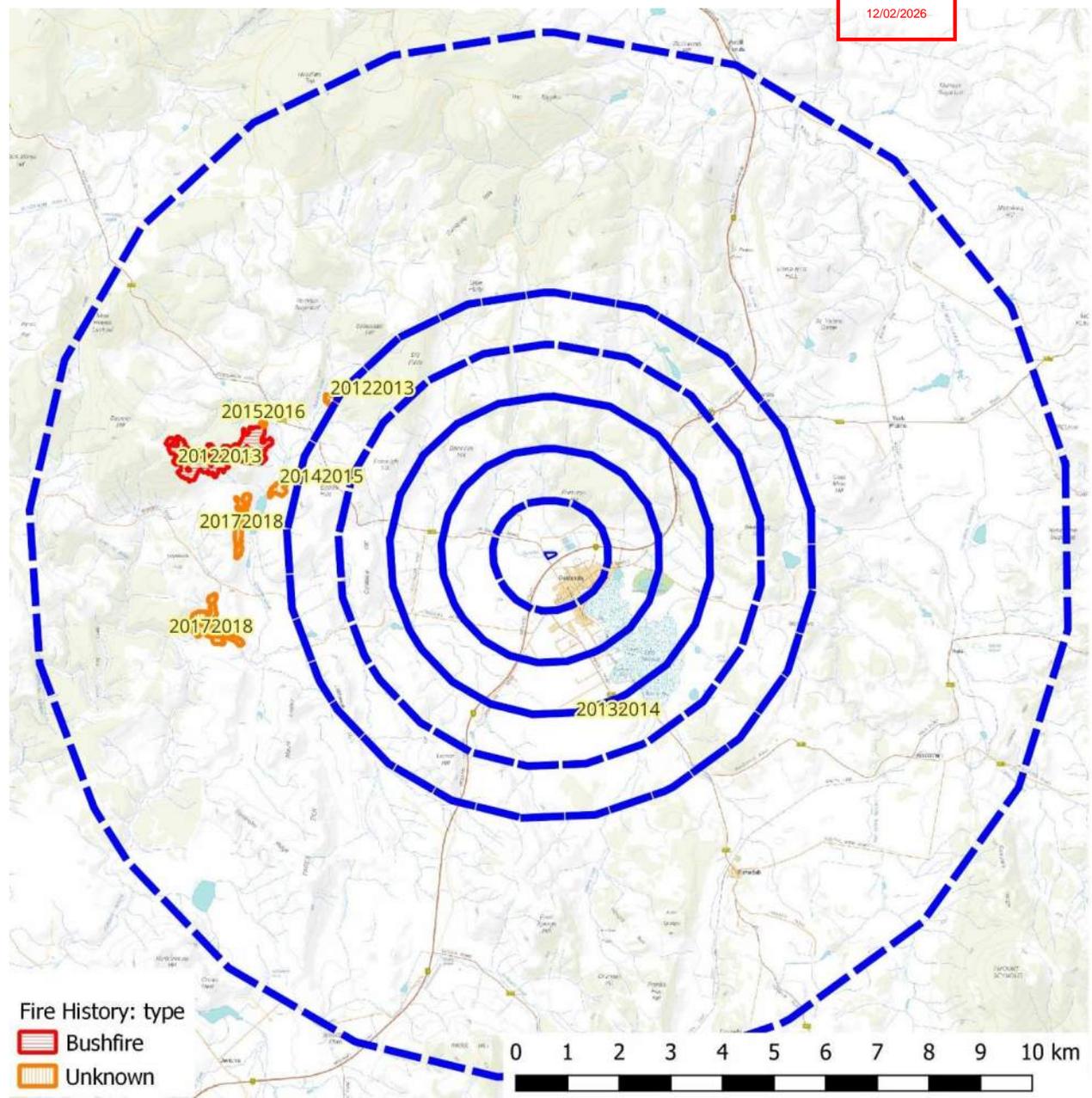
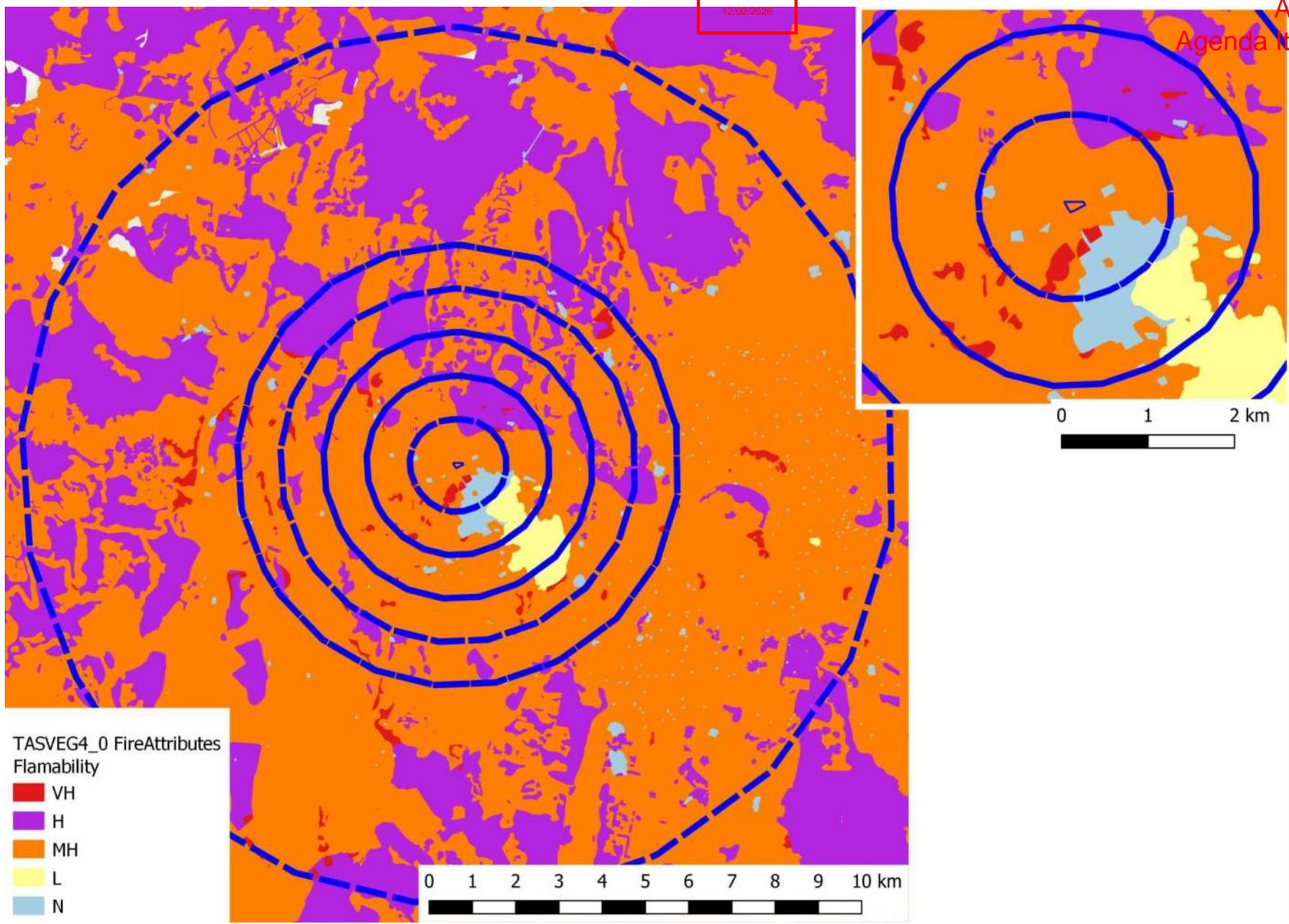


Figure 4: fire history

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TASVEG4_0 FireAttributes
Flamability
VH
H
MH
L
N

Figure 5: TasVeg Flammability



RE: BFP#105 - 20 Interlaken Road, Oatlands - TFS Endorsement of Emergency Management Strategy |

Bushfire Practitioner

to me, Bushfire ▾

9:52 AM

Good Morning Scott,

Thank you for providing the updated strategy.

The Emergency Management Strategy attached, Version 2, dated 14/04/2025 is endorsed by the TFS.

Please feel free to contact us if you have any questions.

Best regards,

Alex Duncan

Planning & Assessment Officer

Bushfire Risk Unit

Tasmania Fire Service

Service | Professionalism | Integrity | Consideration

Cnr Argyle and Melville Streets | GPO Box 308 Hobart Tasmania 7001

Mob: 0458 385 873

alex.duncan@fire.tas.gov.au | www.fire.tas.gov.au

I live and work on unceded palawa land



Bushfire Hazard Management Report: Hazardous Use

Report for: Tas Petroleum

Property Location: 20 Interlaken Road, Oatlands

Prepared by: Scott Livingston
Livingston Natural Resource Services

Date: 28th April 2025

Version: 1





Summary

Client: Tas Petroleum

**Property
identification:**

Current zoning: Rural, *Tasmanian Planning Scheme- Southern Midlands*

20 Interlaken Road Oatlands, CT 44807/6, PID 5841861

Proposal: Development of an unmanned Service Station is proposed on existing title 44807/6 at 20 Interlaken Road, Oatlands.

**Assessment
by:**

Scott Livingston,
Master Environmental Management,
Natural Resource Management Consultant.
Accredited Person under part 4A of the Fire Service Act 1979:
Accreditation # BFP-105.

Version	Date	Notes
1	28/4/2025	

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LIMITATIONS

This report only deals with potential bushfire risk and does not consider any other potential statutory or planning requirements. This report classifies type of vegetation at time of inspection and cannot be relied upon for future development or changes in vegetation of assessed area.

DESCRIPTION

Development of an unmanned Service Station is proposed on existing title 44807/6 at 20 Interlaken Road, Oatlands. The area is mapped as bushfire prone in planning scheme overlays.

The proposed Service Station will have fuel storage capacity that exceeds manifest quantities and will be considered a Hazardous Use and Bushfire Code C13.5.2 applies. Fuel tanks will have a combined capacity of 223,200L. The site will have 8 bowsers, under a canopy, a control room and amenities building and 3 EVC charging bays.

The lot is currently vacant, grassland with some hardstand storage area on the proposed site. Areas outside the proposed hardstand a likely to remain grassland.

The lot has frontage to Interlaken Road along its western boundary. Surrounding land is grassland, with the Midlands Highway 100m to the southeast.

See Appendix 1 for maps and site plan, Appendix 2 for photos

HAZARDOUS USE

Fuel tanks will have a combined capacity of 223,200L (to be confirmed)

Underground tanks (2)

	Type	tank (L)	Total (L)
petrol	91	66000	107,000
	95	20500	
	98	20500	
diesel	Std	71200	116,200

223,200

EMERGENCY PLANNING

An Emergency Strategy for the proposal has been endorsed by Tasmania Fire Service (28/4/2025). A Bushfire Emergency Plan (BEMP) endorsed by Tasmania Fire Service must be in place prior to storage of hazardous materials in excess of manifest quantities on the site. The BEMP must be updated annually and supplied to TFS while ever the site remains mapped as Bushfire Prone Area in planning Schem overlays.

BAL AND RISK ASSESSMENT

The following table shows distances to classified vegetation from, fuel bowsers, fuel tanks and control room.

Facility		Northeast	Southeast	Southwest	Northwest
Bowsers & fuel tanks	Vegetation within 100m	0- 27m low threat (hardstand) 27-100m grassland	0- 25m low threat (hardstand) 25-100m grassland	0- 24m low threat (hardstand / road) 24-100m grassland	0- 22m low threat (hardstand) 22-100m grassland
	Slope (degrees, over 100m)	upslope/flat	Downslope 0-5°	upslope/flat	upslope/flat
	BAL Rating at boundary	BAL 12.5	BAL 12.5	BAL 12.5	BAL 12.5
	Overall BAL Rating	BAL 12.5			

ACCESS

The proposed service station will have sufficient hardstand for large truck turning with entry /exit points to Interlaken Road, no additional access is required to water supply points. No specific access requirements apply in addition to the proposed hard stand area.

WATER SUPPLY

The site is not currently serviced by a reticulated water supply; the nearest hydrants are within the Oatlands township around 500m by road from the site. A 225mm reticulation main runs 90m to the south of the lot and may provide a suitable water supply subject to negotiations with TasWater. If no reticulated supply is available a static supply will be required within a 90m hoselay of all areas of the canopy, tank farm and amenities building.

CONCLUSIONS

Development of an unmanned Service Station is proposed on existing title 44807/6 at 20 Interlaken Road, Oatlands. The area is mapped as bushfire prone in planning scheme overlays. The proposed fuel storage will exceed manifest quantities and be a hazardous use under Bushfire Prone Areas Code and Directors Determination.

A hazard management area for BAL 12.5 rating of the bowsers, fuel tanks is well inside the proposed hardstand area and no additional fuel management is required. The hardstand provides compliant access. A static water supply will be required prior to storage of fuel in excess of manifest quantities unless a hydrant can be provided within 120m hose lay of all buildings and tanks.

An Emergency Plan endorsed by Tasmania Fire Service must be in place prior to storage of hazardous materials in excess of manifest quantities. The plan must be updated annually and supplied to TFS while the Bushfire Prone Areas Overlay includes the site.

REFERENCES

Australian Building Codes Board. (2015). *National Construction Code - Volume 2*. ABCB.

Director of Building Control. Director's Determination for Bushfire Hazard Areas v1.2 2024
Department of Premier and Cabinet (Tasmania). (2017). *Building Act 2016*.

Standards Australia Limited. (2018). *AS 3959-2018 Construction of buildings in bushfire prone areas*

Tasmanian Planning Commission. *Tasmanian Planning Scheme- Southern Midlands*

APPENDIX 1 – MAPS



Figure 1: Location



Figure 2: Aerial Image

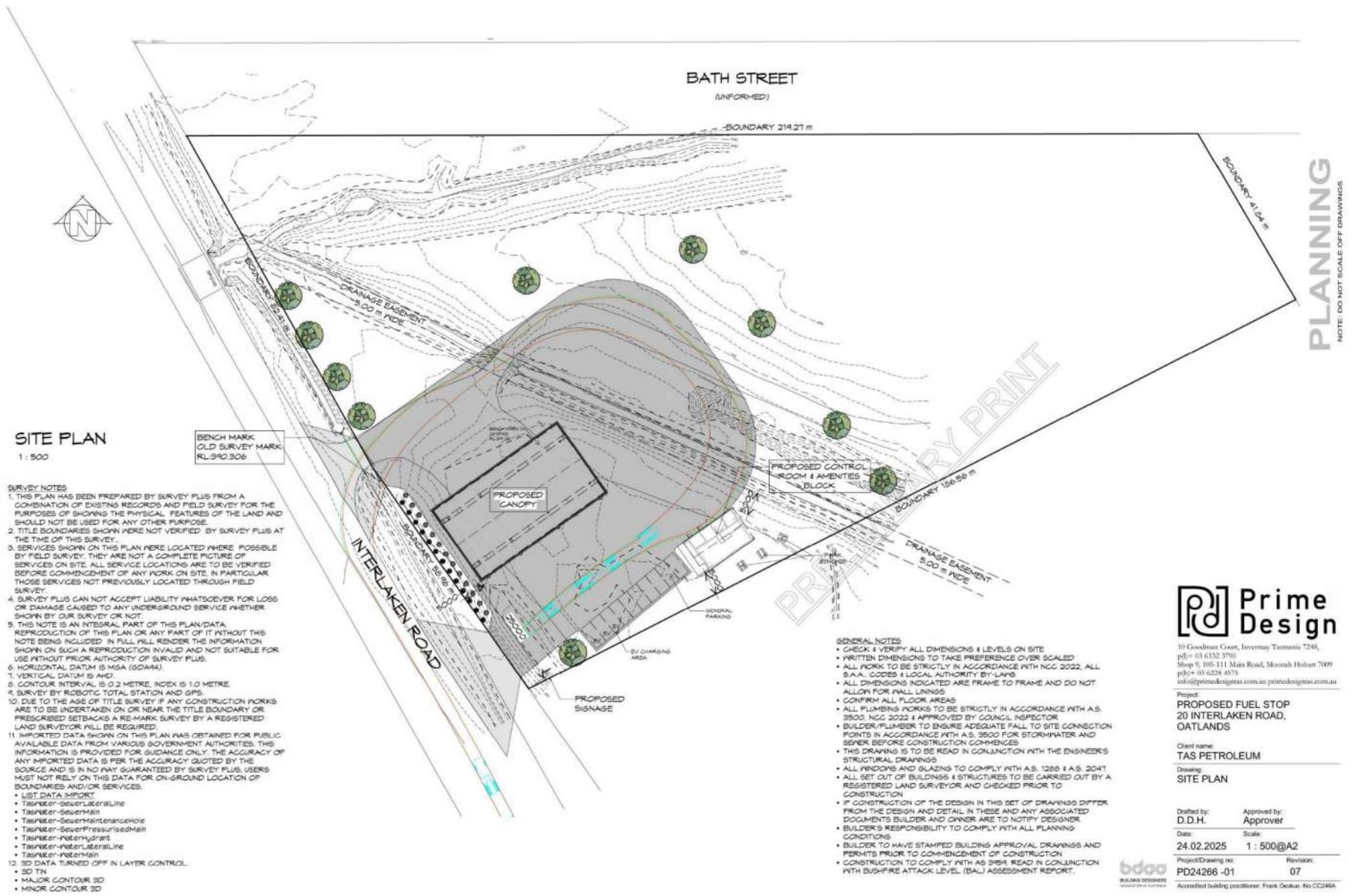


Figure 3: Site Plan

APPENDIX 2 – PHOTOS



Figure 4: north across site from Interlaken Road



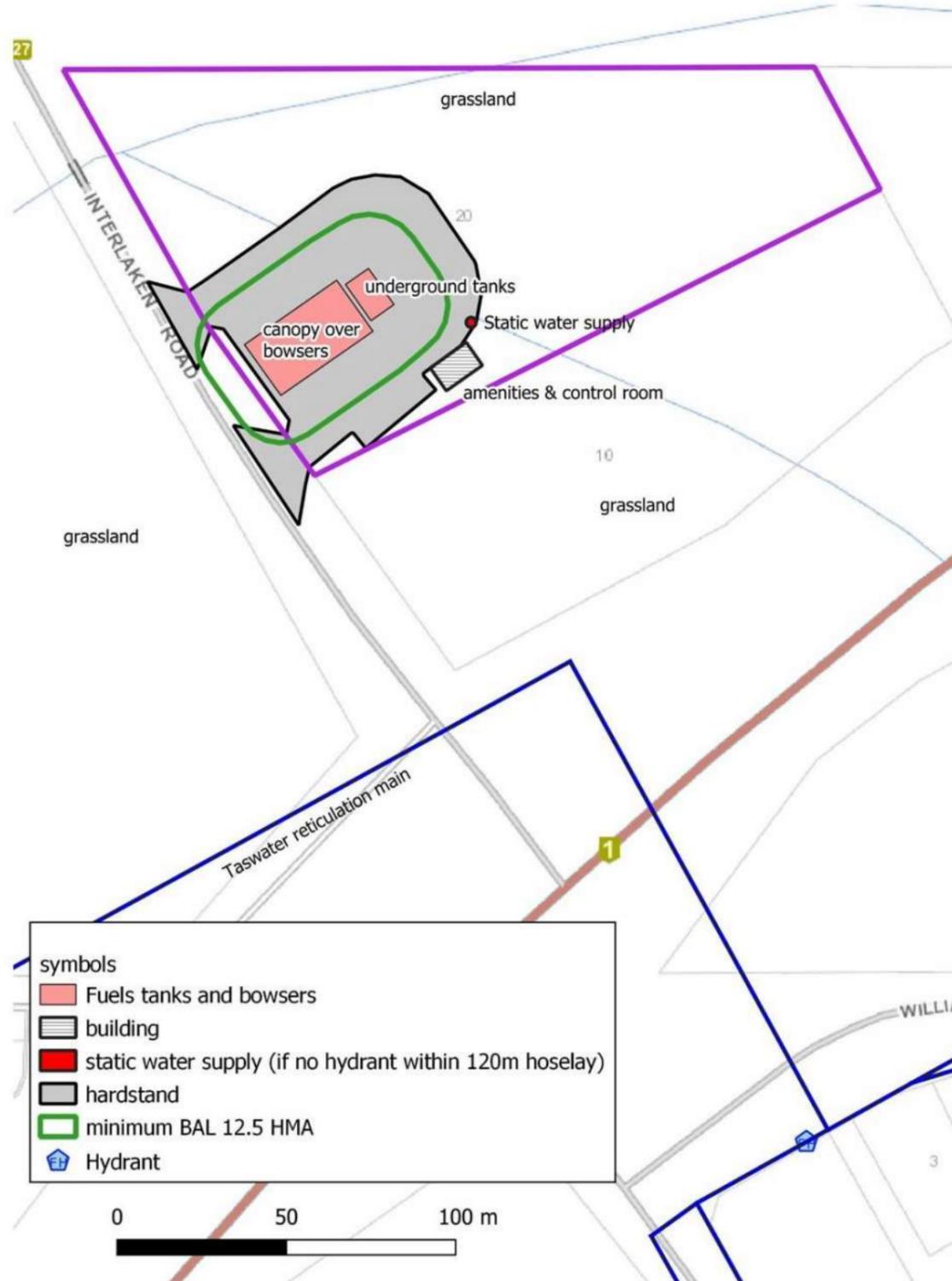
Figure 5: east east across site from Interlaken Road

Bushfire Hazard Management Plan:

Construction: BAL 12.5

Buildings in Bushfire Prone Area to be built in accordance with the Building Code of Australia and Australian Standard AS3959.
Building setbacks / BAL ratings apply to habitable buildings (Class 1, 2 3, 8 or 9) and class 10a buildings within 6m of a habitable building.

Proposed Development	Service Station (Hazardous Use)
Address	20 Interlaken Road, Oatlands
Developer	Tas Petroleum
Site Plan	Prime Design
CT	44807/6.
PID	5841861



following must be installed/ compliant prior to commencement of hazardous use (fuel storage in excess of manifest quantities.)

Hazard management, all areas of shown as hardstand to be low threat
"low threat" includes maintained lawns (<100mm in height) gardens and orchards. To be maintained in perpetuity.

Static water supply of 10,000L if no new hydrant within 120m hose lay of bowsers and tanks. see report for detail

Remote Fuels shut off -see report for detail
Emergency Plan endorsed by Tasmania Fire Service

This BHMP has been prepared to satisfy the requirements of the Tasmanian Planning Scheme Southern Midlands Bushfire Prone Areas Code & Director's Determination for Bushfire Hazard Areas v1.2 2024

This plan should be read in conjunction with the Bushfire Management Report 20 Interlaken Road Oatlands. Livingston Natural Resource Services.

Scott Livingston
Accreditation: BFP- 105: 1, 2, 3A, 3B, 3C
Date 28/4/2025

SRL25/21H



BUSHFIRE-PRONE AREAS CODE

CERTIFICATE¹ UNDER S51(2)(d) LAND USE PLANNING AND APPROVALS ACT 1993

1. Land to which certificate applies

The subject site includes property that is proposed for use and development and includes all properties upon which works are proposed for bushfire protection purposes.

Street address:

20 Interlaken Road, Oatlands

Certificate of Title / PID:

CT 44807/6, PID 5841861

2. Proposed Use or Development

Description of proposed Use and Development:

Unmanned Service Station (Hazardous Use)

Applicable Planning Scheme:

Tasmanian Planning Scheme -Southern Midlands

3. Documents relied upon

This certificate relates to the following documents:

Title	Author	Date	Version
Bushfire Hazard Management Report 20 Interlaken Road, Oatlands	Scott Livingston	28/4/2025	1
Bushfire Hazard Management Plan 20 Interlaken Road, Oatlands	Scott Livingston	28/4/2025	1
Bushfire Emergency Strategy 20 Interlaken Road, Oatlands	Scott Livingston	14/4/2025	2
Site Plan	Prime Design	24/2/2025	07

4. Nature of Certificate

The following requirements are applicable to the proposed use and development:

<input type="checkbox"/>	E1.4 / C13.4 – Use or development exempt from this Code	
	Compliance test	Compliance Requirement

¹ This document is the approved form of certification for this purpose and must not be altered from its original form.

<input type="checkbox"/>	E1.4(a) / C13.4.1(a)	Insufficient increase in risk
--------------------------	----------------------	-------------------------------

<input type="checkbox"/> E1.5.1 / C13.5.1 – Vulnerable Uses		
	Acceptable Solution	Compliance Requirement
<input type="checkbox"/>	E1.5.1 P1 / C13.5.1 P1	<i>Planning authority discretion required. A proposal cannot be certified as compliant with P1.</i>
<input type="checkbox"/>	E1.5.1 A2 / C13.5.1 A2	Emergency management strategy
<input type="checkbox"/>	E1.5.1 A3 / C13.5.1 A2	Bushfire hazard management plan

<input checked="" type="checkbox"/> E1.5.2 / C13.5.2 – Hazardous Uses		
	Acceptable Solution	Compliance Requirement
<input type="checkbox"/>	E1.5.2 P1 / C13.5.2 P1	<i>Planning authority discretion required. A proposal cannot be certified as compliant with P1.</i>
<input checked="" type="checkbox"/>	E1.5.2 A2 / C13.5.2 A2	Emergency management strategy endorsed by TFS 28/4/2025
<input checked="" type="checkbox"/>	E1.5.2 A3 / C13.5.2 A3	Bushfire hazard management plan

<input type="checkbox"/> E1.6.1 / C13.6.1 Subdivision: Provision of hazard management areas		
	Acceptable Solution	Compliance Requirement
<input type="checkbox"/>	E1.6.1 P1 / C13.6.1 P1	<i>Planning authority discretion required. A proposal cannot be certified as compliant with P1.</i>
<input type="checkbox"/>	E1.6.1 A1 (a) / C13.6.1 A1(a)	Insufficient increase in risk
<input type="checkbox"/>	E1.6.1 A1 (b) / C13.6.1 A1(b)	Provides BAL-19 for all lots (including any lot designated as ‘balance’)
<input type="checkbox"/>	E1.6.1 A1(c) / C13.6.1 A1(c)	Consent for Part 5 Agreement

<input type="checkbox"/> E1.6.2 / C13.6.2 Subdivision: Public and fire fighting access		
	Acceptable Solution	Compliance Requirement
<input type="checkbox"/>	E1.6.2 P1 / C13.6.2 P1	Planning authority discretion required. A proposal cannot be certified as compliant with P1.
<input type="checkbox"/>	E1.6.2 A1 (a) / C13.6.2 A1 (a)	Insufficient increase in risk

<input type="checkbox"/>	E1.6.2 A1 (b) / C13.6.2 A1 (b)	Access complies with relevant Tables
--------------------------	--------------------------------	--------------------------------------

E1.6.3 / C13.1.6.3 Subdivision: Provision of water supply for fire fighting purposes		
	Acceptable Solution	Compliance Requirement
<input type="checkbox"/>	E1.6.3 A1 (a) / C13.6.3 A1 (a)	Insufficient increase in risk
<input type="checkbox"/>	E1.6.3 A1 (b) / C13.6.3 A1 (b)	Reticulated water supply complies with relevant Table
<input type="checkbox"/>	E1.6.3 A1 (c) / C13.6.3 A1 (c)	Water supply consistent with the objective,
<input type="checkbox"/>	E1.6.3 A2 (a) / C13.6.3 A2 (a)	Insufficient increase in risk
<input checked="" type="checkbox"/>	E1.6.3 A2 (b) / C13.6.3 A2 (b)	Static water supply complies with relevant
<input type="checkbox"/>	E1.6.3 A2 (c) / C13.6.3 A2 (c)	Static water supply consistent with the objective

5. Bushfire Hazard Practitioner

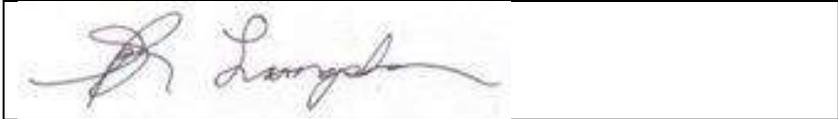
Name:	Scott Livingston	Phone No:	0438 951 021
Postal Address:	PO Box 178, Orford, 7190	Email Address:	scottlivingston.lnrs@gmail.com
Accreditation No:	BFP – 105	Scope:	1, 2, 3A, 3B, 3C

6. Certification

I certify that in accordance with the authority given under Part 4A of the *Fire Service Act 1979* that the proposed use and development:

- Is exempt from the requirement Bushfire-Prone Areas Code because, having regard to the objective of all applicable standards in the Code, there is considered to be an insufficient increase in risk to the use or development from bushfire to warrant any specific bushfire protection measures, or
- The Bushfire Hazard Management Plan/s identified in Section 3 of this certificate is/are in accordance with the Chief Officer’s requirements and compliant with the relevant **Acceptable Solutions** identified in Section 4 of this Certificate.

Signed:
certifier



Name: Scott Livingston **Date:** 28/4/2025

Certificate Number: SRL 25/21H

(for Practitioner Use only)



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Appendix E

Stormwater Management Plan



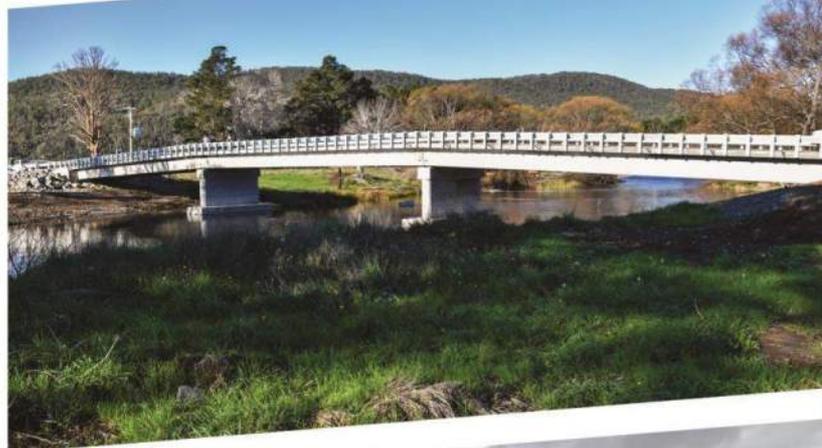
PO Box 58 South HOBART, Tasmania 7004

0438376840 evan@e3planning.com.au

Stormwater Management Report

Proposed Fuel Stop
20 Interlaken Road, Oatlands
(CT 44807/6)

Prepared for: Tas Petroleum
Project No: 251035
Document No: 251035-RPT-002
Issue No: 01
Revision No: -



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Launceston, TAS 7250

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Project	Tas Petroleum – Proposed Fuel Stop – 20 Interlaken Rd, Oatlands
Report Title	Stormwater Management Report
Project No	251035
Document ID	251035-RPT-002
File Path	R:\Projects\2025\251000 Civil\251035-Interlaken Fuel Stop-PR-OA\4 Internal\Reports\SMR
Client	Tas Petroleum

Record of Report

Issue	Reason	Revision	Date	Prepared By	Approved By
01	Client Issue	-	30/10/2025	JS	RJ

Distribution of Report

Company	Name & Address	Contact	Copies
Tas Petroleum C/- Prime Design	Drew Den Hartog 10 Goodman Court, Invermay, TAS, 7248	E: drew@primedesigntas.com.au Ph: 03 6332 3790	1 (elec)

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1. INTRODUCTION

Rare Innovation Pty Ltd (Rare) have been engaged to undertake engineering design and provide a stormwater management report for the proposed fuel stop development located at 20 Interlaken Road, Oatlands. The purpose of this report is to provide supporting information regarding stormwater management for the site for development approval in conjunction with Rare project drawings 251035-CDA series and the recommendations within the Flood Hazard Report 251035-RPT-001 FHR.

2. EXISTING SITE

2.1. Location

The proposed development is located on a single existing land title at 20 Interlaken Road, Oatlands (Property ID: 5841861, Title Reference: 44807/6). The site is surrounded by existing undeveloped rural land on all sides and is bounded by Interlaken Road on the south-western side as well as an undeveloped road reserve on the northern side. The Oatlands Sewage Treatment Plant is located approximately 200m to the north-east.

2.2. Existing Stormwater System

The site is currently un-serviced by a reticulated stormwater system/connection. Two existing drainage courses currently extend through the site as shown in Figure 1 including Dulverton Rivulet and an artificial drainage course which connect near the north-western corner of the site before discharging beneath an existing bridge under Interlaken Road.

An existing vehicular crossing over the artificial drainage course is present with a DN600 culvert. The existing vehicular access onto Interlaken Road contains an existing DN300 culvert beneath as part of a roadside drain.

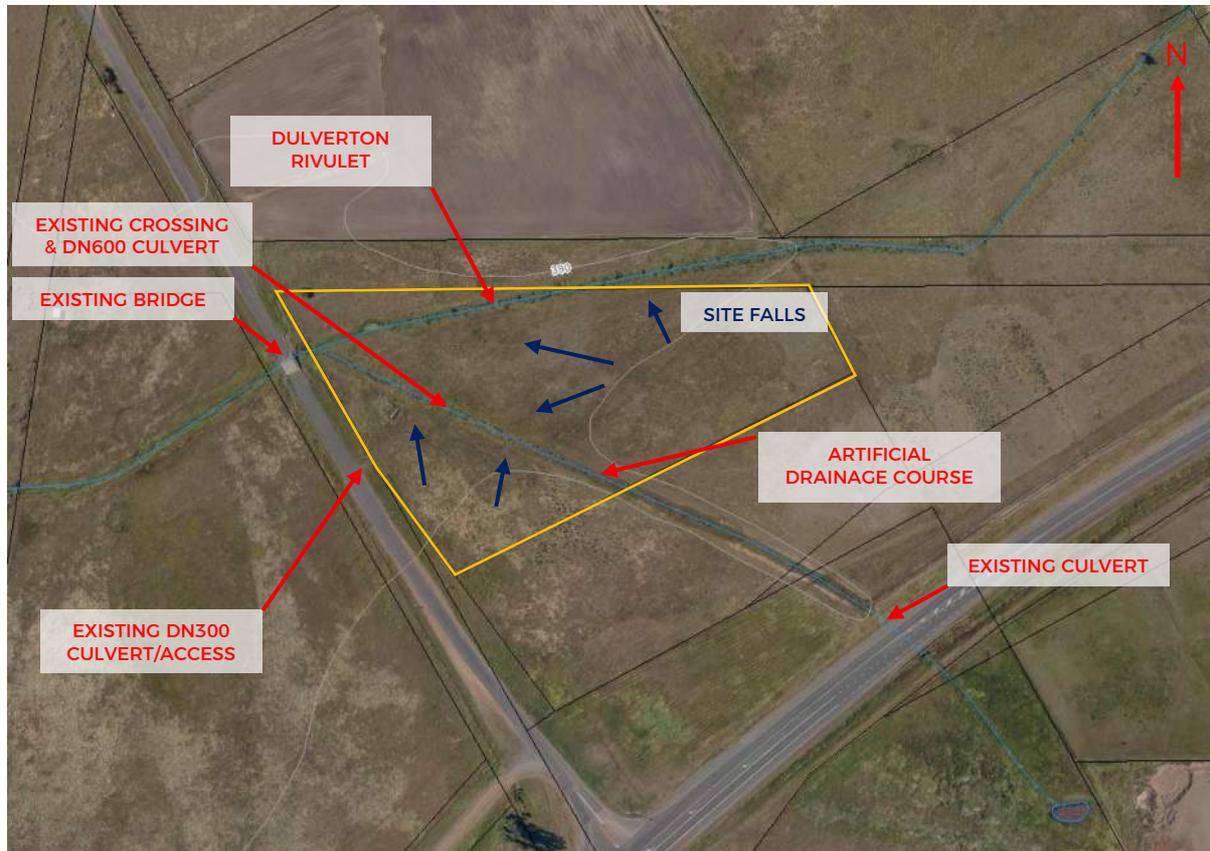


Figure 1 – Existing Site and Features

3. PROPOSED WORKS

3.1. Proposed Development

The proposed development is for the construction of a new fuel stop area which includes a roof canopy over bunded re-fuelling areas and an amenities/control room building. A plan of the proposed development is shown in Figure 2.

Works include a new hardstand area with localised falls to new drainage infrastructure, including within the re-fuelling areas to ensure bunding for potential fuel spills. Additional parking areas and charging for electric vehicles is also provided. Finished site levels are to be above the nominal flood level of RL 390.6m AHD as identified in the Flood Hazard Report except where required to match into existing access levels.

Access to the site is currently provided by a single existing gravel crossover. It is proposed to upgrade the existing crossover to concrete including widening and construct an additional crossover, including drainage culverts.

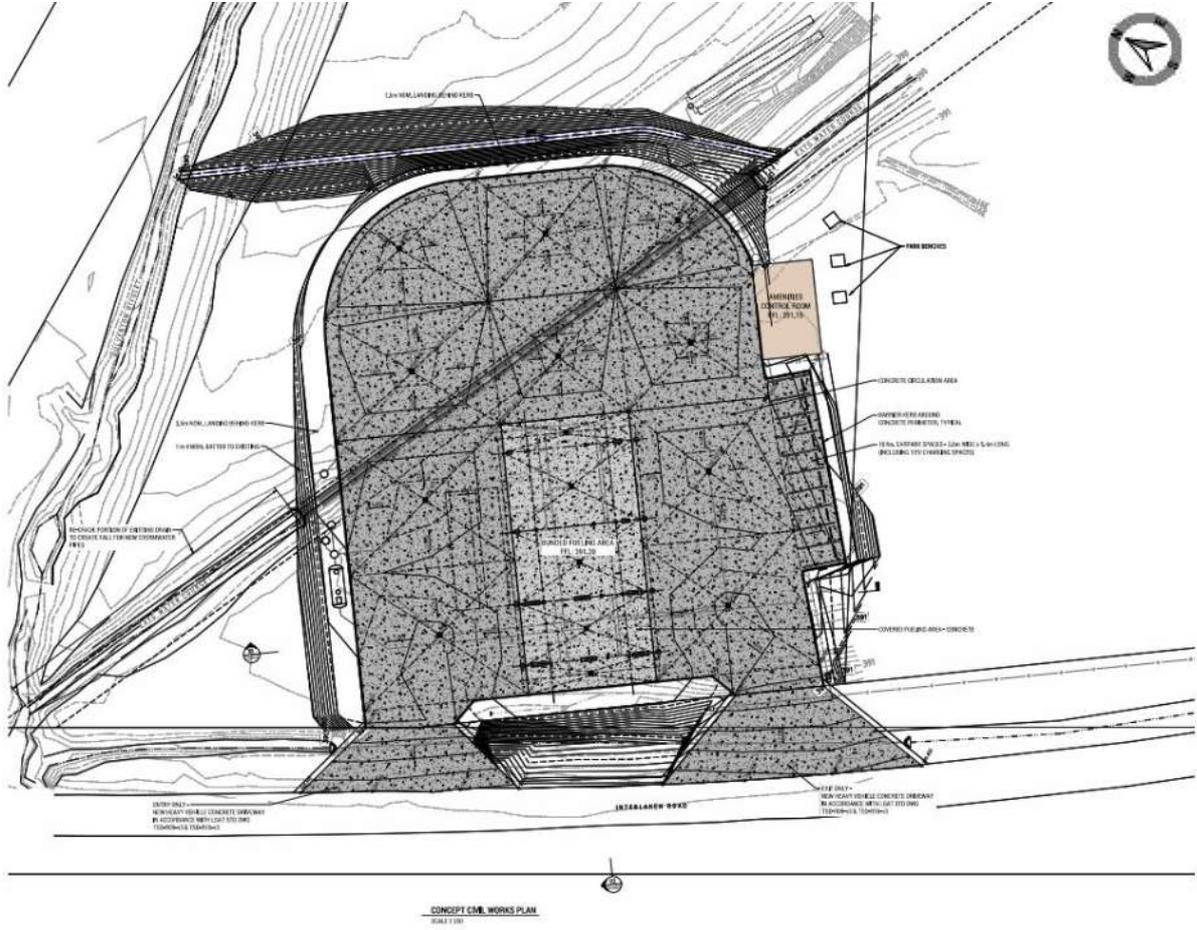


Figure 2 – Proposed Site Works

3.2. Proposed Stormwater Works

The proposed stormwater works are as shown in Figure 3.

The proposed works include the construction of a hardstand area over the existing artificial drainage course. It is proposed to demolish the existing DN600 culvert under the vehicular crossing within this open drain and to construct a twin DN600 culvert extending the full length of the hardstand area. The twin culvert is proposed in order to improve on the unobstructed capacity of the existing drain. In accordance with the recommendations of the Flood Hazard Report, it is also proposed to construct a high-level overflow drain to ensure that flows above the capacity of the new culvert can safely bypass the developed fuel stop area. The proposed high-level overflow drain is to connect back into Dulverton Rivulet and the combined capacity of the culvert and overflow is to maintain the capacity of the existing water course. Final sizing of drains is to be undertaken during detailed design to meet requirements.

It is noted that the existing artificial drainage course has zero fall across most of its length with only some minor fall developing at the downstream end prior to discharging into Dulverton Rivulet. To ensure that the proposed twin culvert achieves some minor fall across its length, it is proposed to regrade the downstream portion of the existing artificial drainage course and to continue this grade through the culvert. This will



ensure that low level (frequent event) flows can flow through the site without creating standing water as is currently occurring.

The proposed hardstand area is to be drained by falling to localised pits throughout which are to ultimately discharge into the existing watercourse. Due to site level constraints and existing open drain levels, the stormwater system is to be divided into two separate discharges to the open drain on either side of the proposed twin culvert.

The existing (to be upgraded) and proposed site accesses are to be constructed with DN300 culverts to match existing culvert sizing.

The reticulated minor stormwater network is to be sized to accommodate flows for a minimum 5% AEP storm event with overflows above system capacity to discharge to the existing open drains.

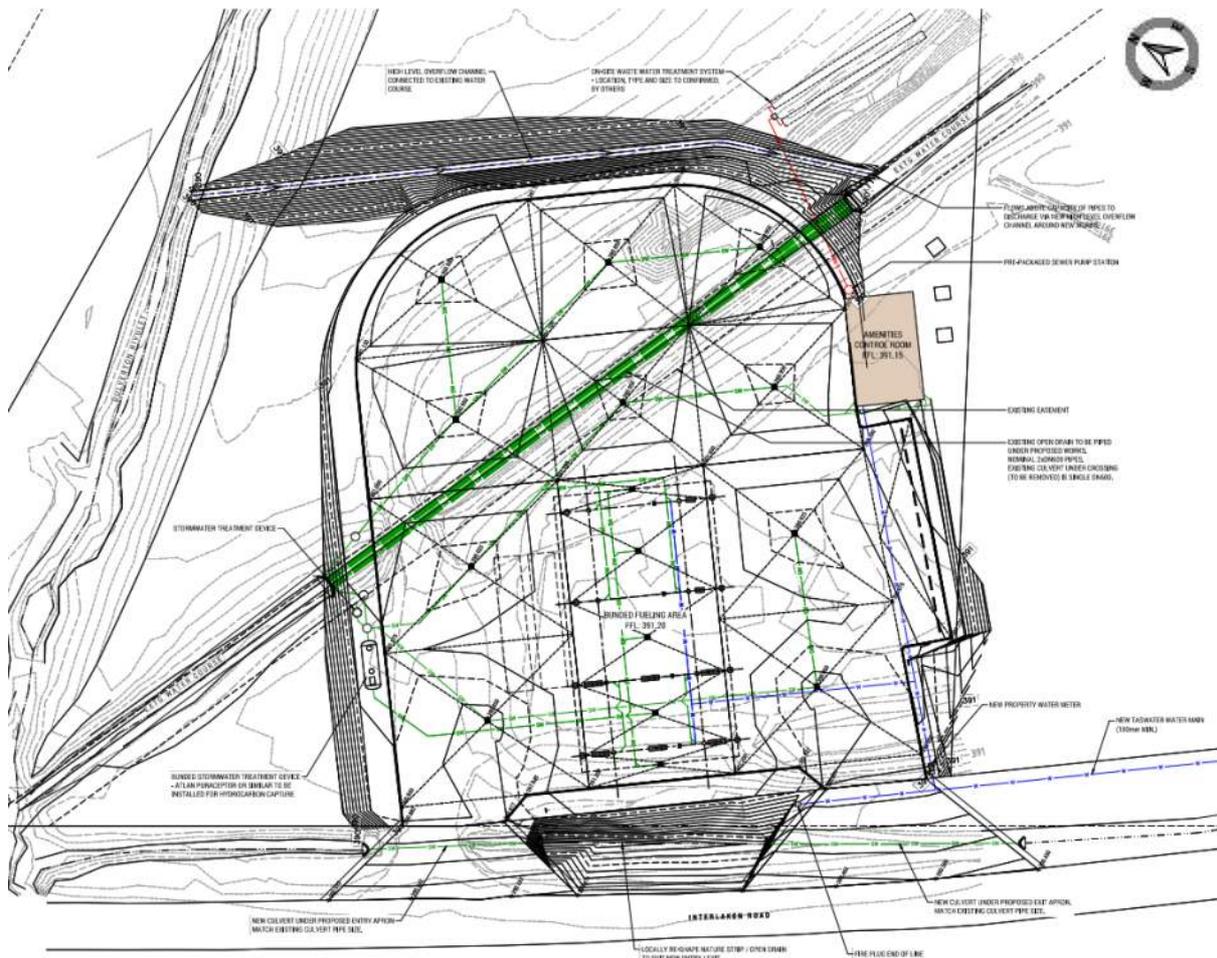


Figure 3 – Proposed Stormwater System

3.3. Proposed Stormwater Treatment Works

Due to the presence of the potential for hydrocarbon spills from re-fuelling activities, it is proposed to install a stormwater treatment/oil water separator device to capture hydrocarbons and prevent discharge to the receiving environment. A SPEL Purceptor or similar device is proposed as has been used in previous similar developments with sizing to be determined by SPEL (or other manufacturer) in accordance with relevant standards/guidelines. Sufficient available space is present to enable installation of varying sized systems as required.

The re-fuelling areas as well as tank fill points are to be suitably bunded to ensure containment of hydrocarbon spills, with all stormwater from bunded areas to be directed to the proposed treatment system.

Final grades, sizes and configurations of treatment systems and the reticulated stormwater network are to be confirmed during detailed design.



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Appendix F

Flood Hazard Management Report



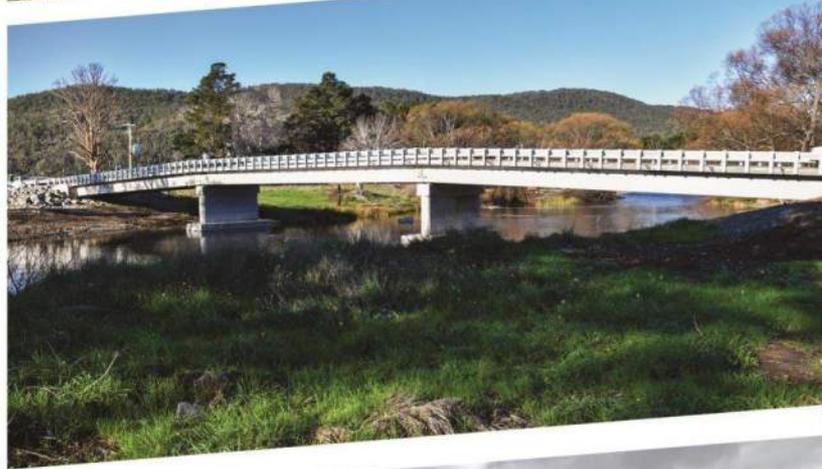
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Flood Hazard Report

Proposed Fuel Stop
20 Interlaken Road, Oatlands
(CT 44807/6)

Prepared for: Tas Petroleum
Project No: 251035
Document No: 251035-RPT-001
Issue No: 01
Revision No: -



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Project	Tas Petroleum – Proposed Fuel Stop – 20 Interlaken Rd, Oatlands
Report Title	Flood Hazard Report
Project No	251035
Document ID	251035-RPT-001
File Path	R:\Projects\2025\251000 Civil\251035-Interlaken Fuel Stop-PR-OA\4 Internal\Reports\Flood
Client	Tas Petroleum

Record of Report					
Issue	Reason	Revision	Date	Prepared By	Approved By
01	Client Issue	-	30/10/25	JWS	RJ

Distribution of Report			
Company	Name & Address	Contact	Copies
Tas Petroleum C/- Prime Design	Drew Den Hartog 10 Goodman Court, Invermay, TAS, 7248	E: drew@primedesigntas.com.au Ph: 03 6332 3790	1 (elec)

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1. INTRODUCTION

Rare Innovation have been engaged to provide a flood hazard report for the proposed new fuel stop located at 20 Interlaken Road, Oatlands. The purpose of this flood hazard report is to undertake a desktop review and analysis of the existing site and proposed works to assess the likely hazard to structures and persons due to flooding and to address the relevant sections of the Tasmanian Planning Scheme. This report has been prepared based on available data for the site at the time of investigation.

2. SITE EVALUATION

2.1. Location & Property Details

The proposed development is located on a single existing land title at 20 Interlaken Road, Oatlands (Property ID: 5841861, Title Reference: 44807/6) as shown in Figure 1. The site is currently zoned as Rural and is surrounded by existing undeveloped rural land on all sides. The site is also bounded by Interlaken Road on the south-western side as well as an undeveloped road reserve on the northern side. The Oatlands Sewage Treatment Plant is located approximately 200m to the north-east.



Figure 1 – Existing Site Aerial Image (LISTmap)

2.2. Land Description

The site is currently largely undeveloped however a portion of the site currently contains gravel laydown areas with material stockpiles and general vehicle access tracks. Access to the site is currently provided by an existing gravel crossover onto Interlaken Road along the south-western boundary.

The site is currently un-serviced by reticulated stormwater, water or sewer. Two existing watercourses currently run through the site as shown in Figure 2. Dulverton Rivulet is an existing creek which flows from the north-east and cuts through the site along the northern boundary before flowing beneath Interlaken Road via an existing bridge near the north-western corner of the site. An existing artificial drainage watercourse also crosses the site from the south-east (from beneath Midland Hwy) and discharges into Dulverton Rivulet just prior to exiting the site near the existing bridge. An existing DN300 stormwater culvert is present beneath the existing site crossover access within a roadside open drain, and an existing DN600 culvert is present beneath an existing vehicular crossing over the existing open drain in the centre of site.

The site's topography is varied with changes in grades typically in the range of 0-5%. Multiple localised high points are present with falls generally directed towards the existing watercourses. It is noted that the existing grades within the two watercourses are generally flat, with Dulverton Rivulet being approximately 0.5% and the existing open drain being flat with no fall for most of its length with standing water present. No flowing water was observed in either watercourse during a site visit on 9th July 2025.

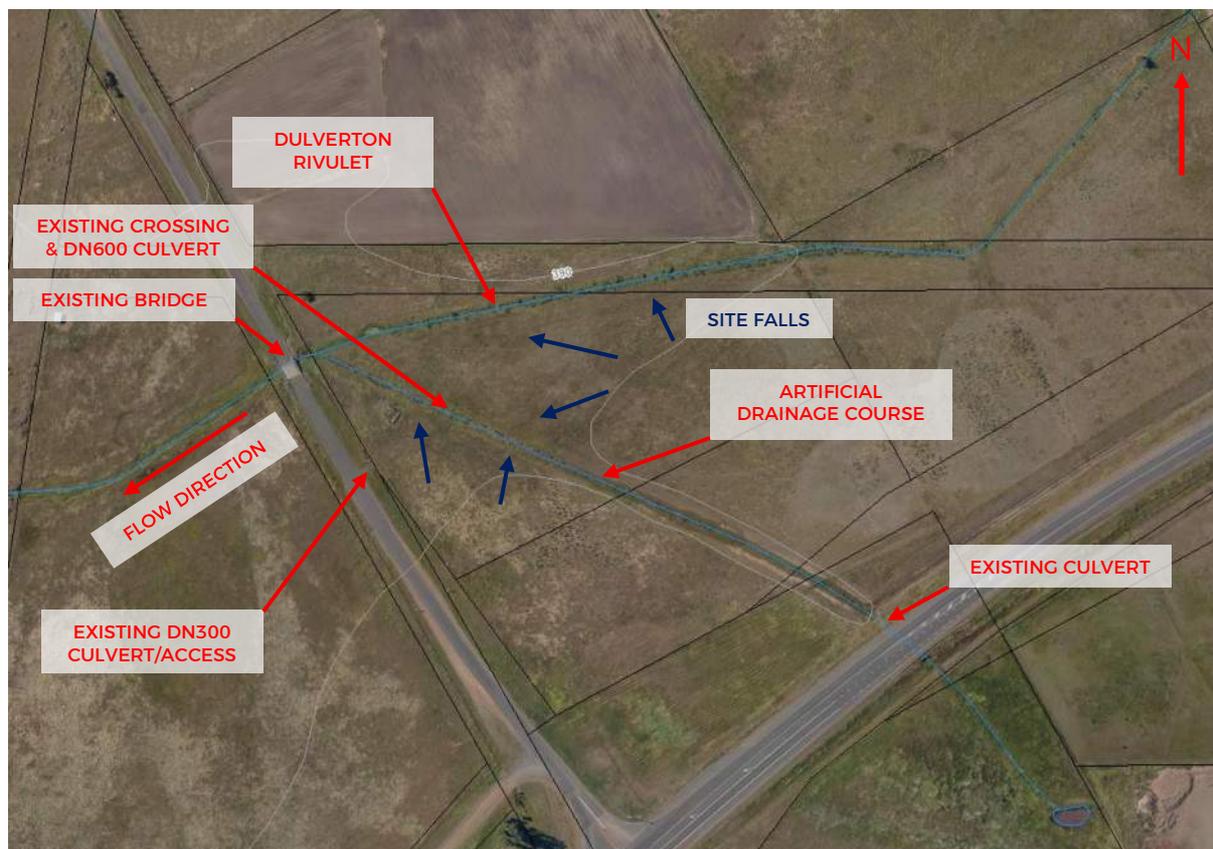


Figure 2 – Site Topography & Features

2.3. Proposed Works

The proposed development is for the construction of a new fuel stop and rest area with access from Interlaken Road as shown in Figure 3. This consists of new bunded refuelling areas, areas of trafficable concrete hardstand, amenities/control room building and associated services infrastructure.

The finished surface levels of the hardstand area feature varied cut and fill to level the use area, with levels predominantly proposed at or above existing surface levels, and above the levels of the existing watercourses. Additional drainage infrastructure is proposed including a new culvert beneath the additional site vehicular access to match the existing pipe size of the previous access. Drainage works are also proposed for the existing open drain watercourse where the new hardstand area crosses over. This includes demolition of the existing single DN600 culvert and construction of a new increased capacity DN600 twin culvert arrangement which extends the full length of the hardstand area where the open drain is proposed to be filled. An additional high-level overflow provision in the form of an open drain is proposed to direct flows above the capacity of the twin culvert around the site and connects back into Dulverton Rivulet as per recommendations made in Section 3.3 of this report. Improvements to the grading of the existing drain are also proposed as part of the new culvert construction.

Further details on the proposed works are provided in Rare's *Stormwater Management Report 251035-RPT-002 SMR*.

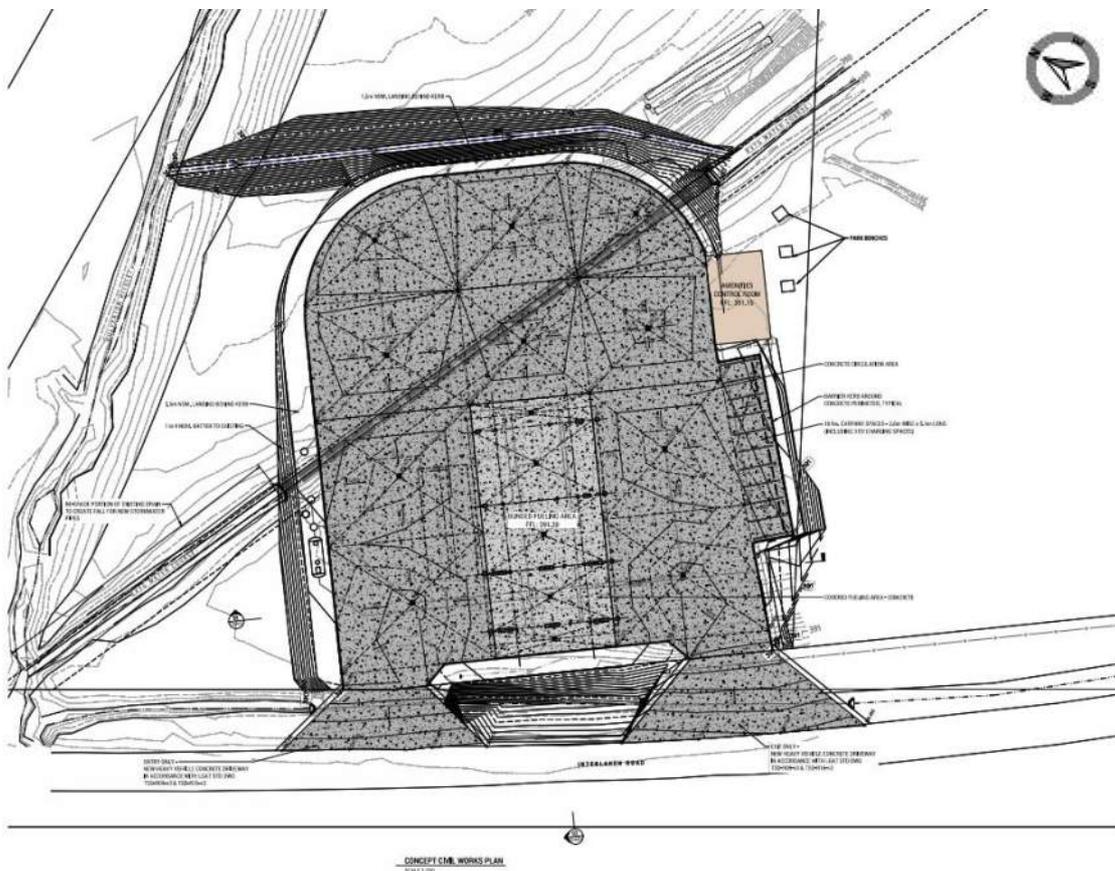


Figure 3 – Proposed Site Plan

3. FLOOD RISK

3.1. Existing Flood Risk

LISTmap identifies that the site falls within mapped areas of the Statewide Flood Hazard layer. Mapping for a range of annual exceedance probabilities (AEP) is provided within the layer including 2%, 1% and 0.5% AEP. This report will focus on the 1% AEP layers as a standard practice for overland flow assessment for development and as requested by Council. Each AEP layer has individual maps for flood depth, flood velocity, flood hazard and water level. Figures 4-6 show relevant mapped flood layers for the 1% AEP event relative to the development location.

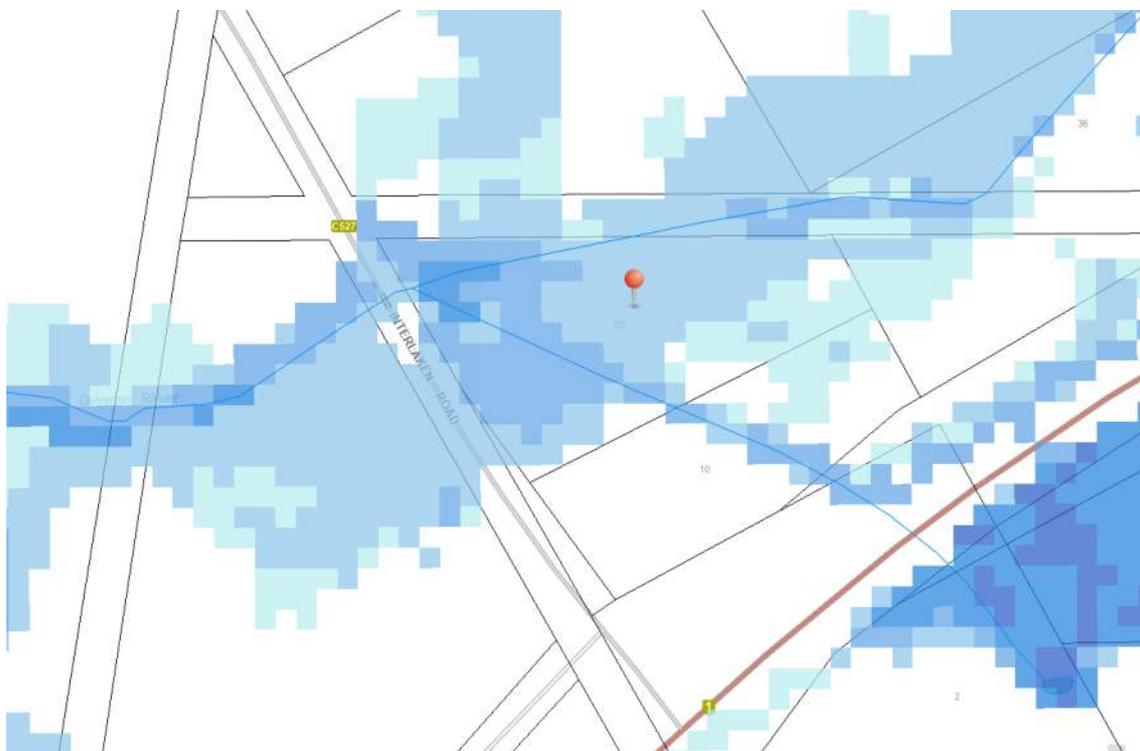


Figure 4 – Statewide Flood Hazard – 1% AEP Depth (Overland + Riverine)

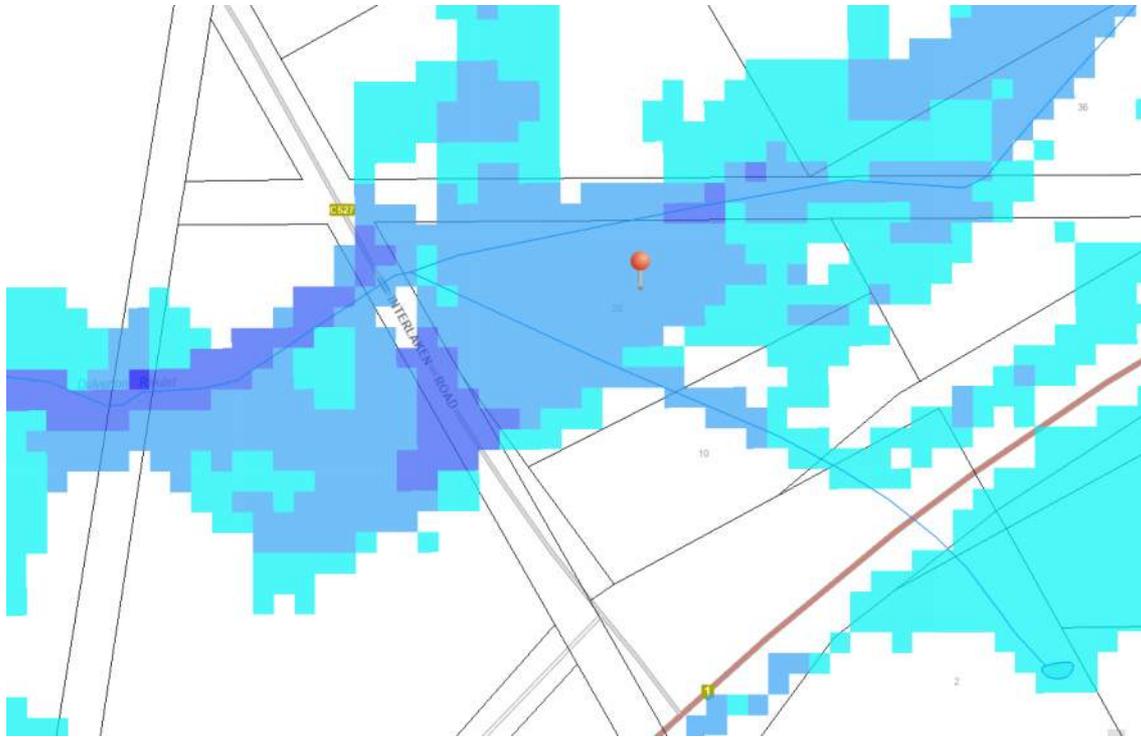


Figure 5 – Statewide Flood Hazard – 1% AEP Velocity (Overland + Riverine)

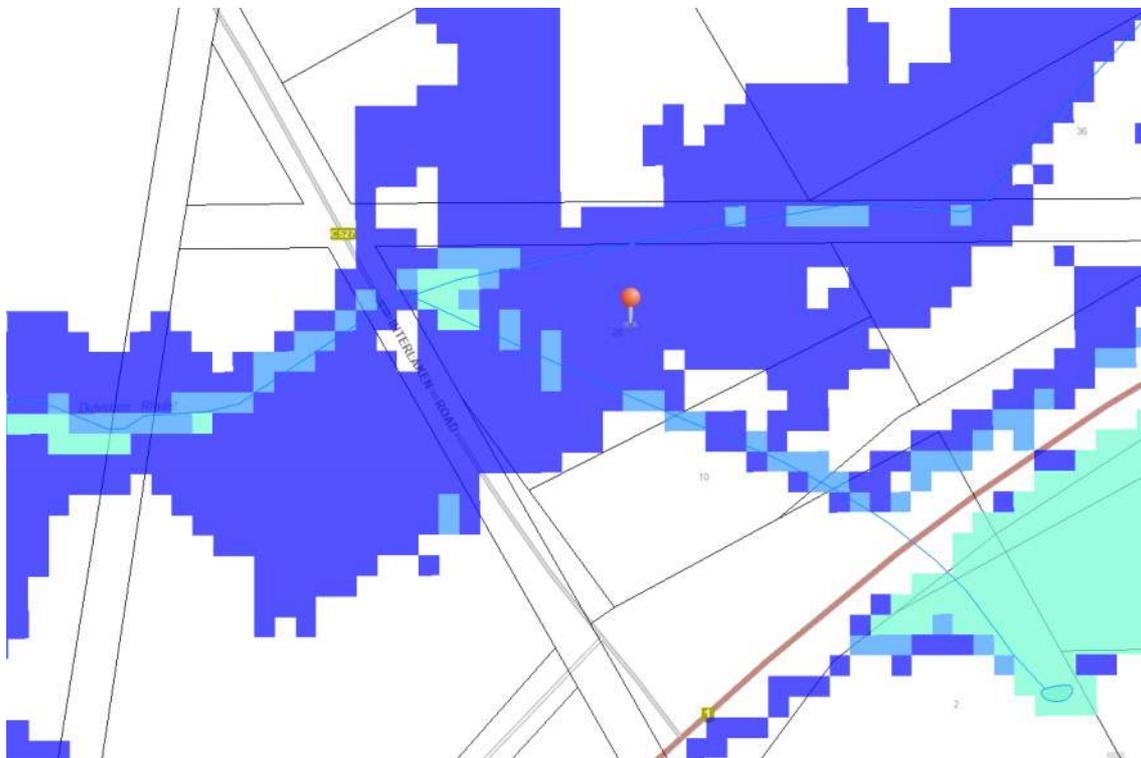


Figure 6 – Statewide Flood Hazard – 1% AEP Hazard (Overland + Riverine)

The flood mapping of hazard rating as shown in Figure 6 identifies flood hazard vulnerability classifications in accordance with *Figure 6.7.9 Australian Rainfall and Runoff Book 6 Chapter 7* and as shown in Figure 7. Classifications of hazard vulnerability are determined as a function of flood depth and flow velocity, with resulting classifications of H1 to H6 given.

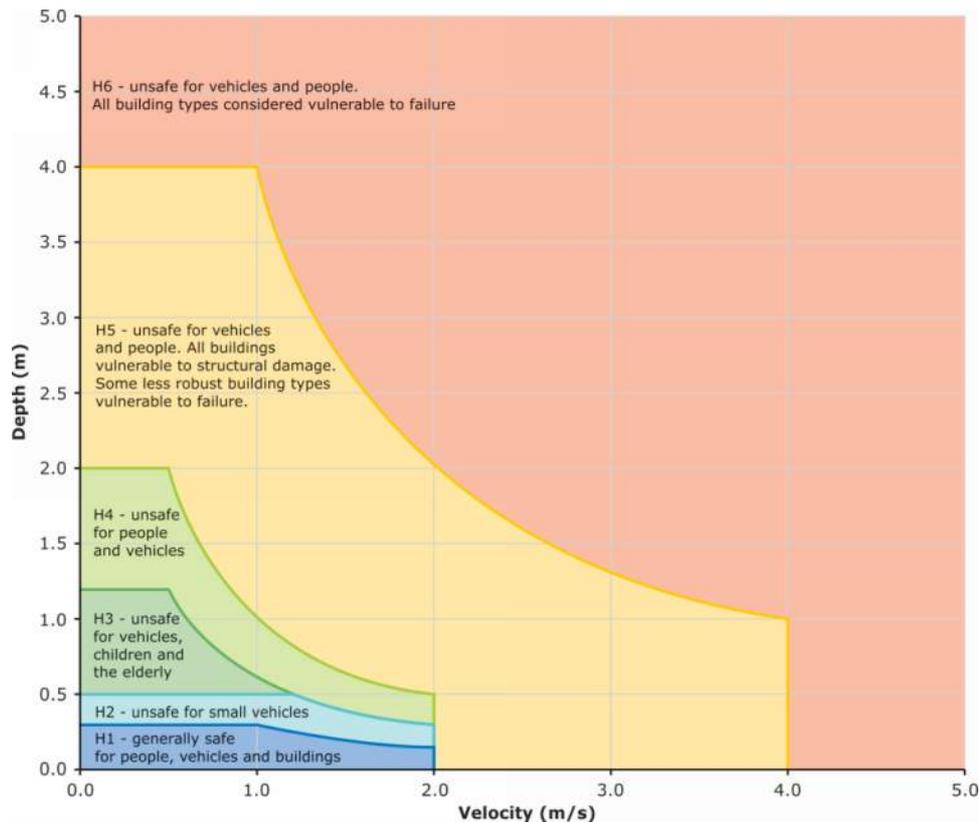


Figure 7 – Combined Flood Hazard Curves (AR&R 2019)

Identified hazard vulnerability classifications for the site range from H1 – ‘generally safe for vehicles, people and buildings’ (dark blue squares) to H3 – ‘unsafe for vehicles, children and the elderly’ (light aqua squares) as shown in Figure 6. The large majority of the site is shown to be covered by areas of H1 classification with areas of higher hazard H2 and H3 shown to be confined to portions of the existing drains, and in particular at the confluence of Dulverton Rivulet and the artificial drainage course where flows are restricted by the existing bridge.

Flood levels indicated by the referenced flood mapping are at approximately RL 390.6m AHD in the vicinity of the artificial drainage course and proposed development location.

Comparison of flood levels provided by the referenced flood mapping and local survey data indicates some discrepancy between the two, with local surveyed levels typically higher than predicted flood levels.

Calibration reports provided by SES for the referenced flood modelling indicate lidar data was used where available for the generation of the digital terrain model used in the flood modelling.

Comparisons against freely available 2019 lidar surface elevation data indicates that levels within the lower end of Dulverton Rivulet and the artificial drainage course are similar to locally surveyed levels (± 100 -200mm typically). However levels within other areas of the site, particularly the undeveloped areas in the eastern portion of the site vary to an increased degree with a level difference of approximately 1.5m between local survey (higher) and lidar (lower). Surface topographies are also varied between the two, with lidar topography visually representing the mapped flood paths closer than survey topography.

The local survey data with higher levels towards the eastern side of the site indicates the presence of a small hill across much of this area (also visually confirmed on site) that has likely not been accurately accounted for in the referenced flood mapping with use of lidar data. The flood mapping indicates largely even sheet flows of low hazard classification H1 crossing most of the site from the north-east, assuming lower surface levels than actually present. The actual presence of elevated levels through this area (+1.5m approx.) is likely to prevent such a flow regime across this portion of the site and is expected to concentrate flows more-so within the confines of Dulverton Rivulet or further north.

Due to the above, it is more likely that flood waters passing through the site and Dulverton Rivulet from the north-east will be confined further north than shown in Figures 6 & 8. Flood waters are expected to continue to backflow and pond in the vicinity of the confluence of the two drainage courses due to the flow restriction at the bridge, however flows (in particular, flow velocities) through the proposed development location are expected to be less than as shown in the referenced flood mapping and as shown in Figure 8 (flood mapping overlaid onto proposed site plan). Given the hazard classifications in these areas outside of the drainage courses are predominantly of low H1 classification, hazards are expected to remain at this level or lower.

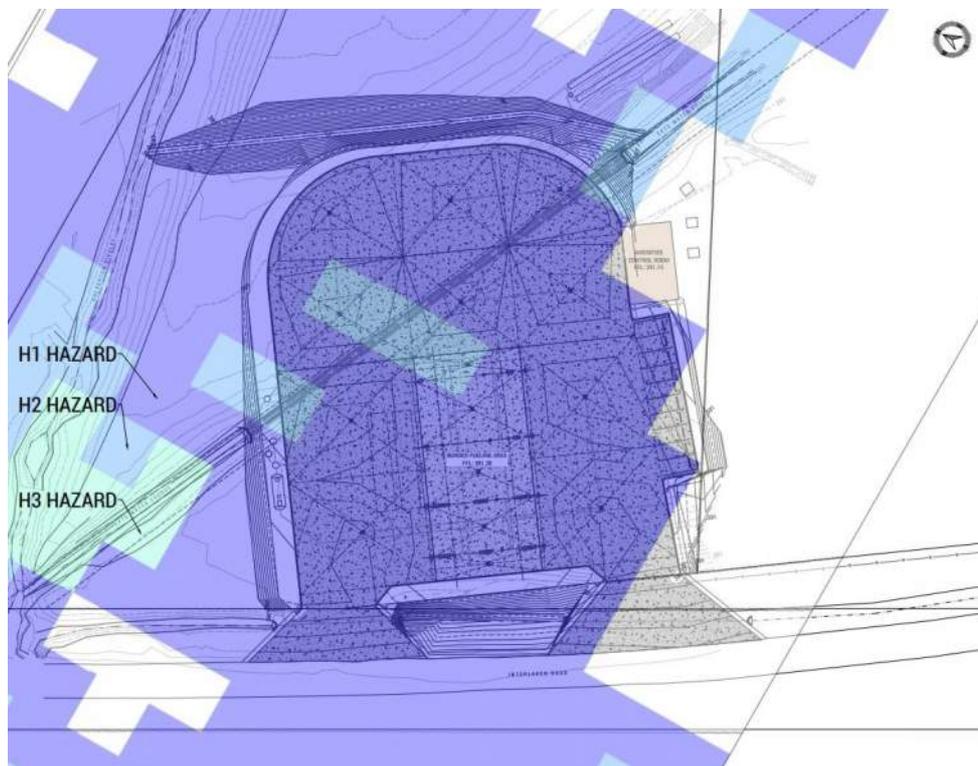


Figure 8 – 1% AEP Hazard Flood Mapping Overlay on Site Plan

3.2. Development Effects & Building Works

Proposed development works predominantly consist of site levelling to create a new hardstand area for vehicular access and fuel sales with generally minor cut/fill (<1m) for most of the site outside of filling of open drains. Proposed structures are considered relatively minor and include an overhead roof canopy for the fuel servicing area and an amenities block/control room.

The finished surface level of the hardstand areas varies but typically ranges between RL 391m - 391.2m AHD with some lower levels to match into existing accesses. Works are proposed within the mapped flood areas of typically H1 hazard as shown in Figure 8 with an approximate expected flood level of RL 390.6m AHD. It is expected that the proposed hardstand area will be located above the expected flood level except at the site's accesses where levels are lower at approximately RL 390.5m AHD.

As discussed previously in Section 3.1, the mapped flooding over the proposed development location is expected to be concentrated further north due to actual topography and flows passing through the site are expected to be minimal. Instead, flows across the proposed hardstand area are expected to be as a result of backflowing/ponding from downstream flow restrictions at the bridge. With finished surface levels located above the expected flood levels, backflow/ponding is expected to be confined to the areas adjacent the rivulet on the north-western side of the hardstand area and within culverts/drains. The finished works proposed remain clear of the existing rivulet and open drain confluence area where most flood ponding is expected.

The proposed works are not located within the main flood paths through Dulverton Rivulet and are expected to be less impacted by ponding waters than as shown in the referenced flood mapping. As such, flood waters are not expected to be impacted or redirected onto adjacent land other than the potential alteration of the footprint of ponding waters within the site.

The proposed works include the replacement of the existing DN600 culvert across the artificial drainage course in the centre of the site with a new twin DN600 culvert arrangement beneath the proposed hardstand area. This is expected to improve drainage capacity within the water course, however it is unlikely that the existing DN600 culvert was included in the high-level flood modelling and so an obstruction within this drain was likely not accounted for. It is recommended to assist in preventing flows above the capacity of the proposed culvert from prematurely flooding into the site, that an open drain or similar high-level overflow be maintained around the site to more safely direct overflows and should achieve a similar capacity between the over-flow and culvert to that of the existing open drain. It is noted that the proposed works includes such an overflow.

It is noted that the grade of the existing artificial drainage course is flat with no fall for a large portion of its length. In order to achieve a minimal grade for the proposed culverts, re-grading of the downstream section of this open drain where adjoining Dulverton Rivulet is proposed. This is expected to assist in drainage of flows in low and frequent rain events, however will have minimal impact in large events.

Building works including the fuelling area canopy and amenities block are expected to have minimal to no impact on obstruction of flood waters. The proposed hardstand area is expected to remain clear of flooding as described previously, however should flooding still occur as shown in Figure 6, the level of risk being H1 –

'generally safe for vehicles, people and buildings' indicates that there will be low risk of damage to these structures.

The proposed development will increase the amount of impervious area on the site which will increase stormwater runoff. The site is expected to have a low time of concentration for peak flows (likely 5 mins) from developed areas. The existing catchment generating flood waters through Dulverton Rivulet is of significant size in comparison to the site with times of concentration for peak flows likely to be of a magnitude of hours. It is highly unlikely that peak flows generated from the site will coincide with peak flows from the upper catchment. Due to the existing flow restriction at the bridge close to the site, it is expected to be beneficial to allow peak flows from the site to discharge as early as possible without detention so as not to force flows to coincide with the peak flood and worsen flood effects at the bridge.

3.3. Recommendations

The following actions are recommended to ensure risk levels are maintained at sufficiently low levels for the proposed development:

- It is recommended to construct finished surface levels above the mapped flood level of RL 390.6m AHD except where required to match into existing road accesses.
- It is recommended to construct a high-level overflow drain to maintain capacities of the existing artificial drainage course above the capacity of the proposed culverts to minimise risk of overflows passing through the hardstand areas.

3.4. Residual Risk

Given the above recommendations, the residual risk to the development due to flooding in the 1% AEP event as informed by the Statewide Flood Hazard mapping is considered low.

Flood mapping for the existing site indicates generally low levels of H1 hazard within the proposed development areas. As discussed in Section 3.1, discrepancies exist between the flood mapping topography and actual site topography. As a result, hazards are expected to be lower than those indicated across the proposed development area with flows likely to be concentrated further north following actual site topography.

With site accesses connecting to Interlaken Road located at existing levels below the expected flood level of 390.6m AHD, the mapped flood hazards of H1 across these accesses is expected to remain. This is not expected to prevent egress from the site due to low depths/velocities for such a flood hazard.

The proposed site use is for unmanned vehicle fuel sales which indicates intermittent use by the public with generally short stays. Electric vehicle charging infrastructure may present the longest duration use of the site of up to 45 mins typically for charging vehicles. Sufficient visual warning of rising flood levels outside of the site would be expected prior to any inundation of the development area occurring. If flooding occurs, no evacuation of workers will be required due to no permanent presence with the site using automated sales facilities. Safe egress from the site is available through Interlaken Road to the Midland Highway.

4. FLOOD CODE ASSESSMENT

Tasmanian Planning Scheme
C12.0 Flood Prone Areas Hazard Code
C12.6 Development Standards for Buildings and Works

C12.6.1 Buildings and works within a flood-prone hazard area

<p>Objective:</p> <p>That:</p> <p>(a) building and works within a flood-prone hazard area can achieve and maintain a tolerable risk from flood; and</p> <p>(b) buildings and works do not increase the risk from flood to adjacent land and public infrastructure</p>	
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>No Acceptable Solution</p>	<p>P1.1</p> <p>Buildings and works within a flood-prone hazard area must achieve and maintain a tolerable risk from a flood, having regard to:</p> <p>(a) the type, form, scale and intended duration of the development;</p> <p>(b) whether any increase in the level of risk from flood requires any specific hazard reduction or protection measures;</p> <p>(c) any advice from a State authority, regulated entity or a council; and</p> <p>(d) the advice contained in a flood hazard report.</p> <p>P1.2</p> <p>A flood hazard report also demonstrates that the building and works:</p> <p>(a) do not cause or contribute to flood on the site, on adjacent land or public infrastructure; and</p> <p>(b) can achieve and maintain a tolerable risk from a 1% annual exceedance probability flood event for the intended life of the use without requiring any flood protection measures</p>

Response

P1.1

As discussed in Section 3 of this report, the residual risk for the proposed development is low and considered tolerable for its intended use. Flood mapping indicates that the existing risk for the area on which the development is proposed is predominantly of low H1 hazard. Additional investigation of site topography against flood mapped topography further indicates that the existing hazard is likely less than shown. The proposed development area is also proposed to be located above the indicated 1% AEP flood level of RL 390.6m AHD and is likely to maintain a tolerable hazard for use, including the proposed canopy structure and amenities building.

Access to and from the site is expected to remain at a maximum of low hazard H1 which is generally safe for vehicles and people and will continue to allow for safe site evacuation if required. The development use is for intermittent access for vehicle fuel sales with no habitable structures or permanent employees stationed on site which poses low risk to persons who may otherwise become isolated due to flooding.

Recommended actions to maintain overflow routes for waters through the existing artificial drainage course are proposed and are not expected to increase the levels of risk of inundation from existing levels. No further flood protection measures are required aside from those recommended in Section 3.3 of this report.

P1.2

Cut and fill earthworks are proposed to predominantly level the existing land on the proposed development footprint. Existing levels within this area are largely above the indicated flood level of RL 390.6m AHD meaning minimal reshaping of land within the actual expected flood ponding area. The proposed development footprint is not located within the main flood path and is not expected to obstruct flows. Flows through the artificial drainage course are to be maintained through the provision of new culverts and high-level overflow infrastructure as recommended in Section 3.3 of this report. Flooding on adjacent land is not expected to be worsened as a result of the proposed works.

As described in the response to P1.1 and Section 3 of this report, the development is expected to maintain a tolerable risk from the 1% AEP flood without requiring flood protection measures above those recommended in Section 3.3.

5. SUMMARY

The proposed development site has been identified by Statewide Flood Hazard mapping within LISTmap as being situated within areas of 1% AEP flood hazard and therefore subject to the flood-prone areas code of the Tasmanian Planning Scheme.

The proposed development consists of the construction of new hardstand areas for vehicle fuel sales including an overhead canopy and amenities/control room building. The existing site has been assessed as having a predominantly low risk to flooding of H1 hazard across the proposed development footprint.

It is recommended that the floor level for the proposed hardstand area be located above the indicated flood level of RL 390.6m AHD and that a high-level overflow be constructed for flows within the existing artificial drainage course to bypass the site.

The proposed development works are considered acceptable in regard to flood risk for the site.

Should you have any further queries please do not hesitate to contact us.

Yours faithfully,



Jack W. Saunders
Civil Engineer
B Eng (Civil)



Attachment
Agenda Item 12.1.1

04/06/2025

Southern Midlands Council
85 Main Street
Kempton Tas 7030

RE: 20 Interlaken Road Oatlands Site Specific Qualification Tas Petroleum

Please accept this correspondence as C & M Livestock Consulting Tasmania Pty Ltd (the Owners) consent to the request to amend the Tasmanian Planning Scheme Southern Midlands Local Provision Schedule, to insert a Site-Specific Qualification to provide for a Vehicle Fuel Sales and Service as a Discretionary Use on our Property at 20 Interlaken Road Oatlands.

Regards,

Cathy A McDowell

Malcolm P Frost



Attachment
Agenda Item 12.1.1



RE: 20 Interlaken Rd TIA

From Head, Alex <Alex.Head@stategrowth.tas.gov.au>
Date Tue 12-Aug-25 10:50 AM
To evan@e3planning.com.au <evan@e3planning.com.au>

Hi Evan,

Thank you for your email.

The Department has reviewed the Traffic Impact Assessment prepared by Keith Midson and, consistent with our email of 29 July 2025, is supportive of its findings. We agree that the proposal, and its associated traffic impact on the Interlaken / Midland Highway junction, is acceptable.

Regards,

Alex Head | Traffic Engineering Technical Officer | Traffic Engineering (south)
State Roads | Department of State Growth
76 Federal St, North Hobart Tas 7000
Phone: (03) 6165 5205 | alex.head@stategrowth.tas.gov.au
www.transport.tas.gov.au

Courage to make a difference through
TEAMWORK | INTEGRITY | RESPECT | EXCELLENCE

In recognition of the deep history and culture of this island, I acknowledge and pay my respects to all Tasmanian Aboriginal people; the past, and present custodians of the Land.

From: evan@e3planning.com.au <evan@e3planning.com.au>
Sent: Monday, 11 August 2025 2:54 PM
To: Head, Alex <Alex.Head@stategrowth.tas.gov.au>
Subject: Fw: 20 Interlaken Rd TIA

You don't often get email from evan@e3planning.com.au. [Learn why this is important](#)

Hi Alex

Thank you for taking my call.

As discussed, I have spoken to Keith Midson regarding this, as he is our Traffic Consultant. It is his opinion as per the attached TIA that the proposal and its impact upon the Interlaken Midland Highway junction is acceptable. Can you please confirm that State Growth share this opinion as per your email of 29 July 2025?



Attachment
Agenda Item 12.1.1

We are about to lodge the planning scheme amendment request.

Regards

Evan

Evan Boardman B Econ, B Science, Grad Dip URP, MEIANZ
Director ph 0438376840



ENVIRONMENT
SUSTAINABLE
DEVELOPMENT
ECONOMICS

From: Admin <admin@midsontraffic.com.au>
Sent: Monday, August 11, 2025 9:21 AM
To: evan <evan@e3planning.com.au>
Subject: Fw: 20 Interlaken Rd TIA

From: Head, Alex <Alex.Head@stategrowth.tas.gov.au>
Sent: 29 July 2025 14:33
To: Admin <admin@midsontraffic.com.au>
Subject: RE: 20 Interlaken Rd TIA

Hi Keith,

Thanks for providing an updated TIA for 20 Interlaken Rd, Oatlands.

Given that your correspondence notes '*Council have raised further issues relating to the provision of an acceleration lane for northbound exiting vehicles on the highway*', it seems reasonable that this should be addressed and communicated to Council (and the Department Cc'd in).

Happy to discuss.

Regards,



Attachment
Agenda Item 12.1.1

Alex Head | Traffic Engineering Technical Officer | Traffic Engineering (south)

State Roads | Department of State Growth

76 Federal St, North Hobart Tas 7000

Phone: (03) 6165 5205 | alex.head@stategrowth.tas.gov.au

www.transport.tas.gov.au

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TEAMWORK | INTEGRITY | RESPECT | EXCELLENCE

In recognition of the deep history and culture of this island, I acknowledge and pay my respects to all Tasmanian Aboriginal people; the past, and present custodians of the Land.

From: Siale, Vili <Vili.Siale@stategrowth.tas.gov.au>
Sent: Thursday, 24 July 2025 11:17 AM
To: Admin <admin@midsontraffic.com.au>; Head, Alex <Alex.Head@stategrowth.tas.gov.au>
Subject: RE: 20 Interlaken Rd TIA

Hi Alex,

As this matter is within your territory, is it possible to provide Keith with your assessment please.

Cheers,

Vili.



Attachment
Agenda Item 12.1.1

From: Admin <admin@midsontraffic.com.au>
Sent: Wednesday, 23 July 2025 12:16 PM
To: Siale, Vili <Vili.Siale@stategrowth.tas.gov.au>
Subject: 20 Interlaken Rd TIA

Hi again Vili,

Sorry to email you while you're off sick. I prepared a TIA earlier this year for an unmanned fuel station at 20 Interlaken Rd. I recently updated the TIA to address some issues raised by State Growth - this version is attached.

Council have raised further issues relating to the provision of an acceleration lane for northbound exiting vehicles on the highway. This would be a show stopper for the development, which is not a high traffic generating development, and is also likely to attract trucks (and vehicles) that are already travelling along Interlaken Rd.

I am keen to understand if the updated TIA satisfies State Growth's matters that were raised through the DA process.

Kind regards,

Keith

Keith Midson

Director

MIDSON Traffic Pty Ltd

traffic engineering | transport planning | road safety

Ph. 0437 366 040

www.midsontraffic.com.au



Attachment
Agenda Item 12.1.1



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Owners' consent

Requests for amendments of a planning scheme or Local Provisions Schedule and applications for combined permits require owners' consent. This form must be completed if the person making the request is not the owner, or the sole owner.

The person making the request must clearly demonstrate that all owners have consented.

Please read the notes below to assist with filling in this form.

1. Request made by:

Name(s):

Email address

Contact number:

2. Site address:

Address:

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

3. Consent of registered land owner(s):

Every owner, joint or part owner of the land to which the application relates must sign this form (or a separate letter signed by each owner is to be attached).

Consent to this request for a draft amendment/and combined permit application is given by:

Registered owner :

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

Position (if applicable):

Signature: Date:

Registered owner (please print):

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

Position (if applicable):

Signature: Date:

Registered owner (please print):

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

Position (if applicable):

Signature: Date:

NOTES:

a. When is owners' consent required?

Owners' consent is required for:

- amendments to an interim planning scheme or to a Local Provisions Schedule¹; or
- combined permits and amendments².

Owners' consent must be provided before the planning authority determines to initiate, certify or prepare the amendment.

b. Who can sign as owner?

Where an owner is a natural person they must generally sign the owner's consent form personally.

Where an owner is not a natural person then the signatory must be a person with legal authority to sign, for example company director or company secretary.

If the person is acting on behalf of the owner under a legal authority, then they must identify their position, for example trustee or under a power of attorney. Documentary evidence of that authority must also be given, such as a full copy of the relevant Trust Deed, Power of Attorney, Grant of Probate; Grant of Letters of Administration; Delegation etc.

Please attach additional pages or separate written authority as required.

c. Strata title lots

Permission must be provided for any affected lot owner and for common property for land under a strata title under the *Strata Titles Act 1998*. For common property, permission can be provided in one of the following ways:

- a letter affixed with the body corporate's common seal, witnessed by at least two members of the body corporate (unless there is only one member, in which case the seal must be witnessed by that member) and which cites the date on which the body corporate or its committee of management met and resolved to give its consent to the application; or,
- the consent of each owner of each lot on the strata plan.

d. Companies

If the land is owned by a company the form is to be signed by a person with authority in accordance with the *Corporations Act 2001 (Cwth)*.

e. Associations

If the land is owned by an incorporated association the form is to be signed by a person with authority in accordance with the rules of the association.

f. Council or the Crown

If the land is owned by a council or the Crown then form is to be signed by a person authorised by the relevant council or, for Crown land, by the Minister responsible for the Crown land, or a duly authorised delegate.

The name and positions of those signing must be provided.

Effective Date: September 2021

¹ under section 33(1) of the former provisions of the *Land Use Planning and Approvals Act 1993* or section 37 of the current provisions.

² under section 43A of the former provisions or section 40T of the current provisions of the Act

Emergency Management Strategy: Fuel Stop 20 Interlaken Road, Oatlands

This document broadly identifies the proposed emergency management arrangements that will be formed into a detailed Bushfire Emergency Plan, for the purposes of a planning permit for a Hazardous Use (Fuel Stop with storage capacity in excess of manifest quantities.)

The purpose of this emergency management strategy is to demonstrate how risk to employees and visitors to the site will be managed to a tolerable level through contextualised emergency procedures. The emergency management strategy informs the more detailed Bushfire Emergency Plan, which is required at the building permit stage.

Facility

Unmanned Fuel Stop 20 Interlaken Road, Oatlands, CT 44807/6.

8 bowsers, under canopy area., and in ground fuel tanks. Amenities block & Control Room. 3 EV charging stations. The control room has no hazardous materials, and relies on electrical controls of fuel flows.

The site will be required to meet requirements of the Bushfire Hazard Management Plan including water supply and hazard management areas, in addition to fire and emergency requirements under other regulations for building approvals.

Fuel Storage Capacity (to be confirmed)

Underground tanks (2) located to the west of bowsers

	Type	tank (L)	Total (L)
petrol	91	66000	107,000
	95	20500	
	98	20500	
diesel	Std	71200	116,200

223,200

Occupancy characteristics

The site is unlikely to regularly have staff on site, occasional staff and contractors may be on site for site maintenance and deliveries. Clients are likely to have 24 hr access to refuelling facilities.

Emergency management structure and capability

Bushfire Emergency Plan to establish structure, chief, deputy wardens. This is likely to include staff not normally on site. Offsite (remote) control / shutdown of fuel pumping to be included in construction planning.

The building(s) and/or site vulnerability

- Canopy, (including bowsers, amenities / control room building and underground fuel tanks will have at least BAL 12.5 separation from bushfire prone vegetation and be maintained in perpetuity.
- Bushfire Emergency Plan, to be endorsed by Tasmania Fire Service, prior to storage of hazardous material in excess of manifest quantities.
- Compliance with AS1940, *The Storage and Handling of Flammable and Combustible Liquids*.
- Staff training as appropriate.

Possible bushfire scenarios

The fire direction under high and above fire danger ratings is likely to be from the northwest. To the northwest the land is grassland with forest around 4km. Forest occurs within 600m to the north. The surrounding land in all directions is grassland. The major bushfire threat to the facility will be from smoke and ash, with adequate onsite hazard management and building protection measures onsite ignition is considered unlikely from bushfires.

Primary and contingency bushfire safety options,

- Primary response is to evacuate for all staff / contractors / clients and closure of fuel flows to bowsers to prevent filling of vehicles, this will require remote shutdown capability.
- Shelter in place possible in the amenities building for limited numbers.

Firefighter access, firefighting services, and firefighter protection;

- Oatlands Fire Station is located at 18 Church Street Oatlands, (2 min east by vehicle)
- The site will have good access to all facilities / site.
- Insite fire equipment may include extinguishers and hose reel, noting the site will not be staffed and clients are expected to evacuate the site.
- Amenities building provides potential (limited space) onsite shelter.
- The site is not currently serviced by a reticulated water supply, a 225mm reticulation main runs 90m to the south of the lot and may provide a suitable water supply subject to negotiations with TasWater and inclusion of a new main to service the facility. If no reticulated supply is available a static supply will be required within a 90m hoselay of all areas of the canopy, tank farm and amenities building. Hydrants are available within the Oatlands township around 500m by road from the site.

Risk analysis

The most likely threat from bushfire to the site under high bushfire conditions is from fires in the forested areas to the northwest and north of the site, these are likely to carry ash and

under severe and higher rating burning embers. The nearest forest area in those directions is 600m from the site. The site may also be impacted by grass fire, all hazardous use points, bowsers and tanks will have in well in excess of BAL 12.5 separation from grassland.

Fires in those areas should give adequate warning times before becoming extensive enough to impact the site. Under known bushfire activity in the area Fuel Stop operators should monitor the situation and the site. Maintenance of grounds and building should minimise the potential fuel/ignition points on site.

Figure 4: fire history, shows the recorded fire history of the area, this shows no fire history to the northeast and south within 10km of the site, several fires have occurred in the 5-10km to the west.

Figure 5: TasVeg Flammability, shows surrounding land to be moderate high flammability.

Bushfire Protection Measures

- Remote shutdown of fuel supply to bowsers to be included in Construction plans.
- Fire Protection requirements to meet or exceed standard for Service Stations.
- Consider installation of a closed sign visible to clients at site entries to be activated if bowsers are shutdown.
- Bushfire Action Plan and offsite refuge map to be displayed at payment point and amenities building.

Proposed Emergency Plan:

- High or greater Bushfire Forecast or known fire in the vicinity: Chief Warden to monitor incident closely, brief staff on situation, review Emergency Plan.
- Ability to close fuel supply to bowsers.
- Consider installation of a closed sign visible to clients at site entries to be activated if
- Cancel fuel deliveries.
- Clients to evacuate at own recognisance.

Potential Offsite Refuge:

- Oatlands, 2 minutes east by vehicle.

Scott Livingston
BFP #105

14/4/2025, Version 2

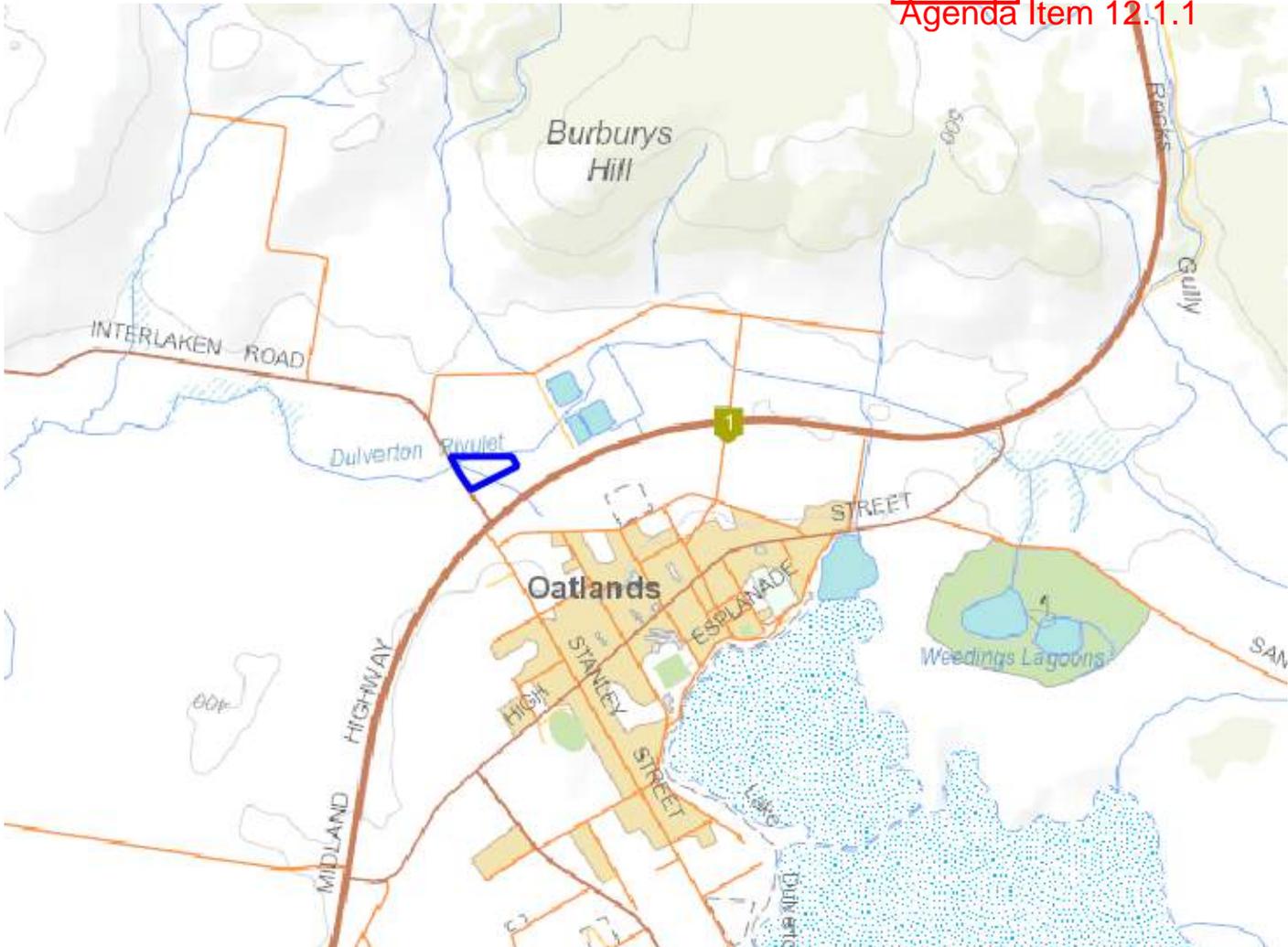
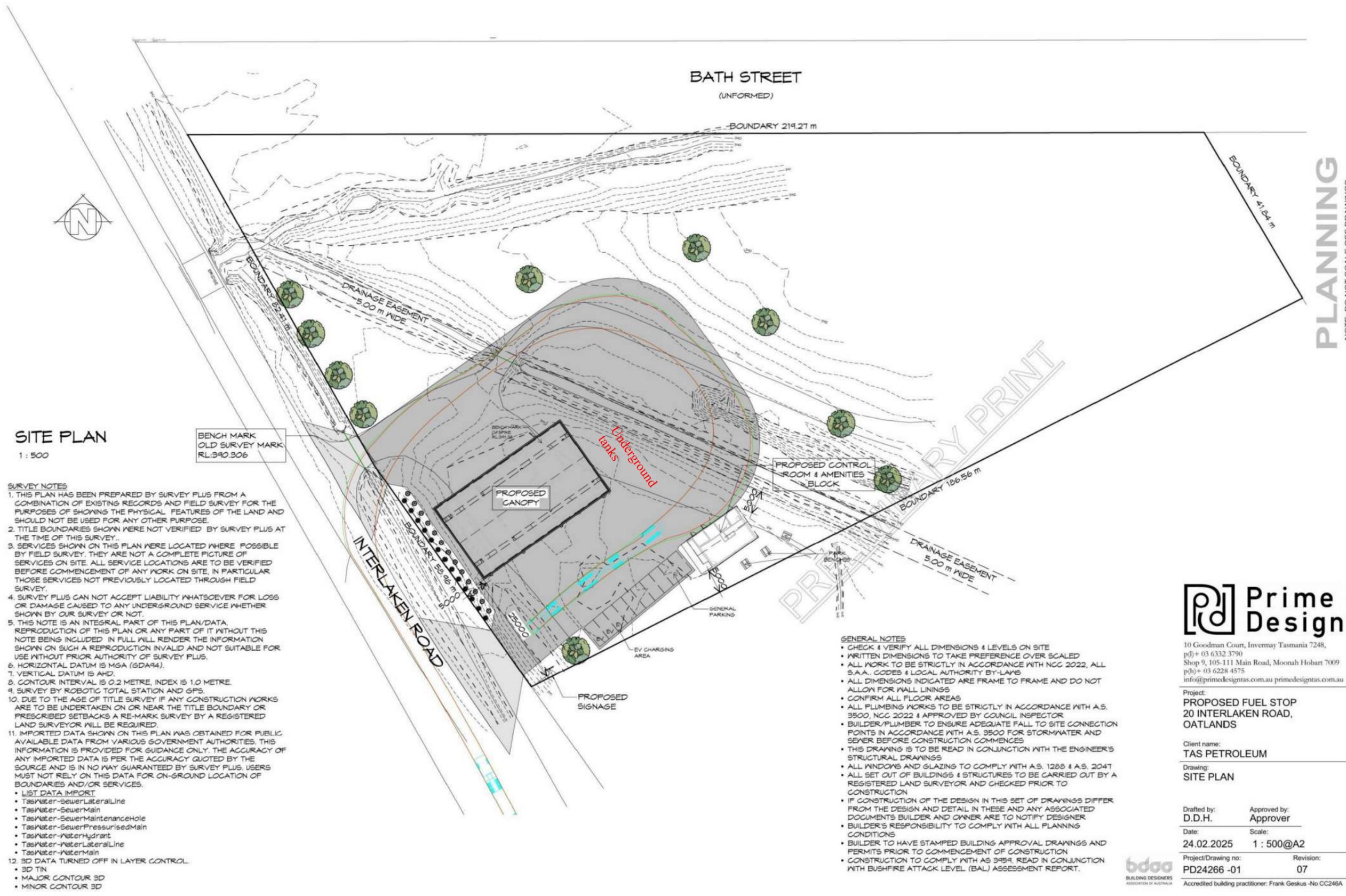


Figure 1: Location



Figure 2: Aerial Image (Google Earth)



PLANNING
NOTE: DO NOT SCALE OFF DRAWINGS

SITE PLAN
1 : 500

BENCH MARK
OLD SURVEY MARK
RL:390.306

SURVEY NOTES

1. THIS PLAN HAS BEEN PREPARED BY SURVEY PLUS FROM A COMBINATION OF EXISTING RECORDS AND FIELD SURVEY FOR THE PURPOSES OF SHOWING THE PHYSICAL FEATURES OF THE LAND AND SHOULD NOT BE USED FOR ANY OTHER PURPOSE.
2. TITLE BOUNDARIES SHOWN WERE NOT VERIFIED BY SURVEY PLUS AT THE TIME OF THIS SURVEY.
3. SERVICES SHOWN ON THIS PLAN WERE LOCATED WHERE POSSIBLE BY FIELD SURVEY. THEY ARE NOT A COMPLETE PICTURE OF SERVICES ON SITE. ALL SERVICE LOCATIONS ARE TO BE VERIFIED BEFORE COMMENCEMENT OF ANY WORK ON SITE, IN PARTICULAR THOSE SERVICES NOT PREVIOUSLY LOCATED THROUGH FIELD SURVEY.
4. SURVEY PLUS CAN NOT ACCEPT LIABILITY WHATSOEVER FOR LOSS OR DAMAGE CAUSED TO ANY UNDERGROUND SERVICE WHETHER SHOWN BY OUR SURVEY OR NOT.
5. THIS NOTE IS AN INTEGRAL PART OF THIS PLAN/DATA. REPRODUCTION OF THIS PLAN OR ANY PART OF IT WITHOUT THIS NOTE BEING INCLUDED IN FULL WILL RENDER THE INFORMATION SHOWN ON SUCH A REPRODUCTION INVALID AND NOT SUITABLE FOR USE WITHOUT PRIOR AUTHORITY OF SURVEY PLUS.
6. HORIZONTAL DATUM IS MGA (GDA94).
7. VERTICAL DATUM IS AHD.
8. CONTOUR INTERVAL IS 0.2 METRE, INDEX IS 1.0 METRE.
9. SURVEY BY ROBOTIC TOTAL STATION AND GPS.
10. DUE TO THE AGE OF TITLE SURVEY IF ANY CONSTRUCTION WORKS ARE TO BE UNDERTAKEN ON OR NEAR THE TITLE BOUNDARY OR PRESCRIBED SETBACKS A RE-MARK SURVEY BY A REGISTERED LAND SURVEYOR WILL BE REQUIRED.
11. IMPORTED DATA SHOWN ON THIS PLAN WAS OBTAINED FOR PUBLIC AVAILABLE DATA FROM VARIOUS GOVERNMENT AUTHORITIES. THIS INFORMATION IS PROVIDED FOR GUIDANCE ONLY. THE ACCURACY OF ANY IMPORTED DATA IS PER THE ACCURACY QUOTED BY THE SOURCE AND IS IN NO WAY GUARANTEED BY SURVEY PLUS. USERS MUST NOT RELY ON THIS DATA FOR ON-GROUND LOCATION OF BOUNDARIES AND/OR SERVICES.
 - LIST DATA IMPORT
 - TasWater-SewerLateralLine
 - TasWater-SewerMain
 - TasWater-SewerMaintenanceHole
 - TasWater-SewerPressurisedMain
 - TasWater-WaterHydrant
 - TasWater-WaterLateralLine
 - TasWater-WaterMain
12. 3D DATA TURNED OFF IN LAYER CONTROL.
 - 3D TIN
 - MAJOR CONTOUR 3D
 - MINOR CONTOUR 3D

GENERAL NOTES

- CHECK & VERIFY ALL DIMENSIONS & LEVELS ON SITE
- WRITTEN DIMENSIONS TO TAKE PREFERENCE OVER SCALED
- ALL WORK TO BE STRICTLY IN ACCORDANCE WITH NCC 2022, ALL S.A.A. CODES & LOCAL AUTHORITY BY-LAWS
- ALL DIMENSIONS INDICATED ARE FRAME TO FRAME AND DO NOT ALLOW FOR WALL LININGS
- CONFIRM ALL FLOOR AREAS
- ALL PLUMBING WORKS TO BE STRICTLY IN ACCORDANCE WITH A.S. 3500, NCC 2022 & APPROVED BY COUNCIL INSPECTOR
- BUILDER/PLUMBER TO ENSURE ADEQUATE FALL TO SITE CONNECTION POINTS IN ACCORDANCE WITH A.S. 3500 FOR STORMWATER AND SEWER BEFORE CONSTRUCTION COMMENCES
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE ENGINEER'S STRUCTURAL DRAWINGS
- ALL WINDOWS AND GLAZING TO COMPLY WITH A.S. 1288 & A.S. 2047
- ALL SET OUT OF BUILDINGS & STRUCTURES TO BE CARRIED OUT BY A REGISTERED LAND SURVEYOR AND CHECKED PRIOR TO CONSTRUCTION
- IF CONSTRUCTION OF THE DESIGN IN THIS SET OF DRAWINGS DIFFER FROM THE DESIGN AND DETAIL IN THESE AND ANY ASSOCIATED DOCUMENTS BUILDER AND OWNER ARE TO NOTIFY DESIGNER
- BUILDER'S RESPONSIBILITY TO COMPLY WITH ALL PLANNING CONDITIONS
- BUILDER TO HAVE STAMPED BUILDING APPROVAL DRAWINGS AND PERMITS PRIOR TO COMMENCEMENT OF CONSTRUCTION
- CONSTRUCTION TO COMPLY WITH AS 3159, READ IN CONJUNCTION WITH BUSHFIRE ATTACK LEVEL (BAL) ASSESSMENT REPORT.



10 Goodman Court, Invermay Tasmania 7248,
p(t) + 03 6332 3790
Shop 9, 105-111 Main Road, Moonah Hobart 7009
p(h) + 03 6228 4575
info@primedesigntas.com.au primedesigntas.com.au

Project:
**PROPOSED FUEL STOP
20 INTERLAKEN ROAD,
OATLANDS**

Client name:
TAS PETROLEUM

Drawing:
SITE PLAN

Drafted by:
D.D.H.

Approved by:
Approver

Date:
24.02.2025

Scale:
1 : 500@A2

Project/Drawing no:
PD24266 -01

Revision:
07



Accredited building practitioner: Frank Geskus -No CC246A

Figure 3: Site Plan

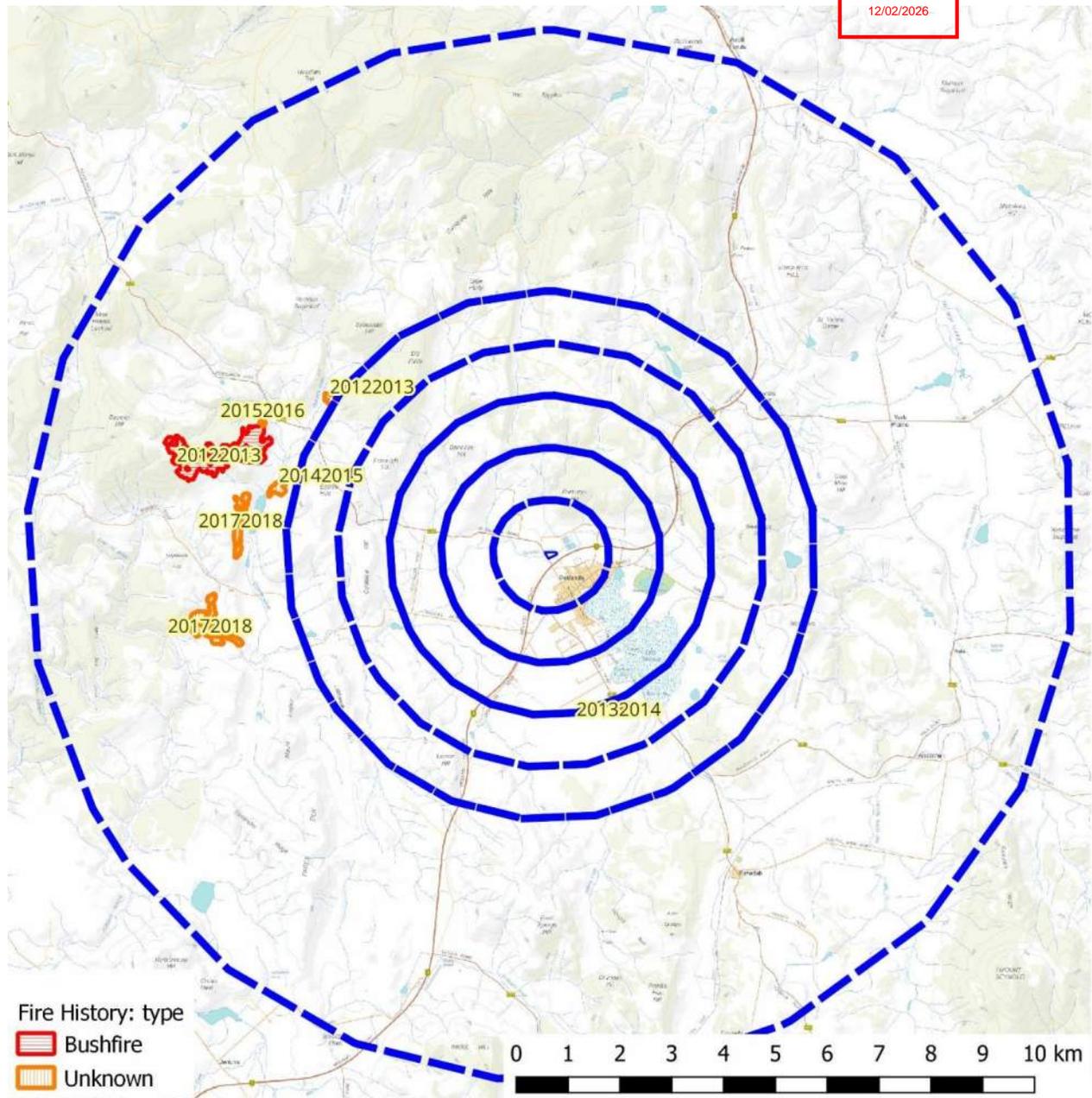
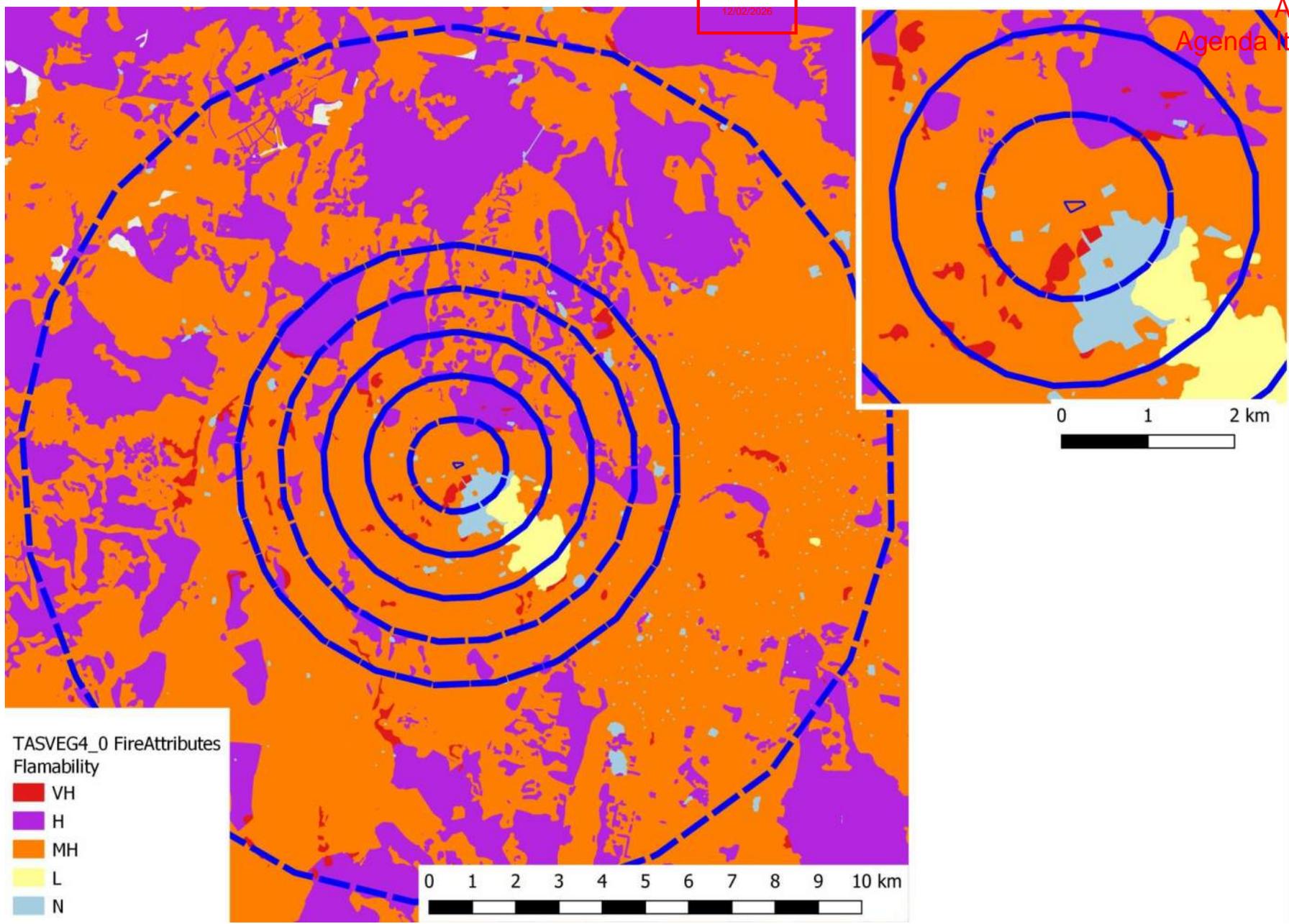


Figure 4: fire history

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TASVEG4_0 FireAttributes
Flamability
VH
H
MH
L
N

Figure 5: TasVeg Flammability



RE: BFP#105 - 20 Interlaken Road, Oatlands - TFS Endorsement of Emergency Management Strategy |

Bushfire Practitioner

to me, Bushfire ▾

9:52 AM

Good Morning Scott,

Thank you for providing the updated strategy.

The Emergency Management Strategy attached, Version 2, dated 14/04/2025 is endorsed by the TFS.

Please feel free to contact us if you have any questions.

Best regards,

Alex Duncan

Planning & Assessment Officer

Bushfire Risk Unit

Tasmania Fire Service

Service | Professionalism | Integrity | Consideration

Cnr Argyle and Melville Streets | GPO Box 308 Hobart Tasmania 7001

Mob: 0458 385 873

alex.duncan@fire.tas.gov.au | www.fire.tas.gov.au

I live and work on unceded palawa land



Attachment
Agenda Item 12.1.1
E3 Planning

Environment • Economy • Evolution

Appendix G

Letter from landowners C & M Livestock Consulting
Tasmania Pty Ltd 4 June 2025



PO Box 58 South HOBART, Tasmania 7004

0438376840 evan@e3planning.com.au



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Agenda Item 12.1.1

04/06/2025

Southern Midlands Council
85 Main Street
Kempton Tas 7030

RE: 20 Interlaken Road Oatlands Site Specific Qualification Tas Petroleum

Please accept this correspondence as C & M Livestock Consulting Tasmania Pty Ltd (the Owners) consent to the request to amend the Tasmanian Planning Scheme Southern Midlands Local Provision Schedule, to insert a Site-Specific Qualification to provide for a Vehicle Fuel Sales and Service as a Discretionary Use on our Property at 20 Interlaken Road Oatlands.

Regards,

Cathy A McDowell

Malcolm P Frost



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Appendix H

Crown Land Consent



PO Box 58 South HOBART, Tasmania 7004

0438376840 evan@e3planning.com.au



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Agenda Item 12.1.1



Tasmanian
Government

Department of State Growth

SALAMANCA BUILDING PARLIAMENT SQUARE
4 SALAMANCA PLACE, HOBART TAS

GPO Box 536, Hobart TAS 7001 Australia
Email permits@stategrowth.tas.gov.au Web www.transport.tas.gov.au

Evan Boardman
e3planning Pty Ltd
By email: evan@e3planning.com.au

Dear Evan

Crown Landowner Consent Granted SRA-25-570

I refer to your recent request for Crown landowner consent relating to the development application at 20 Interlaken Road, Oatlands for Fuel Outlet.

I, Fiona McLeod, Director Asset Management, the Department of State Growth, having been duly delegated by the Minister under section 52(1F) of the Land Use Planning and Approvals Act 1993 (the Act), and duly authorised for the purposes of a Permit application that requires amendment of LPS and in accordance with the provisions of sections 52(1B)(b) and section 40T(6) of the Act, hereby give my consent to the making of the application, insofar as it affects the State road network and any Crown land under the jurisdiction of this department.

The consent given by this letter is for the making of the application only insofar as that it impacts Department of State Growth administered Crown land and is with reference to your application dated 20 August 2025, and the approved documents, as accessible via the link below:

<https://files.stategrowth.tas.gov.au/index.php/s/tlapBpQEeUHTC9d>

Copies of the Instrument of Delegation and Instrument of Authorisation from the Minister can also be accessed via the above link.

Please access and download these documents for your records as soon as possible as this link will expire six (6) months from the date of this letter.

In giving consent to lodge the subject development application, the department notes the following applicable advice:

The department reserves the right to make a representation to the relevant council in relation to any aspect of the proposed development relating to its road network and/or property.

Yours sincerely

Fiona McLeod
DIRECTOR ASSET MANAGEMENT

Delegate for the Minister administering the *Roads and Jetties Act 1935*

15 September 2025

Cc: General Manager, Southern Midlands



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APPLICATION FOR PLANNING PERMIT DEVELOPMENT / USE

Use this form to apply for a permit in accordance with section 57 and 58 of the *Land Use Planning and Approvals Act 1993*

Proposed use/development:
(Provide details of proposed works and use).

Fuel Outlet

Location of Development:
(If the development includes more than one site, or is over another property include address of both Properties).

20 Interlaken Road Oatlands

Certificate of Title/s Volume Number/Lot Number:

CT44807/6

Land Owners Name:

C&M LIVESTOCK CONSULTING TASMANIA PTY LTD

Full Name/s or Full Business/Company Name

Applicant's Name:

E3planning pty ltd

Full Name/s or Full Business/ Company Name (ABN if registered business or company name)

Contact details:

PO Box 58
South Hobart Tas 7004:

Telephone or Mobile:
0438376840

Email address:
evan@e3planning.com.au

(Please note it is your responsibility to provide your correct email address and to check your email for communications from the Council.)

Details Tax Invoice for application fees to be in the name of: (if different from applicant)

TAS Petroleum Pty Ltd

Full Name/s or Full Business or Company Name and ABN if registered business or company name

As Above
Print email address

ABN

What is the estimated value of all the new work proposed
\$750,000
\$



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For Commercial Planning Permit Applications Only

Signage: Yes No

If yes, attach details: size, location and art work

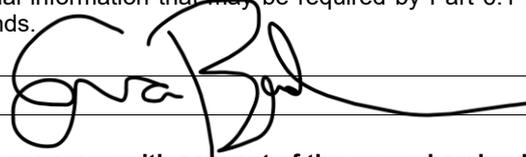
Business Details: Existing hours of operation vs Proposed hours of new operation 24 hours 7 days per week. Table with columns for Hours, am, to, pm and rows for Weekdays, Sat, Sun.

Number of existing employees: Number of proposed new employees:

Traffic Movements: Table with 4 columns. Row 1: Number of commercial vehicles serving the site at present, As per the attached TIA, Approximate number of commercial vehicles servicing the site in the future, As per the attached TIA. Row 2: Number of Car Parking Spaces: How many car spaces are currently provided, As per the attached TIA, How many new car spaces are proposed, As per the attached TIA.

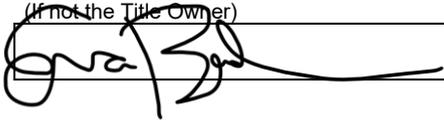
Is the development to be staged: Please tick ✓ answer. Yes No X

Please attach any additional information that may be required by Part 6.1 Application Requirements of the Tasmanian Planning Scheme – Southern Midlands.

Signed Declaration 

I/we as owner of the land or person with consent of the owner hereby declare that:

- 1. I/we have read the Certificate of Title and Schedule of Easements for the land and I/we are satisfied that this application is not prevented by any restrictions, easements or covenants.
- 2. I/we provide permission by or on behalf of the applicant for Council officers to enter the site to assess the application.
- 3. The information given in this application is true and accurate. I/we understand that the information and materials provided with this application may be made available to the public. I/we understand that the Council may make such copies of the information and materials as, in its opinion, are necessary to facilitate a thorough consideration of the application.
- 4. I/we have secured the necessary permission from the copyright owner to communicate and reproduce the plans submitted with the application for assessment. I/we indemnify the Southern Midlands Council for any claim or action taken against it regarding a breach of copyright in respect of any of the information or material provided.
- 5. I/we declare that, in accordance with Section 52(1) of the Land Use Planning and Approvals Act 1993, that I have notified the owner of the intention to make this application. Where the subject property is owned or controlled by Council or the Crown, their consent is attached and the application form signed by the Minister of the Crown responsible and/or the General Manager of the Council.

Applicant Signature (If not the Title Owner) 

Applicant Name (please print) **Evan Boardman**

Date **20 August 2025**



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Land Owner(s) Signature
The landowners have been notified of the making of the application as per the Land Use Planning and Approvals Act 1993

Land Owners Name (please print)

Date

Land Owner(s) Signature
Fiona McLeod

Land Owners Name (please print)
Fiona McLeod

Date
15/09/2025

Delegate for the Minister administering the Roads and Jetties Act 1935

PRIVACY STATEMENT

The Southern Midlands Council abides by the Personal Information Protection Act 2004 and views the protection of your privacy as an integral part of its commitment towards complete accountability and integrity in all its activities and programs.

Collection of Personal Information: The personal information being collected from you for the purposes of the Personal Information Protection Act, 2004 and will be used solely by Council in accordance with its Privacy Policy. Council is collecting this information from you in order to process your application.

Disclosure of Personal Information: Council will take all necessary measures to prevent unauthorised access to or disclosure of your personal information. External organisations to whom this personal information will be disclosed as required under the Building Act 2000. This information will not be disclosed to any other external agencies unless required or authorised by law.

Correction of Personal Information: If you wish to alter any personal information you have supplied to Council please telephone the Southern Midlands Council on (03) 62545050. Please contact the Council's Privacy Officer on (03) 6254 5000 if you have any other enquires concerning Council's privacy procedures.



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Appendix I

TPC Form 1



PO Box 58 South HOBART, Tasmania 7004

0438376840 evan@e3planning.com.au



Owners' consent

Requests for amendments of a planning scheme or Local Provisions Schedule and applications for combined permits require owners' consent. This form must be completed if the person making the request is not the owner, or the sole owner.

The person making the request must clearly demonstrate that all owners have consented.

Please read the notes below to assist with filling in this form.

1. Request made by:

Name(s):

Email address

Contact number:

2. Site address:

Address:

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

3. Consent of registered land owner(s):

Every owner, joint or part owner of the land to which the application relates must sign this form (or a separate letter signed by each owner is to be attached).

Consent to this request for a draft amendment/and combined permit application is given by:

Registered owner :

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

Position
(if applicable):

Signature:

Date:

Registered owner
(please print):

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

Position
(if applicable):

Signature:

Date:

Registered owner
(please print):

Property identifier (folio of the Register for all lots, PIDs, or affected lot numbers on a strata plan):

Position
(if applicable):

Signature:

Date:

NOTES:

a. When is owners' consent required?

Owners' consent is required for:

- amendments to an interim planning scheme or to a Local Provisions Schedule¹; or
- combined permits and amendments².

Owners' consent must be provided before the planning authority determines to initiate, certify or prepare the amendment.

b. Who can sign as owner?

Where an owner is a natural person they must generally sign the owner's consent form personally.

Where an owner is not a natural person then the signatory must be a person with legal authority to sign, for example company director or company secretary.

If the person is acting on behalf of the owner under a legal authority, then they must identify their position, for example trustee or under a power of attorney. Documentary evidence of that authority must also be given, such as a full copy of the relevant Trust Deed, Power of Attorney, Grant of Probate; Grant of Letters of Administration; Delegation etc.

Please attach additional pages or separate written authority as required.

c. Strata title lots

Permission must be provided for any affected lot owner and for common property for land under a strata title under the *Strata Titles Act 1998*. For common property, permission can be provided in one of the following ways:

- a letter affixed with the body corporate's common seal, witnessed by at least two members of the body corporate (unless there is only one member, in which case the seal must be witnessed by that member) and which cites the date on which the body corporate or its committee of management met and resolved to give its consent to the application; or,
- the consent of each owner of each lot on the strata plan.

d. Companies

If the land is owned by a company the form is to be signed by a person with authority in accordance with the *Corporations Act 2001 (Cwth)*.

e. Associations

If the land is owned by an incorporated association the form is to be signed by a person with authority in accordance with the rules of the association.

f. Council or the Crown

If the land is owned by a council or the Crown then form is to be signed by a person authorised by the relevant council or, for Crown land, by the Minister responsible for the Crown land, or a duly authorised delegate.

The name and positions of those signing must be provided.

Effective Date: September 2021

¹ under section 33(1) of the former provisions of the *Land Use Planning and Approvals Act 1993* or section 37 of the current provisions.

² under section 43A of the former provisions or section 40T of the current provisions of the Act

CLIENT:
TAS PETROLEUM

PROJECT:
INTERLAKEN FUEL STOP

ADDRESS:
20 INTERLAKEN ROAD, OATLANDS

PROJECT No:
251035

STATUS:
PRELIMINARY / INFORMATION

ISSUED FOR / DESCRIPTION:
DEVELOPMENT APPROVAL

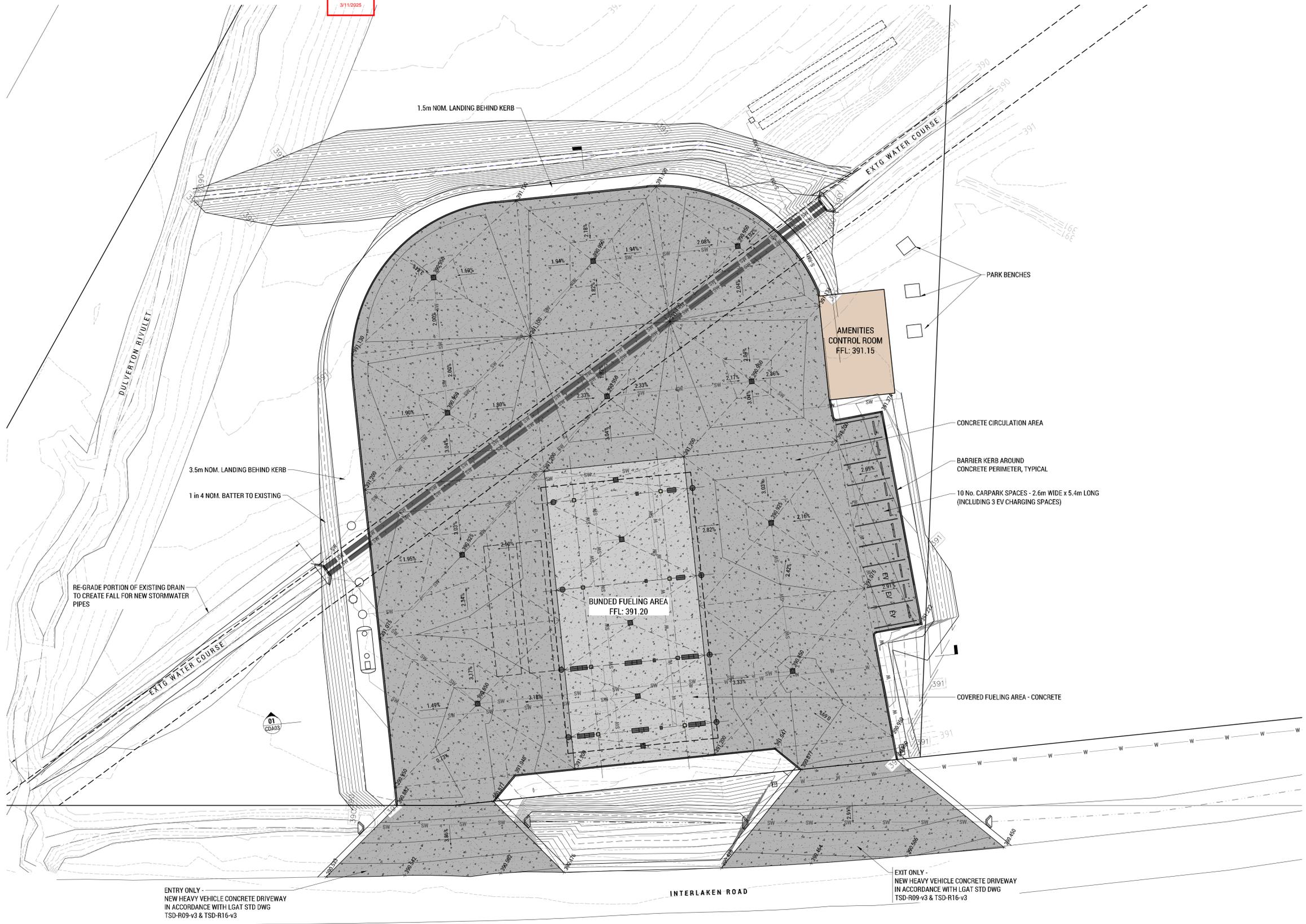
DRAWINGS:

- COV - COVER SHEET
- CDA01 - CIVIL WORKS PLAN
- CDA02 - CONCEPT SITE SERVICE PLAN
- CDA03 - TYPICAL SITE SECTIONS

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		DO NOT SCALE - IF IN DOUBT, ASK <small>THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257</small>		DESIGN CHK: JS		PROJECT: INTERLAKEN FUEL STOP	SCALE: - SHEET SIZE: A1 DWGS IN SET: -
				DRAWN BY: PVD		ADDRESS: 20 INTERLAKEN ROAD, OATLANDS	PROJECT No: 251035 DWG No: COV REV: A
A	DEVELOPMENT APPROVAL	PVD	25-07-25	DRAFT CHK: JS			
REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:	APPROVED: -	ACRED. No: -	DATE: -	



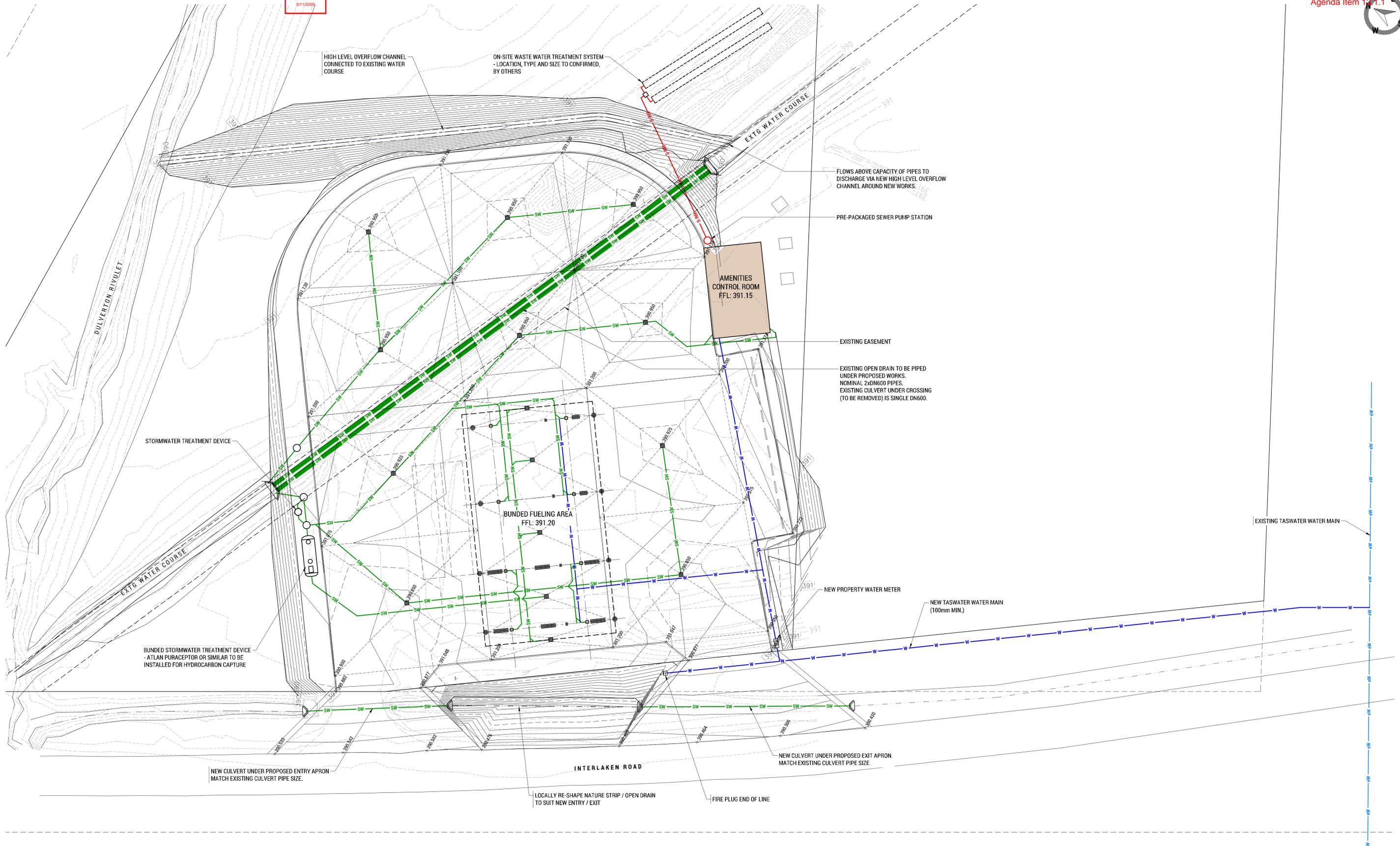
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3/11/2025



CONCEPT CIVIL WORKS PLAN
SCALE 1:250

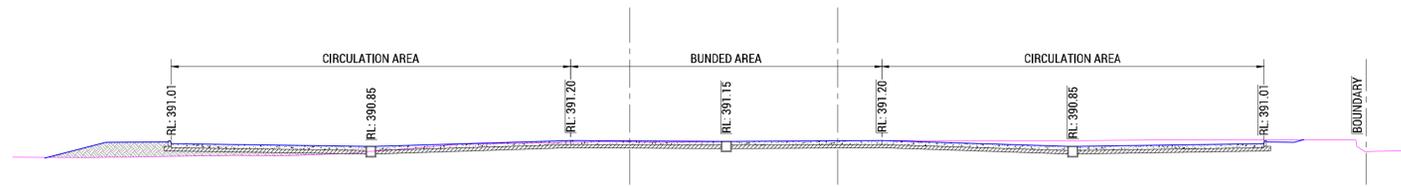
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REV: ISSUED FOR / DESCRIPTION:		BY: DATE:	DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257		DESIGN CHK: JS		PROJECT: INTERLAKEN FUEL STOP	SCALE: 1:250 SHEET SIZE: A1 DWGS IN SET: -
		APPROVED: -	ACRED. No: -	DRAFT CHK: JS	ADDRESS: 20 INTERLAKEN ROAD, OATLANDS		PROJECT No: 251035 DWG No: CDA01 REV: A	

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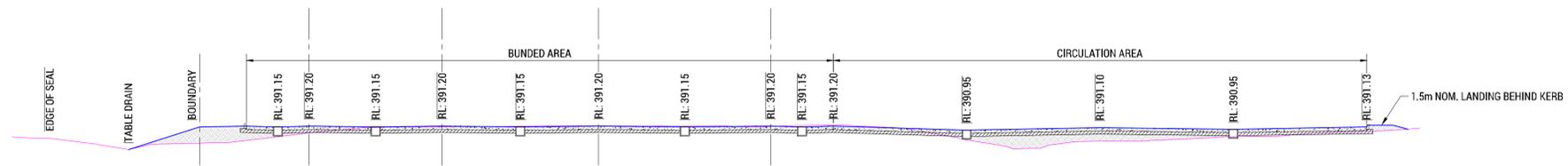


CONCEPT SITE SERVICE PLAN
SCALE 1:250

A DEVELOPMENT APPROVAL		PVD 25-07-25	STATUS: PRELIMINARY / INFORMATION		DESIGN BY: PVD	CLIENT: TAS PETROLEUM	TITLE: CONCEPT SITE SERVICE PLAN
REV: ISSUED FOR / DESCRIPTION:		BY: DATE:	DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257		DESIGN CHK: JS			PROJECT: INTERLAKEN FUEL STOP	SCALE: 1:250 SHEET SIZE: A1 DWGS IN SET: -
			APPROVED: -	ACRED. No: -	DRAFT CHK: JS	ADDRESS: 20 INTERLAKEN ROAD, OATLANDS	PROJECT No: 251035 DWG No: CDA02 REV: A
					 22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200		



01 TYPICAL SITE SECTION
CDA01 SCALE 1:200



02 TYPICAL SITE SECTION
CDA01 SCALE 1:200

		STATUS: PRELIMINARY / INFORMATION		DESIGN BY: PVD	 22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200	CLIENT: TAS PETROLEUM	TITLE: TYPICAL SITE SECTIONS
		DO NOT SCALE - IF IN DOUBT, ASK <small>THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257</small>		DESIGN CHK: JS		PROJECT: INTERLAKEN FUEL STOP	SCALE: 1:200 SHEET SIZE: A1 DWGS IN SET: -
				DRAWN BY: PVD		ADDRESS: 20 INTERLAKEN ROAD, OATLANDS	
				DRAFT CHK: JS			PROJECT No: 251035 DWG No: CDA03 REV: A
A	DEVELOPMENT APPROVAL	PVD	25-07-25	APPROVED: -	ACRED. No: -	DATE: -	
REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:				



Supreme Court of Tasmania - Full Court

**Dourias MGH Pty Ltd v Clarence City Council [2025]
TASFC 10 (22 December 2025)**

Last Updated: 23 December 2025

[2025] TASFC 10**COURT:** SUPREME COURT OF TASMANIA (FULL COURT)**CITATION:** *Dourias MGH Pty Ltd v Clarence City Council* [2025] TASFC 10**PARTIES:** DOURIAS MGH PTY LTD (ACN 609 879 987)

v

CLARENCE CITY COUNCIL

FILE NO: 165/2025**DELIVERED ON:** 22 December 2025**DELIVERED AT:** Hobart**HEARING DATE:** 28 May 2025**JUDGMENT OF:** Shanahan CJ, Wood J, Brett J**CATCHWORDS:**

Environment and planning – Planning – Development assessment and control – Assessment and control of particular matters – Subdivision – Other matters – Date and methodology of valuation of payment in lieu of increasing public open space under s 117(2) of *Local Government (Building and Miscellaneous Provisions) Act 1993* – Primary judge concluded value must be assessed on assumption the subdivision has been completed and on the date at which the final plan was sent to the Recorder of Titles – Appellant contended date of valuation should be assessed at the time the plan of subdivision first becomes the final plan when sent by the owner’s surveyor to the council – Full Court rejected appellant’s submission – Legislative scheme intends the payment in lieu of provision of public open space as alternative to the receipt by council of actual land in the final subdivision – Necessarily requires value to be assessed on the basis the subdivision process has been

completed and subdivided allotments will be available for individual sale – Primary judge substantially correct in determining date for valuation on an after-subdivision basis – Appeal dismissed.

Attachment
Agenda Item 12.4.1

Aust Dig Environment and Planning [1347]

Environment and planning – Planning – Development assessment and control – Assessment and control of particular matters – Subdivision – Conditions imposed on subdivision approvals – Other conditions – Payment in lieu of increasing public open space – Operation of ss 117(1) and (2) of *Local Government (Building and Miscellaneous Provisions) Act 1993* – Even if the council does not seek security under s 117(1) it can still require payment in lieu of public open space under s 117(2) – If council intends to require such a payment, whether or not it seeks security for it, council must give notice of that requirement at the time of planning approval – Such requirement must be made in the conditions of planning approval – Once requirement for payment is made by way of condition then provisions of the legislation will apply according to the terms of s 117 – Section 117 will, if applicable, take precedence over any contrary provision in the condition – To extent the condition requiring payment is inconsistent with s 117 it is ultra vires and of no effect.

Aust Dig Environment and Planning [1354]

Legislation:

Acts Interpretation Act 1931 (Tas), s 8A

Land Acquisition Act 1993 (Tas), ss 27, 36, 41, 42, 59(2), 62

Land Use Planning Approvals Act 1993 (Tas), s 51(3A)

Local Government (Building and Miscellaneous Provisions) Act 1993 (Tas), ss 80, 81(2), 83(1), 84, 85, 87, 88(1), 89(1), 89(5), 94, 116(1), 116(6), 117(1), 117(2), 117(3), 117(4), 117(5)

Cases:

Raff Angus Pty Ltd v Resource Management and Planning Appeal Tribunal [2018] TASSC 60

Spencer v The Commonwealth of Australia [1907] HCA 82; (1907) 5 CLR 418

Western Australian Planning Commission v Temwood Holdings Pty Ltd [2004] HCA 63

REPRESENTATION:

Counsel:

Appellant: G O'Rafferty

Respondent: P Jackson SC

Solicitors:

Appellant: Leonard Fernandez Barristers & Solicitors

Respondent: Simmons Wolfhagen

Judgment Number: [2025] TASCCA 10

Number of paragraphs: 40

Serial No 10/2025

File No 165/2025

DOURIAS MGH PTY LTD v CLARENCE CITY COUNCIL

REASONS FOR JUDGMENT FULL COURT

SHANAHAN CJ

WOOD J

BRETT J

22 December 2025

Order of the Court:

1. Appeal dismissed.

DOURIAS MGH PTY LTD v CLARENCE CITY COUNCIL

REASONS FOR JUDGMENT FULL COURT

SHANAHAN CJ

22 December 2025

1. I have had the benefit of Brett J's draft judgment and I would dismiss this appeal for the reasons given by his Honour.

DOURIAS MGH PTY LTD v CLARENCE CITY COUNCIL

REASONS FOR JUDGMENT FULL COURT

WOOD J

22 December 2025

2. I agree with Brett J.

DOURIAS MGH PTY LTD v CLARENCE CITY COUNCIL

REASONS FOR JUDGMENT FULL COURT

BRETT J

22 December 2025

3. This appeal concerns the subdivision of a block of land situated at Lot 26, Mannata Street Lauderdale. In 2015, the then owner of that property applied to the respondent, who was and still is the relevant planning authority, for approval of a plan of subdivision. The proposed subdivision was to divide the land into seven residential lots with an eighth lot constituting the balance.

4. The respondent approved the subdivision by the grant of a permit. The subdivision plan did not include any provision for public open space, but a condition of the approval required the appellant to pay a cash sum in lieu of public open space, calculated at five per cent of the value of the land. A dispute subsequently arose between the parties concerning the value upon which the payment was to be based, and hence the quantum of the payment. By originating application filed on 11 November 2022, the appellant sought to have the question of the amount of the contribution determined by the Supreme Court as a disputed claim for compensation under the *Land Acquisition Act 1993*. The jurisdiction for that determination is asserted to arise pursuant to s 117(3) of the *Local Government (Building and Miscellaneous Provisions) Act 1993* (the LGBMP).

5. The application was heard by Marshall AJ on 2 December 2024. Each party presented expert evidence from a valuer. The primary differences between the valuation evidence were the date at which value was to be assessed and the methodology adopted for the assessment. The valuation asserted by the respondent was \$2,500,000, resulting in a contribution of \$125,000, and by the appellant \$1,300,000 resulting in a contribution of \$65,000. The primary judge handed down his decision on 19 December 2024. His Honour accepted the valuation evidence presented by the respondent and assessed the relevant contribution accordingly. The appellant has now appealed that decision to this Court. In essence, the grounds of appeal assert that the primary judge erred in respect of both the date and the methodology of the valuation.

6. Because the primary judge exercised jurisdiction to determine the amount of the payment as a disputed claim for compensation under the *Land Acquisition Act*, it is arguable that the hearing and determination of that question was in accordance with the power conferred by s 59(2) of that Act. Section 62 provides for appeals to this Court from a determination made under s 59. The appeal is to be by way of rehearing. However, the right to appeal is confined to a question of law. The grounds of appeal assert errors "in law and/or fact". On the face of the legislation, there is no right to appeal on a question of fact. This question did not arise either in the submissions of the parties or on the hearing of the appeal. Because of the view I have taken about the merits of each ground, it is not necessary to determine the point. It may well be in any event that the fundamental questions raised by the grounds of appeal at the very least are questions of mixed fact and law, but as the parties have not had the opportunity to make submissions, I decline to express a concluded view.

Subdivision

7. Before considering the dispute in detail, it is convenient to first consider the legislative context.

8. Subdivision is the division of the surface of a piece of land by creating estates or interests giving separate rights of occupation: s 80 of the LGBMP. The process for subdivision is dealt with and regulated by Part 3 of that Act. The starting point is s 81 which prohibits an owner of land from subdividing the land except, for relevant purposes, in accordance with a plan of subdivision which has been approved by the grant of a permit under the *Land Use Planning Approvals Act 1993* (LUPA). Section 81(2) provides that a planning scheme may provide that an application for approval of the subdivision plan is to be made as if it were an application for a discretionary or permitted development permit. I will refer to this approval as "planning approval". In accordance with the definition contained in s 80, the plan submitted for planning approval is termed the "plan of subdivision". The planning approval stage is fundamental to the balance of the process because it is then that the concept and layout of the proposed subdivision is determined. The application can be rejected. Section 84 provides circumstances in which the council "is not to approve a plan of subdivision". Section 85 prescribes a further set of circumstances, largely to do with layout and design, in which the council can, in its discretion, refuse to approve the plan of subdivision. These include where the council is of the opinion that the layout should be altered to include or omit public open space. Of course, these provisions are in addition to specific provisions relevant to the land contained in the applicable planning scheme. Further, it is at the stage of planning approval that the council can place conditions on the permit. There is no power to do so subsequently. It is therefore at this stage that the owner will be aware as to what conditions it must meet if the subdivision is to proceed.

9. Part 3 then prescribes the process whereby an approved plan of subdivision becomes registered by the Recorder of Titles, thereby creating the new and separate titles. The legislatively prescribed journey between planning approval and registration provides for one or more further iterations of the plan. These iterations provide for provision of further detail, related in particular to survey, engineering, provision of services, and roads. They can be subject to further approval by relevant agencies. There is provision for adjustment to layout and design to meet title requirements including ensuring that each separate parcel has the qualities of a minimum lot. However, it is clear that any further iteration must comply in substance with the approved plan of subdivision. There is nothing to prevent the owner from withdrawing the plan and terminating the process at any time prior to registration. However, once registered, the act of subdivision is complete and the land is then comprised of the individual proprietary interests thereby created.

10. In general overview, the process is as follows:

. Section 87 provides that on receipt of planning approval, the owner is to arrange for the preparation of a final plan by a registered surveyor in accordance with:

- (a) the requirements approved by the Recorder of Titles and
- (b) any other requirement of the permit issued under LUPA.

By virtue of s 80, "final plan" where it appears elsewhere in Part 3, means a final plan referred to in this section.

Section 88 provides that the owner is to lodge the final plan with the council, together with certain other documents. This includes evidence satisfactory to the council of compliance with each of the permit conditions which must be complied with before the plan is sealed.

By s 89, the council within 20 business days after the final plan is lodged with it under s 88, is to determine whether the final plan complies with Part 3. If it does so determine, then the council is to cause its seal to be affixed to the plan and cause the sealed plan to be lodged in the office of the Recorder of Titles.

By s 89(5), on lodgement of the plan, the Recorder must notify the council and the owner that the plan requires to be amended if satisfied that it is deficient in certain defined respects. This includes issues affecting title and minimum lot size.

By s 94, the final plan takes effect as a sealed plan when the Recorder of Titles signs and dates a memorandum on the plan that the plan is accepted. There are then provisions which apply in respect of the registration and issue of the plan and separate titles. Under s 80, a "sealed plan" for the purposes of Part 3 means a plan approved and sealed under the Part and which has taken effect as provided in s 94. Accordingly, although the legislation variously refers to the plan after the council has affixed its seal as the "sealed plan", "priority final plan" and "final plan", it does not comply with the definition of a sealed plan until it takes effect in accordance with s 94.

Public open space

11. It is clear that the legislative scheme contemplates that one of the matters to be considered and determined at the time of the grant of planning approval is the question of provision by the owner of public open space within the proposed subdivision. "Public open space" is defined by s 80 to mean "space for public recreation or public gardens or for similar purposes".

12. Although the legislation permits the council to require a greater proportion, the underlying expectation given effect by the relevant provisions of the LGBMP is that the owner will dedicate, for nominal compensation, five percent of the whole area of the land being subdivided to public open space. There are various ways in which this expectation can be met. The most basic of these is that the plan may show land set apart for public open space. Section 83(1) provides that the council, before it approves a plan of subdivision, may require the owner to sell to it for a nominal consideration any land shown on the plan in this way. If the council considers that the plan does not show sufficient public open space, then it has two options. Firstly, under s 85 it may refuse to approve the plan of subdivision if it is of the opinion that the layout should be altered to include public open space. Secondly, the Act seems to assume that the council may require an owner to increase the area set apart for public open space in the plan. This assumption arises from the provisions of s 116. Section 116(1) provides that if the council requires an owner to increase the area for public open space beyond that provided for in the plan of subdivision, so that the total area so dedicated exceeds five percent of the land, then the council must purchase the excess. The combined effect of ss 83 and 116 is that the council may require an owner to provide any amount of public open space, but may only acquire for nominal consideration under s 83 an area up to five percent of the whole area comprised in the plan of subdivision. Any land required for

public open space in excess of five percent must be purchased by council in accordance with a sum calculated according to the provisions of s 116. These provisions include that the land is to be valued at the date of lodgement of the plan of subdivision and the amount, if not agreed, is to be determined as if it were a disputed claim for compensation under the *Land Acquisition Act*.

13. The other alternative which is available to the council under the legislative scheme, where it takes the view that the owner has not shown adequate or any public open space in the plan of subdivision, is to require a cash payment in lieu of the provision of public open space. This option is the subject of s 117. I will return to this section in due course.

Factual History

14. The respondent granted planning approval for this subdivision on 2 February 2015. The permit contained a number of conditions, but as already noted, the one of relevance to these proceedings is a condition which provided for the payment by the developer to the respondent of a cash sum in lieu of provision of public open space in the plan. The plan did not include any provision for public open space. The condition provided for a payment calculated at 5% of the value of the subdivided land assessed "as at the date of the lodgement of the final plan of subdivision for sealing". The value was to be assessed by the Valuer General. The effect of the condition was that either the payment or provision of security for it in the form of a bond and supporting bank guarantee was to be provided to the respondent prior to the sealing of the final plan.

15. The respondent's decision to approve the subdivision was subject to an appeal to the Resource Management and Planning Appeal Tribunal. On 25 August 2015, in accordance with the decision of the Tribunal, the respondent issued a fresh planning permit for the subdivision. In 2017, the appellant became the registered owner of the land. On 19 January 2021, the appellant's surveyor lodged with the respondent for the first time the final plan and schedule of easements, pursuant to s 88(1). There were also a number of minor amendments to the permit. However, any such changes did not substantially affect the layout of the subdivision originally approved in 2015. The final plan did not show any provision for public space and the condition which provided for a cash contribution in lieu of public open space remained unchanged.

16. On 28 January 2021, the respondent forwarded an invoice to the appellant's surveyor for the public open space contribution. The invoice was in the sum of \$100,000. This sum was calculated as five per cent of a value of \$2 million. Although the condition had provided that the payment was to be based on a valuation performed by the Valuer General, the value upon which the payment was based was assessed by a private valuer, Andy Bevin, who had been engaged for that purpose by the respondent. The appellant paid the invoiced sum under protest. I assume that this was done to avoid any further delay. The proceedings at first instance and this appeal proceeded on the basis that neither party was bound in respect of the quantum of the required payment, by either the invoice or the payment made by the appellant. Both parties subsequently commissioned further valuations, which were the subject of the evidence presented to the primary judge.

17. On 4 August 2021, there was a minor amendment to the planning permit. This seems to have been a consequence of the requirements of Tas Water. The amendment included an adjustment to the plan relating to a significant easement on the land. On 11 August 2021, the appellant's surveyor forwarded the amended final plan to the respondent. There was then some further correspondence to resolve matters of detail. The respondent affixed its seal to the plan on 14 September 2021 and forwarded the sealed plan to the Recorder of Titles on 16 September 2021. This constituted the lodgement of that plan for the purposes of s 89(1). It is not clear from the evidence when the Recorder signed and dated the memorandum in accordance with s 94, but it can be inferred that this was done shortly after 16 September 2021.

The valuation evidence

18. The respondent's valuer, Mr Bevin, was initially instructed to and did value the land as at 20 December 2020. In accordance with his instructions, he valued only Lots 1 to 7 which were the new lots created by the subdivision excluding the balance, Lot 8. This valuation was \$2,000,000. In accordance with amended instructions, his final assessment was of the value of the whole land, including Lot 8, as at 11 August 2021. He assessed the value on that basis at \$2,500,000.

19. Mr Bevin's evidence was that he valued the land on the basis of its market value as at the stated date. In general terms, he relied on comparable sales and applied what he termed the "current international standard" which is consistent with the test set down by the High Court in *Spencer v The Commonwealth of Australia* [1907] HCA 82; 5 CLR 418. However, he was instructed to and did value the land on the assumption that the subdivision was complete, as if the lots were ready to market, suitable for residential development and sold individually. He then applied a discount which, for the final assessment, was 15%, to reflect the assumption that they were to be sold together at one time.

20. The appellant's valuer, Mr Page, applied the same general test when assessing value. However, in accordance with his instructions, he valued the land as at 19 January 2021, which was the date when the final plan was first provided by the surveyor to the respondent under s 87. He utilised a methodology known as the hypothetical development approach complemented by the direct comparison approach. In essence, this involved an assessment of the gross value of the land based on comparison with other sales to achieve an aggregate gross value calculated by reference to the area of each proposed subdivided lot. He calculated an aggregate gross value on this basis of \$2,695,000. He then deducted his assessment of the hypothetical cost of subdivision which reduced the aggregate value to \$1,300,000. The purpose of the deduction was to achieve a market value of the land on the basis that it was to be sold as a single parcel of land in its unsubdivided state, but taking into account its potential for subdivision.

21. The differences between the bases of valuation, in particular the date at which the land was to be valued and whether it was to be valued on the assumption that it was before or after subdivision, reflected the issues raised by the parties before the primary judge. They also reflect the issues raised by the grounds of appeal. The primary judge found in favour of the respondent on both issues and accepted the valuation evidence of Mr Bevin in

preference to that of Mr Page. It is apparent from his Honour's decision that he took the view that the issues are interrelated. He determined that the value must be assessed on the assumption that the subdivision has been completed. His Honour, in fact, found that the correct date for valuation was 16 September 2021, which is the date that the final plan was sent to the Recorder of Titles for final acceptance under s 94. This was subsequent to the date contended for by the respondent and upon which Mr Bevin had based his valuation. However, the primary judge concluded that any potential difference in value was insignificant and, accordingly, simply applied the valuation arrived at by Mr Bevin. Although the primary judge did not directly criticise Mr Page's opinion, he concluded he had applied an incorrect methodology and, in particular, that he should not have deducted the hypothetical costs of development. Ultimately, his Honour concluded that the market value of the land as a single sale but on the basis of individual subdivided allotments was the correct methodology and this accorded with Mr Bevin's opinion. The primary judge assessed the amount payable as \$125,000, being five per cent of Mr Bevin's assessment of value at \$ 2,500,000.

Jurisdiction

22. The common position of the parties, which seems to have been accepted and assumed by the learned primary judge, is that the amount of a cash payment in lieu of provision of public open space was payable and fell to be assessed under the provisions of s 117 of the LGBMP. I set out the relevant provisions of that section:

"117 Payment instead of increasing public open space

(1) Instead of requiring an owner to increase the area for public open space, the council, before approving a plan of subdivision may require security for the payment of an amount calculated under [subsection \(2\)](#) .

(1A) A planning scheme –

(a) may specify that compliance with a requirement specified in [subsection \(1\)](#) is an acceptable solution in relation to subdivisions; and

(b) may enable a permitted development permit to be issued if such an acceptable solution is complied with in relation to a plan of subdivision.

(2) The amount required to be paid is an amount not exceeding an amount which bears the same ratio to the value of the whole area comprised in the plan as one-twentieth of that area less any area provided for public open space in the final plan and any area created by the final plan of the littoral or riparian reserve bears to that whole area.

(3) If the owner and the council do not agree on the amount to be secured, it is to be determined as a disputed claim for compensation under the [Land Acquisition Act 1993](#) .

(4) The security that may be required is –

(a) a bond by the owner of an amount in excess of any possible demand to secure payment of the amount within 90 days after the demand is made after

the final plan has taken effect; and

(b) a guarantee by an authorised deposit-taking institution, a guarantee, money-lending, insurance or trading corporation approved by the council guaranteeing all money payable on the bond.

(5) The council is to receive an amount payable under this section on trust for the acquisition or improvement of land for public open space for the benefit of inhabitants of the municipal area."

23. The claim which came before the primary judge purported to rely upon the conferral of jurisdiction contained in s 117(3), that is as a disputed claim for compensation under the *Land Acquisition Act*. That Act is, of course, concerned with the compulsory acquisition of land by the Crown and other defined acquiring authorities. Section 36 provides for claims for compensation in respect of land so acquired. By s 41, a disputed claim for compensation arises where the amount claimed by the landowner is rejected by the acquiring authority. Section 42 provides for methods of determining disputed claims, one of which is by application to the Supreme Court. The *Land Acquisition Act* does not encompass nor confer jurisdiction in respect of the dispute arising as to the amount payable as a contribution for public open space on a subdivision under the LGBMP. The only available basis for the Supreme Court's jurisdiction to determine this question as a disputed claim for compensation is that contained in s 117(3).

24. Notwithstanding the position of the parties concerning jurisdiction, this Court raised concerns in relation to that question at the commencement of the hearing of the appeal. The Court's concern arose from a possible construction of the section, which rendered its application dependant upon s 117(1) being enlivened by a requirement by the council for security for a payment. Both the parties and the primary judge had assumed that such a requirement had not been made, although having regard to the permit condition this would seem to be incorrect. In any event, the position of the parties is that on a proper construction of the section, s 117(2) authorises the payment of a cash contribution in lieu of public open space irrespective of whether the council has first sought security for such a payment under s 117(1). This Court's concern was whether that position was correct.

25. The resolution of this question depends upon the construction of s 117. The construction of the section is also necessary in any event in order to understand its operation and this in turn is likely to shed light on, if not determine, the substantive questions concerning valuation on this appeal. There are also related questions in respect of the operation of the section, including whether the assessment of value is to follow the provisions of the permit condition or otherwise be determined by the section, and the role played by provisions of the *Land Acquisition Act*, in respect of the determination of the relevant payment.

The construction of s 117

26. The construction of the section, as with any legislative provision, will depend on both textual and contextual considerations. In *Raff Angus Pty Ltd v Resource Management and Planning Appeal Tribunal* [2018] TASSC 60, I summarised the task in this way:

"The starting point of any process of statutory construction is the plain and ordinary meaning of the text, read in the context of the surrounding provisions and the legislative scheme. The aim of the process is to derive from the statutory words read in context, the meaning "that the legislature is taken to have intended them to have": *Project Blue Sky Inc v Australian Broadcasting Authority* [1998] HCA 28; 194 CLR 355 at 384, per McHugh, Gummow, Kirby and Hayne JJ.

In *Taylor v The Owners - Strata Plan No 11564* [2014] HCA 9, 253 CLR 531, Gageler and Keane JJ expanded on the relevance and effect of context in the process of statutory construction:

'Statutory construction involves attribution of legal meaning to statutory text, read in context.' 'Ordinarily, that meaning (the legal meaning) will correspond with the grammatical meaning ... But not always.' Context sometimes favours an ungrammatical legal meaning. Ungrammatical legal meaning sometimes involves reading statutory text as containing implicit words. Implicit words are sometimes words of limitation. They are sometimes words of extension. But they are always words of explanation. The constructional task remains throughout to expound the meaning of the statutory text, not to divine unexpressed legislative intention or to remedy perceived legislative inattention. Construction is not speculation, and it is not repair.

Context more often reveals statutory text to be capable of a range of potential meanings, some of which may be less immediately obvious or more awkward than others, but none of which is wholly ungrammatical or unnatural. The choice between alternative meanings then turns less on linguistic fit than on evaluation of the relative coherence of the alternatives with identified statutory objects or policies." [Footnotes omitted.]

27. To the extent that there is more than one possible construction, I note the provisions of s 8A of the *Acts Interpretation Act 1931*, which provides that an interpretation that promotes the purpose or object of the Act is to be preferred to an interpretation that does not do so.

28. Some observations can be made about the text of s 117 and its apparent operation within the legislative scheme. Firstly, it is apparent that the section is concerned with the liability for and calculation of a cash payment instead of provision of public open space in the subdivision. The section is not limited to the question of provision of security for such a payment as provided in s 117(1) but also contemplates that the payment will ultimately be made. See in particular s 117(5). Secondly, s 117(2) is concerned with calculation of "an amount required to be paid". It does not expressly provide that its operation is subject to a requirement for security for payment of that amount having been made under s 117(1). This suggests that the provision of security is intended to be a mechanism for ensuring payment of the relevant amount, but that the requirement for payment and its calculation is not dependent upon the utilisation of that mechanism. In other words, the council will have the power to require payment of a cash amount calculated under s 117(2) notwithstanding that it has not first sought security under s 117 (1). Although the right to require such a payment is not spelt out expressly, I am of the view that it arises as a matter of necessary implication from the text of s 117(2). Further, both ss 117(1) and 117(5) assume that a payment will

actually be made. Otherwise, there would be nothing to secure or receive as the case may be.

29. The Supreme Court's jurisdiction to determine a dispute about the calculation of the "amount required to be paid" under s 117(2), as a disputed claim for compensation under the *Land Acquisition Act*, depends upon the construction of s 117(3) and, in particular, the meaning of "the amount to be secured". It is common ground between the parties that "the amount" is that arising under s 117(2), although it could encompass, as well, a dispute about the amount of security under s 117(1). I am satisfied that this construction is correct. Section 117(4) provides for the security that may be required, which is clearly a reference to the requirement that may be made under s 117(1). The security includes "a bond by the owner of an amount in excess of any possible demand to secure payment of the amount". The necessary implication from and the assumption which underlies these provisions is that the amount of security and the amount of the payment are different and are to be calculated at different times. This is consistent with the structure of the section. Section 117(1) expressly provides that the requirement is to be made "before approving a plan of subdivision". Having regard to the definition of "plan of subdivision", the relevant approval must be the planning approval. This accords with the overall legislative scheme which requires the question of the provision of public open space to be dealt with at the planning approval stage. If the submitted plan of a subdivision does not provide a satisfactory amount of public open space, then under s 85, the council may reject the plan. Alternatively, it may proceed under s 117 to require a cash payment in lieu of provision of actual land in the subdivision. It is desirable that that question be dealt with at the planning approval stage but, of course, it is not possible at that time to determine the value of the land upon which the cash payment is to be based, and hence the precise amount of the payment. This is because the subdivision process is not then complete. That question can only be determined after it is complete, that is when the final plan takes effect as a sealed plan under s 94. This is consistent with the text of s 117(2) which requires a calculation in accordance with the "final plan". In particular, it requires the determination of "the value of the whole area comprised in the plan" and "any area provided for public open space in the final plan". I have already discussed the definition of this term. The final plan comes into existence after the planning approval. I will return to the precise timing of this calculation shortly.

30. Because the requirement is made at the time of planning approval, but the precise calculation of the payment cannot be made until later, the legislative scheme provides for security for the payment to be put in place when the requirement is made. This is the purpose of s 117 (1). The council can determine that it will seek a payment in lieu of public open space and seek security for that payment at that time. Therefore, s 117(4) provides for the security, the bond, to be for an amount "in excess of any possible demand to secure payment of the amount".

31. The necessary implication is that ss 117(1) and (2), although related, have separate operation. Even if the council does not seek security under s 117(1), it can still require the payment under s 117(2). Of course, this gives rise to the question of the mechanism and timing of such a requirement so as to create liability under s 117(2). Again, this is not dealt with expressly in the section. However, in my view, the question of timing arises by clear

implication from the legislative scheme. If the council intends to require such a payment, then whether or not it seeks security for the payment, it must give notice of that requirement at the time of planning approval. This is necessarily implied by the section, and any other interpretation could lead to an open-ended liability, and this is inconsistent with the legislative scheme. It could result in absurd, unjust, or unreasonable consequences. This interpretation informs the second part of the question, the mechanism by which the requirement is to be made. The position of both parties is that the council's power in this regard arises implicitly from the section, and it can simply make a demand for the payment. I reject this. In my view, it is clear from the legislative scheme that any such requirement must be made in the conditions of planning approval. The council clearly has power to impose such conditions: s 51(3A) of LUPA; *Western Australian Planning Commission v Temwood Holdings Pty Ltd* [2004] HCA 63. That, in fact, is what it did in this case. The condition on the permit required a payment in lieu of provision of public open space to be calculated at a later time and permitted the condition to be satisfied by provision of appropriate security for the payment. The use of a condition of the planning permit to make the requisite requirement is logically consistent with the overall scheme, in particular the intention that the question of public open space is resolved, at least in principle, as part of the planning approval. It fits neatly with the use of provision of security to ensure that the requirement for the payment is established but to leave the precise calculation and making of the payment to a more appropriate time, in particular when the subdivision takes effect. When that calculation is made, the assumption in s 117(4) is that there will be a "demand" for payment. Again, that is what occurred in this case.

32. The question which then arises is the interrelationship between the terms of the condition and the relevant provisions of s 117. In particular, does the liability for and calculation of the payment arise pursuant to and under the terms of the condition, or alternatively is the condition subject to the provisions of s 117 in that regard. I am satisfied that the latter is the case. In other words, once the requirement for a payment including by way of security is made by way of condition, then the provisions of the legislation will apply according to the terms of the section and will, if applicable, take precedence over any contrary provision in the condition. This is of some importance in this case because the relevant condition contains a number of provisions which are inconsistent with the provisions of s 117. Firstly, it required the value to be calculated by reference to "Lots 1-7", rather than "the whole area comprised in the plan" as required by s 117(2). Secondly, it required the contribution to be calculated by the Valuer General. Of course, to the extent that this suggests that the payment must be in accordance with that determination, it would preclude the determination of the amount in accordance with s 117(3), and would therefore have the effect of ousting the legislatively conferred jurisdiction of the Court. A final matter which is relevant to the application of the section is that the council must receive the payment on trust as provided in s 117(5). In my view, it is clear that the section is intended to apply in the circumstances and must take precedence over the terms of the condition where they are in conflict. Accordingly, to the extent that the condition is inconsistent with the section, it is ultra vires and of no effect. In this case, the relevant parts of the condition are easily severed from the balance, leaving the balance valid and with legal effect.

33. This construction disposes of the jurisdictional questions. In particular, the calculation of the payment under s 117(2) was not dependent on whether security had been required or provided under s 117(1), and further that the dispute in respect of calculation fell to be determined as a disputed claim for compensation under the *Land Acquisition Act*. This will be so irrespective of the question of security but in any event it is clear that the relevant permit condition enlivened both ss 117(1) and (2). As events transpired, the appellant purported to make an actual payment to the respondent before the plan was sealed and did not seek to provide alternative security. This does not affect the question of jurisdiction. The amount that required determination was the "amount required to be paid" under s 117(2). It is common ground that the appellant's payment was made provisionally upon the final amount being agreed and that the parties have been unable to reach agreement. Accordingly, the Court had jurisdiction to determine the amount as a disputed claim for compensation under the *Land Acquisition Act*.

Determination of the amount

34. It is clear that the critical question that underpinned the approach of the valuers and the primary judge's determination of value is the date at which value was to be determined. This question, particularly in the circumstances of this case, was important because it affected both the data which informed the assessment of value and the stage of the subdivision process at which value was to be assessed. I am satisfied that this Court should construe the provision to answer the question of the timing and subject matter of the valuation for the purpose of s 117(3).

35. For the appellant, all three grounds are underpinned by the argument that the correct date of assessment was the date on which the final plan was first provided to the respondent by the appellant's surveyor under s 88(1), which is 19 January 2021. The appellant's argument relies on the wording of s 117(2). The section requires value to be assessed by reference to one twentieth (5%) of the whole area comprised in the plan "less any area provided for public open space in the final plan and any area created in the final plan of the littoral or riparian reserve bears to that whole area". The appellant notes that the definition of final plan means "a final plan referred to in s 87". Hence, the argument goes, value must be assessed at the date that the plan of subdivision first becomes the final plan by lodgement under that section.

36. There is a fundamental difficulty with this argument. While it is true that the calculation is to be made in accordance with s 117(2), and the section requires an assessment of value, it is, in contrast with s 116, silent on the question of how that value is to be calculated and in particular, the date of the assessment of value. In my view, there is no grammatical or logical connection in s 117(2) between the date of assessment of the value and the provision of public open space at the point that the plan of subdivision first becomes a final plan. On the contrary, the final plan remains exactly that through various iterations until it is endorsed by the Recorder under s 94. In my view, the reference to the final plan in s 117(2) is not intended to guide the question of when value should be assessed. It is intended to be a reference to the plan which will eventually become the sealed plan under s 94, which until that point proceeds through the process as the "final plan".

37. As already discussed, the better approach is to consider construction of the provision having regard to its legislative purpose and taking account of its place within the legislative scheme. The critical aspect of the legislative purpose of the requirement for such a payment is to place the council as closely as possible to the position it would be in if the relevant land had been set aside in the subdivision. In other words, the council should be able to receive from the developer a sufficient amount of money to enable it to purchase alternative land within the municipality to fulfill the function of public open space that would otherwise have been provided in the subdivision. This necessarily requires value to be determined for the purpose of assessment of the amount required to be paid under s 117(2) on the basis that the subdivision process has been completed, and the subdivided allotments will be available for individual sale. This approach is consistent with s 117(4)(a) which provides that the security that may be required includes a bond "of an amount in excess of any possible demand to secure payment of the amount within 90 days after the demand is made after the final plan is taken effect". The final plan takes effect as provided by s 94, that is when the Recorder has accepted the plan. At this point, the subdivision is, in practical terms, complete and it simply remains for the Recorder to follow through with the consequential formalities and machinery steps including the creation of any necessary folios. This construction accords with the legislative scheme which intends the payment in lieu of provision of public open space to be an alternative to the receipt by the council of actual land in the final subdivision. Section 117(5) makes it clear that the money is to be used for the acquisition and improvement of land for public open space for the benefit of inhabitants of the municipal area generally, and this can only properly be achieved if the council receives in lieu of actual land, an amount sufficient to permit the purchase of compatible land.

38. In my view, s 116 does not assist the construction of s 117 on this question. Section 116(6) expresses a legislative choice with respect to the date of valuation, but such a choice is conspicuously lacking from s 117. In any event, s 116 is concerned with a payment made for a different purpose and in different circumstances. Although it is part of the matrix of provisions which regulate the determination of the public open space issue at the time of planning approval, it is concerned with land which the council wishes to purchase in excess of five per cent of the subdivided land to which the council is otherwise entitled at nominal cost. It is concerned with the price council must pay for the extra land, not with the amount to be paid by the developer in lieu of provision of land which would otherwise be provided to the council at nominal cost. Further, although s 117(3) requires "the amount" to be determined as a disputed claim for compensation under the *Land Acquisition Act*, that legislation also provides little if any, guidance on the question of assessment. Section 27 provides that in determining compensation under the Act, regard is to be had to the market value of the subject land. This was the basis of the valuation methodology used by both valuers, but it does not assist with other aspects of valuation in particular the date of the valuation. Insofar as the *Land Acquisition Act* deals with such questions, its provisions are patently inapplicable to the calculation under s 117(2). Again, it is dealing with a different legislative framework, and subject matter and valuation for a different purpose.

Conclusion

39. Accordingly, I reject the submission made on behalf of the appellant that the date of valuation should be assessed at the time that the plan of subdivision first becomes the final plan upon being sent by the owner's surveyor to the council under s 87. The primary judge was substantially correct when he determined that the appropriate date for valuation was 16 September 2021, which he determined was the date when the respondent forwarded the final plan bearing its seal to the Recorder, so that the plan could be signed and dated and hence take effect in accordance with s 94. His Honour was therefore correct in his determination that the land was to be valued for the purpose of calculating the amount required to be paid under s 117(2) on an after-subdivision basis, which was consistent with the approach adopted by Mr Bevin. His Honour did not err when he accepted Mr Bevin's opinion evidence in preference to that of Mr Page. Mr Page's opinion was of no assistance because it was calculated at an incorrect date and on a before-subdivision basis. Although Mr Bevin had assessed value at a slightly earlier date than that on which the plan took effect under s 94, the ultimate determination of value was a matter for the primary judge. His Honour's view that the earlier assessment would make no material difference to the final value is unimpeachable.

40. For the reasons set out above, I conclude that the grounds of appeal are without merit. I would dismiss the appeal.



Council Policy
PUBLIC OPEN SPACE

Approved by: Council
Approved date: October 2024
Review date: October 2027

1. PURPOSE

The Southern Midlands Council is committed to providing public open space that is fit for purpose and valued by the community.

2. OBJECTIVE

To establish a consistent approach on the contribution of land for public open space and the calculation of payment instead of the land for public open space, as well as the associated expenditure of those funds.

3. BACKGROUND

The *Local Government (Building and Miscellaneous Provisions) Act 1993* (the Act) provides for Council to require land for Public Open Space or a payment instead of increasing public open space.

In addition to ensuring Council meets the requirements of the Act, this policy will:

- (a) Facilitate a strategic approach to the development of a contemporary and well-planned open space network.
- (b) Clarify the process of determining whether a land or cash in lieu contribution is required as part of a subdivision development.
- (c) Provide criteria for determining suitability of land contributions for public open space.
- (d) Provide guidance on how public open space contributions are to be determined and paid.
- (e) Provide guidance on how funds obtained through cash in lieu contributions will be utilised.
- (f) Ensure developers contribute to the provision of public open space commensurate with the scale and type of their development.

4. DEFINITIONS

Council	refers to the Southern Midlands Council.
Lot	means a block of land created by subdivision of a larger block of which it was part.
Public Open Space	means space for public recreation or public gardens or for similar purposes.
Subdivide	means to divide the surface of a block of land by creating estates or interests giving separate rights of occupation otherwise than by: <ol style="list-style-type: none">a) a lease of a building or of the land belonging to and contiguous to a building between the occupiers of that building; orb) lease of air space around or above a building; orc) a lease of a term not exceeding 10 years or for a term not capable of exceeding 10 years; ord) the creation of a lot on a strata scheme or a staged development scheme under the <i>Strata Titles Act 1998</i>; ore) an order adhering existing parcels of land.
Subdivision	means the act of subdividing or the lot subject to an act of subdividing.



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Suitably Qualified Person means a person who can adequately demonstrate relevant tertiary qualifications (or equivalent) and experience in a recognised field of knowledge, expertise or practice with direct relevance to the matter under consideration.

Valid Final Plan A final plan lodged in accordance with section 88 of the *Local Government (Building and Miscellaneous Provisions) Act 1993*.

5. POLICY

That:

- 5.1 Council will require a Public Open Space contribution by way of either:
- (a) A maximum of five percent of the subdivision land area that could result in an increase in demand for public open space allocated as Public Open Space and transferred to Council's ownership; **or**
 - (b) A cash in lieu contribution or either cash in lieu or land, equal to five percent of the ~~unimproved~~ **improved** value of the land ~~not~~ including the balance lot, that is calculated in accordance with s117(2) of the Act.
- 5.2 This policy applies to applications for subdivisions in the Southern Midlands, where new lots are created in the following zones;
- General Residential Zone
 - Low Density Residential Zone
 - Rural Living Zone
 - Village Zone
 - Local Business Zone
 - General Business Zone or
 - Otherwise where a subdivision results in an increased demand and/or utilisation of Public Open Space – to be assessed on its merits in accordance with this Policy.
- 5.3 This policy does not apply to strata developments, minor boundary adjustments where no new lots are created, or subdivision for the development of public utilities, as detailed in the Act
- 5.4 Public Open Space contributions in excess of this policy may be offered by the developer or in all other circumstances as resolved at a General Council meeting.
- 5.5 Cash in lieu is the preferred public open space contribution.
- 5.6 Council will use its discretion in determining the open space contribution and each subdivision will be assessed having regard to its individual facts and circumstances.
- 5.7 The Policy enables Council to refuse a subdivision if it is of the opinion that the proposal should be altered to include (or omit) public open space.

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Council Policy
PUBLIC OPEN SPACE

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6. GENERAL PRINCIPLES

6.1 Procedure to Determine Public Open Space Requirements

- 6.1.1 A decision as to whether land or cash-in-lieu is provided will be dependent upon whether the proposed subdivision provides an opportunity to obtain land that meets the public open criteria detailed at section 6.3 of this policy. If the development is not able to provide public open space of community benefit and in accordance with the criteria for land contributions, then cash-in-lieu will be required. The preference for land versus cash-in-lieu will be at the discretion of Council, based on:
- The attributes of the site
 - The need for public open space provision in the area
 - Benefit and value to the community
 - Compliance with this policy; and
 - Council's Community Infrastructure Plan or any future Public Open Space Strategy or public open space projects endorsed by Council.
- 6.1.2 The planning officer's report supporting the decision of a subdivision application must include a section as to how the relevant parts of the Act and this Policy are complied with.
- 6.1.3 If a subdivision contains land that is of strategic importance in providing for the open space needs of the community and those needs can only be satisfied by requiring public open space to be provided in excess of 5% of the area of the land in the plan of subdivision, Council may require an additional area (more than 5%) to be provided as public open space, subject to the payment of compensation in accordance with s116 of the Act.
- 6.1.4 Where a subdivision is submitted in the form of a staged development and a cash-in-lieu contribution is required, the public open space contribution may be paid proportionately in each stage, subject to a new valuation if required, in accordance with 6.2 below.
- 6.1.5 Where a cash-in-lieu contribution is required, the developer will be required to provide a current valuation of the land calculated in accordance with s117(2) of Act and prepared by a suitably qualified person, having regard to the following:
- (a) The valuation is to be based on the whole area in the plan of subdivision contributing to the increase in demand for Public Open Space, regardless of zoning and/or previous subdivisions, less any area provided for public open space in the final plan and any area created by the final plan of riparian reserve; and
 - (b) The valuation is to be based on the status of the land as at the date of valid lodgement of the final plan, inclusive of any works required to facilitate the subdivision but excluding any improvements in existence at the time of lodgement of the application for subdivision (i.e. existing structural improvements on the land, such as a dwelling/outbuildings); and
 - (c) The valuation must be completed not more than 3 months prior to the date of valid lodgement of the final plan, noting the requirements of Part 6.1.5 (b) above.
- 6.1.7 If Council doubts that the valuation provided by the developer properly reflects the true value of the land, Council reserves the right to ask for a revised valuation or seek its own valuation advice and, if necessary, refer the matter for determination as a disputed claim for compensation under the Land Acquisition Act 1993. Costs to be borne by the subdivider.
- 6.1.8 All money received by way of contribution shall be reserved exclusively for the purposes of Public Open Space acquisition or improvement.



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6.2 Payment of Public Open Space Contributions

- 6.2.1 The cash in lieu of land must be paid prior to the sealing of the Final Plan of Subdivision.
- 6.2.2 If a land transfer is to occur, it must occur at the time of the sealing of the Final Plan, or in a staged subdivision, as part of the first stage.
- 6.2.3 If this is not possible, the transfer is to occur as early as possible during the subdivision.
- 6.2.4 All monies received will be reserved exclusively for the purposes of Public Open Space acquisition or improvement in accordance with s117(5) of the Act and have regard to a future Public Open Space Strategy, Community Infrastructure Plan or any public open space projects endorsed by Council.
- (a) Expenditure from the Public Open Space Contributions Account (or similar) will be subject to a report to Council for consideration.
 - (b) Interest earned on the account will be rolled over into that account.
- 6.2.5 In a staged subdivision, payment shall be made in accordance with Part 6.4 of this policy.

6.3 Criteria for Land Contributions

- 6.3.1 A public open space land contribution will only be considered where one or more of the following apply:
- (a) Acquisition of the land would be consistent with one or more of the following principles
 - (i) it would further the objectives of any adopted Council policy or strategic document
 - (ii) the land is the best location for POS in the wider area where the subdivision is located and there is low likelihood of alternative sites
 - (iii) the land is convenient, accessible and fit for purpose (i.e. useable and not overly constrained by topography, shape, natural hazards or infrastructure) and does not duplicate nearby POS
 - (iv) the land provides or improves permeability through the site or surrounding area
 - (v) the land provides or improves connectivity to other existing or prospective open space areas or public destinations
 - (vi) the land enhances the general amenity and urban character of the area
 - (vii) the land has special values (for example native flora, fauna, habitat, geomorphology, water catchment or cultural heritage values)
 - (viii) the land provides for a riparian reserve or habitat corridor
 - (ix) the land can be sustainably managed having regard to Council's Community Infrastructure Plan.
- 6.3.2 Land used for the following purposes shall not be transferred to Council for public open space;
- (a) Stormwater detention basins or drainage swales and natural water courses that would otherwise form part of the overall drainage infrastructure within the subdivision
 - (b) Above or below ground infrastructure that would limit the use of the land or landscaping treatments
 - (c) Pedestrian footways or other kinds of ways dedicated under section 95 of the LGBMP Act;
 - (d) Inaccessible estuarine/riverine areas, and
 - (e) Land burdened by easements or rights of way.



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- 6.3.3 The following will be considered in the decision to acquire land for public open space;
- (a) the existing provision of public open space in the area
 - (b) whether the land is conveniently located to service existing or future users
 - (c) whether the land is fit for propose in terms of size, shape, topography, gradient, infrastructure, conservation covenants, is not burdened by evasive weeds or other encumbrances; Where evasive weeds are present, Council will require a financial contribution from the subdivider to eliminate evasive weeds to ensure the open space area is 'fit for purpose.'
 - (d) the extent to which the land contributes to Council's ability to support a diversity of recreational activities
 - (e) the biodiversity value, extent and condition of existing vegetation/habitat
 - (f) the best location for public open space in the area and the likelihood of alternative sites
 - (g) the land's vulnerability to natural hazards
 - (h) The size, shape and location of land is consistent with established *Crime Prevention Through Environmental Design* (CPTED) Principles.

7. LEGISLATION & RELATED DOCUMENTS

Local Government (Building & Miscellaneous Provisions) Act 1993

Local Government Act 1993

Tasmanian Planning Scheme

Tasmanian Open Space Policy and Planning Framework

8. DOCUMENT ADMINISTRATION

This Instruction is a managed document and is to be reviewed every three (3) years or as directed by the General Manager.

This document is Version 1.0 effective 28-08-2024. The document is maintained by Development & Environmental Services for the Southern Midlands Council.

SUBMISSION TO THE DRAFT SOUTHERN TASMANIA REGIONAL LAND USE STRATEGY
Southern Midlands Council - 17 February 2026

SUMMARY

The Draft Southern Tasmania Regional Land Use Strategy (STRLUS) is a missed opportunity to plan for a different future for Tasmanians, one in which there is a just society that gives equal opportunity and prosperity to all.

The Draft STRLUS entrenches the status quo, perpetuating the idea that change will be small, slow and incremental. It is not a plan for the future.

If this document is enacted, Tasmania's planning system will continue to exacerbate the housing crisis and private sector economic prosperity initiatives will continue to be frustrated.

Both the Draft STRLUS and the newly created Tasmanian Planning Policies fail to acknowledge the Housing Crisis and its social implications, let alone provide any pro-active policies to address it.

There are no future-oriented pro-active socio-economic strategies to avoid the future demographic doom projected by demographers. On current projections, Tasmania's population is projected to age significantly and grow little. The STRLUS does not set the scene for innovative, creative thinking and the ability to proactively pursue prosperity and development equity in pursuit of lifestyles and livelihoods in non-urban locations.

The Draft STRLUS perpetrates the existing approach rather than addressing the needs of the future.

It fails to recognise the role and accept the responsibility that policy instruments play in facilitating growth and economic prosperity.

However, Tasmania has the opportunity to learn from past mistakes and plan for a different future, now.

Community well-being is determined first and foremost by just access to, and equitable share of, the benefits of economic prosperity and its determinants.

This should be at the heart of the document; pro-active strategies and enabling aspirational targets, creating as many opportunities as is reasonably possible for equitable access to housing and economic prosperity.

1 FAILURE TO PROVIDE FOR ENOUGH FUTURE HOUSING

The Draft STRLUS and the recently approved Tasmanian Planning Policies (TPPs) do not provide for enough future housing opportunities.

1.1 No Acknowledgment of the Housing Crisis.

The Draft STRLUS is silent in regard to the Housing Crisis, which is the worst it has been in a century and would appear to be the most important issue to most members of the community.

The Housing Crisis is also not mentioned in the State's recently released TPPs.

Most worryingly, the Draft STRLUS (page 27) believes the current supply of residential land is sufficient:

“Pressures on housing affordability are likely to continue, but can be addressed at least in part, by ensuring capacity for new homes is maintained ...”

The Draft STRLUS (and the TPPs) adopts a 'business as usual' stance to the provision of new housing. There is no sense of urgency to ramp up housing supply or do anything different than we have done in recent decades.

The Housing Crisis has many contributing factors, only some of which are within the power of the State's planning system to address. Nevertheless the planning system is the foundation to motivate the Housing Crisis solution, and should contribute everything it can towards this aim.

1.2 No Mention of Tasmania's Housing Target Under the National Housing Accord.

Australia has set a nominal housing target under the National Housing Accord. Whilst this may be aspirational, it is important to acknowledge this target if we are to address the Housing Crisis and the impact of the lack of appropriate housing and housing certainty to a significant proportion of the population.

The National Housing Accord identifies that the rate at which we are providing new housing in Tasmania is half that necessary to address the Housing Crisis; 51%. (Refer table below.)

Yet both the Draft STRLUS and the TPPs pay no heed to Tasmania's housing target under the National Housing Accord. Questions over whether this target can be achieved or not in the short term are not reasons to ignore it.

It is the private sector that builds most housing. The planning system needs to make as many opportunities for development available as is reasonably possible.

Table O.1 Gross new housing supply by state and territory

State/territory	Gross new housing supply, 2019–20 to 2023–24	Gross new housing supply, 2024–25 to 2028–29 (forecast)	Implied share of 1.2 million Housing Accord target*	Gross new supply forecast, ratio to share of target* (%)
New South Wales	251,000	246,000	376,000	65
Victoria	306,000	300,000	306,000	98
Queensland	166,000	194,000	246,000	79
Western Australia	75,000	105,000	129,000	81
South Australia	54,000	59,000	84,000	71
Tasmania	16,000	13,000	26,000	51
ACT	24,000	16,000	21,000	78
Northern Territory	3,000	4,000	11,000	31
Australia	896,000	938,000	1,200,000	78

* The Council has apportioned the Housing Accord target to each state and territory by using their share of the national population in December 2022 (these were the latest population figures available when the 1.2 million Housing Accord target was agreed in August 2023). Of the available options for calculating jurisdictions' share of the target, the Council has chosen this method because it is transparent and stable over time. Alternative approaches could be based on projected population growth over the Housing Accord period. The calculated target shares do not reflect formal agreement between the Australian and state and territory governments.

Note: The sums of figures across states and territories may not align with national totals due to rounding.

Source: NHSAC 2025, ABS National, state and territory population 2023 and ABS Building Activity 2025

1.3 A Passive Acceptance of Demographic Projections and a Lack of Aspirational Planning

Population and demographic projections claim that, over the next 25 years:

- Tasmania's population growth will slow down.
- Tasmania's population will age significantly .

Instead of passively accepting this future, we should take the demographers' projections as a <u>call to action</u> .

The focus on future growth needs to be balanced with consideration of shortfalls. There is a shortfall in many of the capital domains on which social inclusion and economic prosperity are based. Even if population growth was to slow, the unmet housing demand necessitates the provision of zoning to support this. This is a critical factor in regional towns and settlements.

An acceptance of projected slow growth creates a false narrative that Tasmania is doomed to spiral downwards to become little more than one large retirement village. If we put aside the fundamental flaws in arriving at this conclusion described elsewhere in this report and accept slow growth projections as true, then the question is: what do we do with this information?

The response from Tasmania's State planning system is to passively accept this doom and adopt it as the premise on which the amount of new residential zoning is based.

Further, it does not consider the critical complementary commercial and industrial facilitation necessary to ensure local areas work and provide opportunities that would attract a new younger cohort and start to reverse demographic trends. The proposed expansion of the Boyer industrial precinct is not promoted in the Draft STRLUS.

As demographer Lisa Denny states, this demographic doom is: *"what will happen if nothing changes"*.

This paper contends that changes will be of two types:

- Those changes that will be thrust upon us no matter what we do:
 - For example: climate change-driven immigration to Tasmania, which will further drive demand for housing in the future.
 - Tasmania could, in fact, view this as an opportunity to proactively attract a younger cohort of migrants.
- Those changes that we choose to make.
 - Proactive, forward looking socio-economic strategies.
 - Tasmania's own policy of hydro-industrialisation in the 20th Century is one example. Hydro-electric schemes and the new industries they facilitated enabled Tasmania to absorb large numbers of post-war European migrants and their families whilst providing plentiful jobs and improving overall living standards.
 - The Republic of Ireland's dramatic change in thinking that led to its 'Celtic Tiger' economic revival is another example. The term refers to the economy of Ireland from the mid-1990s to the end of the first decade of the 21st century, a period of rapid real economic improvement and population growth. Although certain mistakes paused the revival for seven years after 2010, the economy is now growing significantly again and Ireland is no longer the poor Western European backwater it once was up to the 1990s.

In short: strategic land use planning does not have to be passive and reactive. It can be – and should be – proactive and aspirational.

Tasmania can choose to aim to change the future from what it would otherwise be.

But this will need a fundamental change in mindset within the State's planning system.

This is particularly relevant to socio-economic planning (refer Chapter 2, below) but is also highly relevant to the provision of land for housing supply. Indeed the two are intertwined.

1.4 Confusing Past Supply with Past Demand

A critical flaw in the State Planning System's approach, which is instilled in the Draft STRLUS, is in adopting past housing / population growth as a proxy for demand when the region has undersupplied housing for decades.

If historical supply had matched demand there would be no Housing Crisis.

This method has continuously underestimated past demand. These numbers are then projected into the future and planned upon, continuing to exacerbate the Housing Crisis.

Local government, embedded within the community, knows that demand exists through regular and consistent anecdotal evidence. Yet this evidence is not accepted by the State's planning system that only recognises the above (flawed) statistical methodology.

A clear way of understanding this 'self-checkmate' is: if there are no lots available then there has been no growth in the new housing. The State's planning system wrongly interprets this statistic as clear evidence of 'no demand'.

This results in rural communities being stuck, with no new residential land being allowed by the State, despite strong demand. The same applies on a bigger scale to greater Hobart.

Past statistics are from a constrained world in which the supply of residential land development opportunities has not equalled demand. This fundamental fact appears to be unrecognised within the State's planning system.

For the State's planning system to continue to demand that Council Planning Authorities provide irrefutable evidence that demand exists before allowing new rezonings in the middle of the worst Housing Crisis in a century fails to comprehend that factors that are constraining real/actual supply of land and the provision of housing.

Rather than using past supply statistics, factors such as vacancy rates and local employee shortages are preferable measures of housing demand.

A new STRLUS presents an opportunity for Tasmania's planning system to set pro-active strategies. We should not defer this major reset to a future iteration of the STRLUS.

Every *reasonable* measure that can be done to increase housing supply must be done. We should at least base our housing target on our share of the National Housing Accord target. This is double the growth envisaged within the Draft STRLUS.

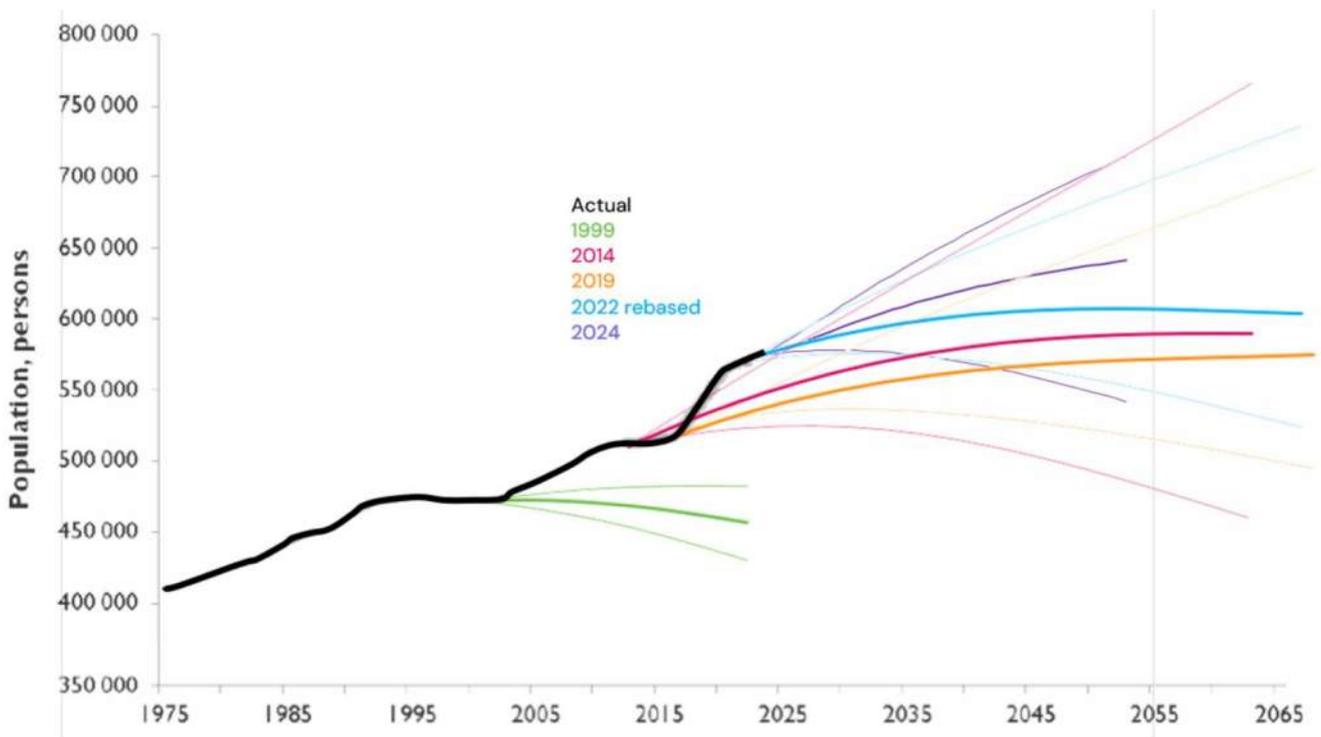
We will need infill and greenfield in Greater Hobart plus growth in rural towns if we are to make a dent in the Housing Crisis.

The following chart demonstrates the State's chronic problem in underestimating demand. Population projections have consistently undershot the eventual reality.

Accepting population projections as inevitable was a mistake of the 2010 STRLUS.

We are now making the same mistake, with RemPlan and Treasury both projecting a slowing growth rate over the next 25 years.

The real world consequences of underestimating demand are worse than those of over estimating demand, as the depth of the Housing Crisis attests.



The State's planning system metes out new residential zoned land that is the *minimum theoretically necessary*. However, it should be allowing the *maximum reasonably possible*.

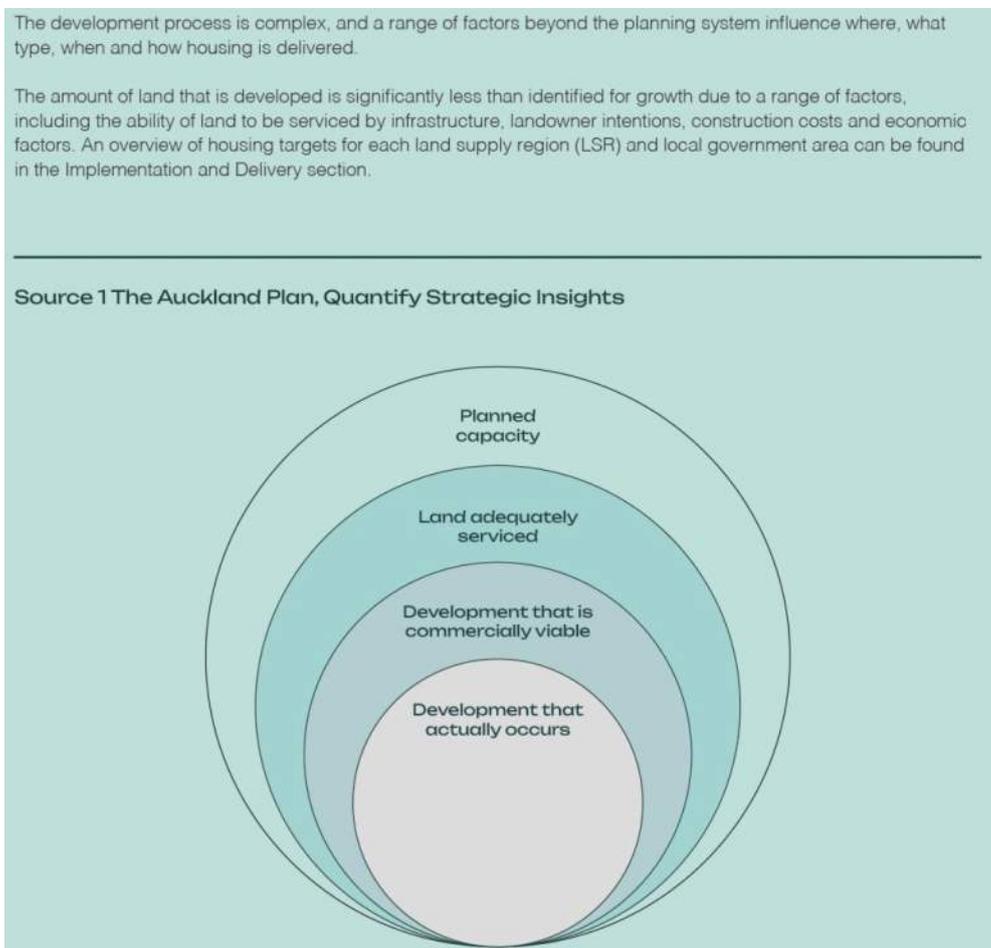
We have created an artificial shortage of zoned land, and when supply does not meet demand, prices go up. This pushes up prices at all levels of the housing market, with the most socially disadvantaged at the bottom of the market missing out. Home ownership aspirations and rental opportunities are equally impacted. The law of supply and demand is as real and omnipresent as gravity.

Additionally, the minimal opportunity conditions we have created has encouraged land-banking by property developers.

1.5 Maintaining Unrealistic View of the Residential Land Supply Industry

Once we have determined how much land we need for new housing, even more land should be made available given the Tasmanian market particularly does not operate in accordance with neat planning theories.

The following diagram from the Auckland Regional Plan (recently adopted into the Adelaide Regional Plan) illustrates the need to zone more land for residential use than we might calculate we need:



The diagram recognises the real relationship between land zoned for residential development opportunity and real-world residential land supply. This is the difference between theoretical supply, as imagined by the State's planning system, and the real-world supply that materialises in practice.

Put simply: landowners and developers do not line up to subdivide on time and in sequence in an orderly fashion. Our strategic planning system has to account for this fact.

In addition to the above, the Draft STRLUS does not understand that inner city medium density dwellings are relatively expensive to construct and are not the preferred option for the majority of the population. Whether we like it or not, many people prefer a detached house with a front and back yard and many don't mind living on the city fringe or in country towns. Contrary to the thinking underpinning the Draft STRLUS, greater Hobart is not of such a size that it experiences all the very real problems of much larger cities.

The longer we ignore these realities, the longer the Tasmanian planning system will remain part of the Housing Crisis problem and not become part of the Housing Crisis solution.

More land needs to be rezoned for potential residential use, not less.

This will need a major change of mindset within Tasmanian's State planning system.

To signal such as change of mindset, in the new STRLUS the chapter titled 'Growth Management' should be renamed to 'Growth Facilitation'.

1.6 Ignoring Future Climate Change Driven Immigration

Increased climate change-induced immigration to Tasmania will occur over coming decades.

Tasmania is unique within Australia in this respect. To deny this is to deny Climate Change, the extremes of which will be felt most acutely on the Mainland.

Local Government, embedded in the community as it is, is aware of regular and consistent anecdotal evidence that it is already happening. People are moving to Tasmania now and are giving Climate Change as either the main reason or one of the reasons for doing so.

As Climate Change worsens, the mainland heat extremes and other catastrophic weather events will get worse. Whatever the current increase in demand is that is driven by climate change now, we have to acknowledge that it will increase in the future.

Whilst this is difficult to objectively measure, the STRLUS should recognise this impact on future housing demand and enable increases in housing supply opportunities without necessitating amendments to the STRLUS.

A small ripple outwards from the mainland States will manifest as a large increase in demand for Tasmanian residential properties – which will come on top of the existing unmet demand.

Population projections based on past statistics do not, and cannot, take Climate Change-driven immigration pressure into account. RemPlan and other demographers have all stated that the reason for not considering this issue is because they cannot gather data on something that will happen in the future. However, this is no reason for the State planning system to ignore this issue.

It is not the function of demographers to assess this future influence. (They can only look backwards for statistical evidence.)

But it is the job of the planning system to plan for the future. In doing so it must take into account a range of information and 'future thinking'. Not just past statistics.

This is one of many reasons why the STRLUS and the Tasmanian Planning Policies must be forward-looking, aspirational and pro-active.

1.7 A Flawed Concept of Growth Boundaries.

The Settlement Growth Boundary (SGB) / Metropolitan Urban Boundary (MUB) concept within the STRLUS is mis-construed and is not fit for purpose. This is because:

- It is merely a short-term guide to the next parcels of land to be rezoned to residential use.
- It does not have a sufficiently long time horizon to be otherwise useful.
 - The stated minimum 15 / 25 year time horizon implies that a longer timeframe could be envisaged. However, past practice has shown that the State's planning system will take the stated range as the limit.
- The major infrastructure providers, for instance, would prefer a much longer timeframe, even a 50-year time horizon, to adequately plan their infrastructure.
 - Infrastructure providers build assets with 50-100 year lifespans. They need to size the major components properly.
 - Without longer timeframes, calculating headworks charges per new lot in a given future catchment area becomes difficult or impossible. The total long term catchment is needed, not a short term increment.
 - This would appear to be a major reason why TasWater has adopted its 'first mover pays' policy, (a policy that is causing bottlenecks in the supply chain for new residential lots).
 - At least one of the major infrastructure providers, frustrated with the State planning system's short time horizons, has now started developing their own 50-year growth estimates.

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- The current SGB/MUBs are based on ‘population projections’ which are fundamentally flawed for multiple reasons described elsewhere in this report.

SGBs, if we must have them, should be the outer long-term limit of settlement growth.

- They may approximately identify land that will be used for residential, commercial, industrial, open space or recreational, etc. purposes, but should not pre-define such spatial arrangements.
 - This thought process should be left to local structure planning.
 - The level of detail imposed by the State should only be that necessary to address genuine regional strategic land use planning matters.
 - The current highly definitive approach represents a major over-reach by the State’s planning system in local-level planning issues. These should be left to the Council Planning Authorities and their local communities.

Concerns expressed by the TPC in its 2025 recommendation to the Planning Minister that an expanded MUB around Greater Hobart will lead to un-serviced residential lots and/or an over-supply of residential lots were unfounded:

- Drawing a MUB is just the first of three major decision points before new residential lots materialise.
 - The rezoning stage is the second, at which the issue of infrastructure servicing is explored in a general sense.
 - The subdivision development application stage is the third, at which the infrastructure servicing issues are addressed in great detail. It is simply not possible for a residential subdivision to receive planning approval unless the servicing challenges have been resolved between the proponent, the infrastructure providers and the local Council.
 - Then, there is ultimately a judgment of financial risk on the part of the developer, taking into account the cost of development (which includes infrastructure costs) balanced against the potential sale value of the lots and the time in which it will take to sell them. If the sums don't add up to an acceptable risk, the proponent will not go forward with the development of the subdivision.
- The Free Market will ensure there is not an over-supply of lots. If we ever approach the point of market over-supply, this will send a price signal to developers who will slow or cease investment in subdivisions. (Tasmania’s housing supply system is a long way from this point, as the depth of the Housing Crisis attests, however.)

The fact that the Tasmanian Planning Commission apparently does not understand the above is deeply concerning. Yet it perhaps goes some way to explaining past refusals of rezoning applications put forward by Council Planning Authorities.

SGBs are not the only way to manage growth in rural communities and represent an unnecessary restriction. They preclude the consideration of opportunities not previously recognised, however this does not make these opportunities inappropriate. The tests contained in the LUPAA and other, aspirational, considerations in the STRLUS can analyse whether expanding opportunity for growth is appropriate or not, in a locality.

Issues such as intentional or unintentional land-banking by landowners and old zonings on undeveloped land that have services or environmental constraints, or landowner intentions to maintain the status quo, are matters that influence whether residential land supply in a given town is effective, affordable and realistic.

Within the Draft STRLUS there is no recognition of the complexity of the economics of developing land, only a fixation with drawing lines on maps to set where the growth is imagined to go in the short term.

A MUB around Greater Hobart is justified on the basis that it is one large settlement cut up between multiple Council Planning Authorities and there are issues that need to be managed in a coordinated way across the area.

This is not that case for rural towns, however, and the content of the STRLUS needs to be altered to reflect that the strategy of mapping growth boundaries is just one way to achieve the objectives, but is not prioritised above an appropriate demonstration of alternatives, or additional opportunities. In this way, settlements that are better placed to set out the preferred growth areas within a spatial boundary can implement this, however, this does not preclude other settlements or areas demonstrating sustainability through growth on a case-by-case basis, as the detailed work is undertaken.

Whilst the case exists for a MUB around Greater Hobart, the criticisms in this paper on the State planning system's *concept* of a MUB / SGB remain valid. The proposed Greater Hobart MUB is also short sighted spatially & temporally, and in the range of issues it ought to address.

1.8 Unnecessarily Micro-Managing Rural Communities and Regional Towns

The draft STRLUS appears to be written on the presumption that it knows everything at the local level.

Rural towns do not need Settlement Growth Boundaries. They particularly do not need them imposed in an adversarial manner by the State's centralised planning system.

Nor do they need the 'de facto' Settlement Growth Boundaries set up by the Draft STRLUS through which towns with no SGBs are limited to existing zoned land.

This paper contends there are no regionally significant strategic planning reasons for SGBs.

There is no flexibility to consider the practical realisation of currently zoned land and the maintenance of a rolling reserve of land supply that is practically and commercially developable.

The imposition of growth restrictions on rural towns is unnecessary and will impede any future revitalisation opportunities. For example the instance where a significant employer may be looking for a new location. Having to amend the planning scheme to allow for such opportunities is often enough to dissuade such potential investors, but having to amend the STRLUS would most certainly spell the end of any such opportunity.

We need a planning system that is nimble and responsive if it is to be a facilitator rather than a hindrance in revitalising local communities.

Overall, the underlying intent of the Draft STRLUS appears to be to direct population growth and economic opportunity to Greater Hobart, away from rural communities.

This demonstrates a failure of the State's planning system to understand the function of rural towns as service centres to surrounding economic engines, which are mostly made up of export-oriented industries that bring new prosperity into the Tasmanian community.

Generally speaking, there are no regional strategic planning ramifications if these towns grow and it is unwarranted for the centralised State planning system to interfere with the aspirations and sustainability of rural communities and their surrounding rural industries.

What matters regionally is where the next 100,000 people will live in and around Greater Hobart, not whether rural towns grow by a hundreds of people.

Essentially, rural towns perform different and more significant functions than similar sized urban/suburban centres within Greater Hobart. In the Draft STRLUS they are treated the same.

The Draft STRLUS appears to be written with a focus on metropolitan planning practice and has a simplistic view of rural and regional areas, failing to recognise the role that these localities, settlements and communities have in the broader economy. (An example of this is the significant over-use of the Agriculture Zone in the Southern Tasmania that was imposed by the State's planning system.)

1.9 Pursuing the Perfect at the Expense of the Good.

The TPPs and the Draft STRLUS demand the perfect pattern of growth and development. This approach often leads to development stagnation, expansion choke-points and exacerbates land-banking.

Greater Hobart is not a big city with all the problems of a big city demanding rigid-theory strategic land use planning solutions.

The biggest problem by far, right now, is the Housing Crisis.

We need to find many new housing development opportunities for both the private and public sector to take up that provide reasonably acceptable outcomes.

We need many *reasonably well* located homes, not few *perfectly* located homes.

Pursuit of the perfect is the enemy of the good.

2. ABSENCE OF ASPIRATIONAL SOCIAL-ECONOMIC PLANNING (Section 2 by Dr Martin Farley)

Policy instruments influence a place's culture, for better or worse.

The Southern Tasmanian Regional Land Use Strategy (STRLUS) is a key governance instrument establishing the policy, rules and practice of land use and development within the region. It ought to be the link between the values and aspirations of the Southern Tasmanian community and how they manifest in the development & sustainability of the natural and built environment.

While it may be considered a technical document which identifies "we can develop and do this here and not there", the strategy is a translation of principles and values which intersect those of the community, impacting how the Southern Region and locales works as a place, how industry sectors and businesses operate and how people pursue aspirations, livelihoods and live. If the governance strategy is "out of synch" with the aspirations and values of the regional community, the whole system is compromised.

Government policy instruments are important; they establish the stance which frames the rules. In the land use context, stance can be considered on a continuum from constraining, through benign to facilitative to active promotion, the STRLUS is considered heavily weighted to a constraining stance when considering the dynamics of a place.

The test for the STRLUS is the degree to which it supports the aspirations of the community, reflected in Council Strategy, State Government and industry policy and individuals' business and life goals, while mitigating harm to others and the environment in the short and longer term.

This analysis concludes that the STRLUS does not reflect a strategic or dynamic futures orientation and facilitative stance and requires a pivot to this perspective to enable aspiration, creativity and innovation in the built and developed environment, simultaneously addressing strategic region wide challenges such as housing shortages.

The regional dimension is important but can mask local perspectives. Central Highlands is an example where the renewable energy investment pipeline shocks the way in which the community operates and the constraints of the planning system create an opportunity cost to the community by being unable to efficiently provide local housing and to convert jobs into resident population. This flows on to create challenges in providing the local service infrastructure which underpins liveability and wellbeing and therefore resident attraction – a cycle of growth or decline centred on land use.

Dynamics and the places and policy instruments to beneficially exploit or mitigate those dynamics is important, particularly when the strategy is designed with a focus on stability and order that is illusional at the margins. Smaller, rural LGAs are susceptible to shocks and flow-on which would not statistically significantly impact a city, attraction of a new business, or a small industrial sub-division.

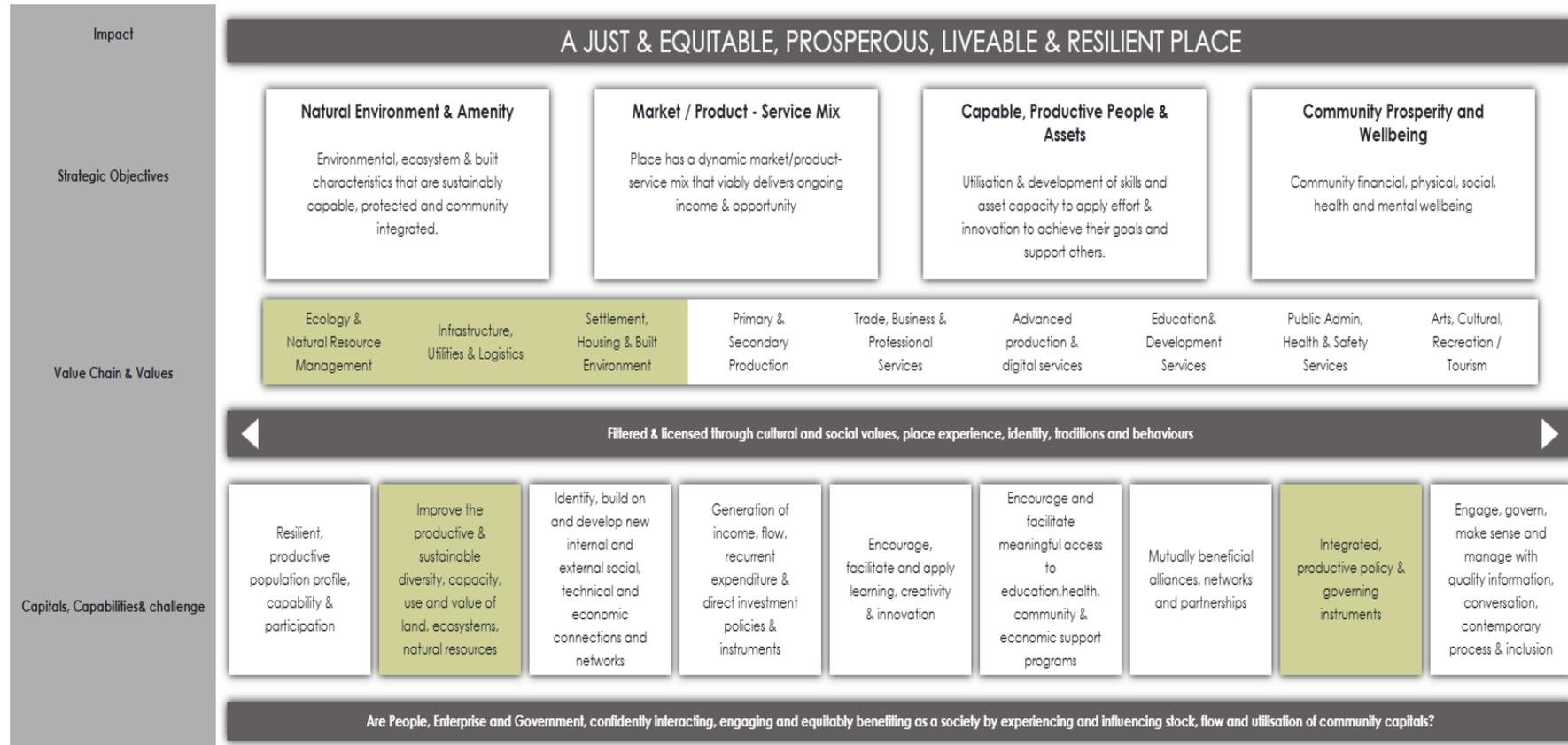
In the absence of dynamic rules/conditions a just & equitable process to ensure the pathway to rezoning and locally significant development is not limited to those resourced with high levels of capital and capacity to carry risk is required. Such processes exist in other jurisdictions.

The figure below is a general representation of the factors that frame how a place operates, that is how it applies along with its natural environment, population, capital base and formal/informal institutions and relationships. These capitals work together as inputs into what people and organisations do to deliver & create value for themselves, domestic/export markets and the community and simultaneously contribute to the strategic objectives which frame sustainable liveability and wellbeing.

The STRLUS is a component of the region's community capitals profile, highlighted in green it identifies the relationship the STRLUS as a governance instrument and its impact on the achievement of "best use" of land so that the first 3 elements of the value chain are optimised so that the following elements of the value chain deliver opportunity and just equitable participation and benefits in the short to medium to longer term.

The framework is scalable, the principles and dimensions applying at both the regional and local levels.

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3. FLAWS IN THE PROCESS OF DEVELOPING THE DRAFT STRLUS

3.1 Insufficient Input from the Local Elected Representatives of the People

The process to develop the Draft STRLUS was flawed.

Policy was developed without any formalised or methodical input from the locally elected representatives of the people. The Councillors.

There was no systematic pro-active empowerment of the elected representatives to input the values and aspirations of the people they were elected to represent.

This is one of the reasons why the Draft STRLUS is devoid of practical aspiration and pro-active socio-economic development thinking.

It should be recognised that the will of the people - their hopes, aspirations and values – flows through their elected representatives, first and foremost.

The will of the people is not garnered through public consultation processes, especially those that engage with a tiny fraction of the population.

3.2 The State of Play Report Included Statements of Policy and Values - Without Input From, or Endorsement of, the Local Elected Representatives of the People.

The State of Play Report was intended to be merely a statistical snapshot of the current situation in the region. It was to contain objective information only, and was not to venture into the realm of policy creation or value judgments. As such, it was reasonable to determine that it did not need the endorsement of the local elected representatives of the people.

However, its 'Region Shapers' are clearly policy positions.

These have been transferred into the Draft STRLUS and now clearly function as 'given' policy positions.

Yet they were never put to the elected representatives of the people for endorsement.

The Draft STRLUS should not imply that Council elected representatives share any measure of political accountability for them.

If they are to remain, it is the Planning Minister that will have to accept full political accountability for them.

3.3 Preliminary Draft STRLUS Not Referred to Councils for Input.

The original Project Plan for the development of the Draft STRLUS, as agreed between the State and the 12 Councils, included a stage where a penultimate draft would be formally referred to Councils for endorsement (or otherwise) before a draft was finalised by the Minister for general public consultation.

This would have at least gone a little way to addressing the lack of formal engagement with the local elected representatives.

However, this critical stage was abandoned, seemingly at the last minute, and the Draft STRLUS went directly to general community consultation from the Minister's office.

Therefore, statements within the Draft STRLUS that it has been developed 'through a collaborative process with the 12 Southern Councils' are, at the very least, an exaggeration.

4. SUBMISSIONS ON THE DETAIL OF THE DRAFT STRLUS

As detailed above, this paper contends that there are major foundational flaws in the Draft STRLUS which, largely, are unable to be remedied through amendments to the text. This is particularly relevant to the way rural areas and towns are dealt with.

In general, these constitute an unnecessary centralist overreach into micro-managing local planning matters that have no regional implications. These will need to be remedied by removal from the document.

Nevertheless, the following issues are raised which could be addressed through changes to the text:

4.1 Tasmanian Aboriginal People Today.

On page 5, the Draft STRLUS states: "*The only survivors of the invasion of Lutruwita/Tasmania were the people of the Northeast Nation As no other nations survive ...*"

The implications of this is that only people who trace their ancestors to Aboriginal people who were removed to Flinders Island by Robinson are Tasmanian Aboriginal people.

This is not supported by Council, as there are Tasmanian Aboriginal people in the municipality who are descended from those that were not taken to Flinders Island.

Refer to the case of *Shaw v Wolf* (1998) 83 FCR 113 or (1998) 163 ALR 205 where the Federal Court of Australia clearly defined aboriginality in Tasmania.

4.2 Significant Agricultural Land:

The refocussing of the protection of agricultural land to 'prime and significant agricultural land' is supported, as it represents an effort to return to dealing with this issue as Parliament intended in the State Policy on the Protection of Agricultural Land.

This needs to translate into a significant general reduction in the amount of land in the Agriculture Zone in Southern Tasmania, which should be rezoned back to the Rural Zone.

It is noted that 'significant agricultural land' is not defined in the Draft STRLUS, which is a major deficiency.

The most practical and cost effective way to address this deficiency is to take the Significant Agriculture Land map from the 2010 STRLUS and add any new irrigated farmland areas. Then define the term 'significant agricultural land' via this map. (The map can then be updated as considered necessary.)

This would cover-off on more than 99% of this issue and avoid the need for costly and time-consuming re-definition processes that would have to occur within the ongoing operation of the STRLUS, at the expense of Council Planning Authorities.

The reasons that the Agriculture Zone should only apply to prime and significant agricultural land include:

- The Agriculture Zone is restrictive for rural use/development other than agriculture. It omits a range of potential rural and complementary industries.
- The Rural Zone is a multi-purpose zone (including agriculture) and therefore provides opportunity for a broad range of economic activity.
- Poor-to-average agricultural land is abundant in Southern Tasmania and the change of relatively small proportions to another industry would not constitute a significant loss to Tasmania's agricultural estate.
- The Agriculture Zone when applied to poor-to-average agricultural land does not provide for its highest and best use, and therefore represents a socio-economic opportunity cost.
- Significant remnant priority vegetation is better recognised and protected in the Rural Zone.

4.3 Figure 13, Map of Agriculture and Marine Farming Areas:

The 'Agriculture Area' depicted on this map should be recast as the Significant Agriculture Land Area, using the above suggested methodology.

4.4 In the Glossary: Definition of Working Forest:

The definition of 'working forest' would appear to omit native forestry on long-term rotations. For example: 75-95 years for sawlogs. A future legal interpretation of 'in continual harvesting' could well exclude such forests.

4.5 Sustainable Economic Growth Chapter: Renewable Energy:

New major renewable energy projects will play a very significant role in boosting the region's economic prosperity in the future. The impact on Tasmania's economic prosperity has the potential to be as significant as hydro-industrialisation was in the 20th Century, but **only if** we adopt aspirational pro-active strategies to facilitate this new future and eliminate unnecessary blockages.

A new STRLUS has the potential to do this. We need to do this now, and not put it off to a future iteration.

For a start, Renewable Energy should be added to the top of the first paragraph of this chapter.

Further, this potential is much more than renewable energy 'supply'. It is in attracting future oriented industries that want to take advantage of this supply.

4.6 Sustainable Economic Growth Chapter: Linkages

This chapter should include explicit acknowledgment of the crucial link between zoning for identified economic engines, (those which generate export income and create local supply opportunities) and the role of service centres in supporting the transactions which generate income multipliers. This is particularly crucial for rural areas, and very important for New Norfolk.

4.7 Table 9, Activity Centre Categorisation:

The definition of Neighbourhood Centre under 'catchment' makes sense for centres in metro Hobart, but does not make sense in a rural context where towns are often spatially spread out to a greater degree. In the rural town context, the community that a Neighbourhood Centre services is not the 'immediate community' within the 200-400 metre nominated distance for residential consolidation. This will not make sense in the particular context of many towns. It is proposed that a new definition for this concept for rural towns be developed or the STRLUS should leave this to local structure planning by Councils and their local communities.

4.8 New Rural Residential Land and Existing De-Facto Rural Residential Land:

The removal of the existing STRLUS mechanism enabling the recognition of de-facto rural residential land use is a flaw. Such land, if zoned Rural Living, represents 'low hanging fruit' in terms of increasing potential housing supply with no adverse land use planning consequences and no loss of viable agricultural land.

Indeed, the major impact would be a positive one, wherein residents obtain a planning scheme zone that matches their existing lifestyle, with some gaining the option of subdividing much-needed new residential blocks.

It is noted that the draft STRLUS and the TPPs appear to have taken on an almost fanatical opposition to new rural residential land. This is at odds with similar jurisdictions in Australia and New Zealand.

This is opposed as there are some situations around Southern Tasmania where the highest and best use of land is this land use.

Rural residential lots are in demand and they enable a range of hobby farming semi-commercial enterprises as well as environmentally self-sufficient lifestyle choices.

Typically, these are on mediocre agricultural land that is already fractured into non-economically viable lot sizes in locations that do not threaten commercial agriculture or other rural industries, are not subject to unacceptable risks and do not contain high-priority environmental values.

4.9 No Distinction Between Local Industrial Land and Export-Oriented Industrial Land

3.4.2.5 should include a statement to acknowledge that demand for industrial land can only be projected for local service industrial land, not for the export-oriented industries that require locations outside existing industrial precincts.

A supply and demand analysis for industrial land can only be undertaken for local industries servicing the needs of a local population. It cannot be done for many export-oriented industries which often have unique and unpredictable locational requirements.

The planning system needs to make as many locational options potentially available as is reasonably possible and appropriate. This is particularly relevant to the omission of Boyer from the list of Regionally Significant Industrial Precincts (and the corresponding omission from the State's new Industrial Land Study which was released in mid-February 2026).

If a planning scheme amendment is required or, worse still, an amendment to the STRLUS, the delay, cost and risk to potential investors will usually mean that these economic prosperity opportunities will be lost to Tasmania before they get to first base. Investors will look elsewhere in Australia or overseas to put their money.

If we do not provide flexibility and insist on locking in the deterministic approach embedded in the Draft STRLUS, Tasmania will be overlooked for some potential new jobs & investment, and the future economic prosperity of Tasmania will be lessened.

4.10 No Mechanism to Enable Balanced Decisions at the Tasmanian Planning Commission.

The Draft STRLUS includes a chapter on protecting environmental values (Section 3.2) and a chapter on sustainable economic growth (Section 3.4).

Council supports the economic growth strategy statements and the general intent of the statements on protecting environmental values, (whilst noting that there are multiple pieces of existing legislation and regulation that already achieve what the chapter on Environmental Values purports to do).

The greater concern is that there is no detail or mechanism for balancing the drive to protect values with the economic development imperative.

This will mean that the need to protect relatively small values will over-ride significant economic development initiatives in Tasmanian Planning Commission considerations of proposed planning scheme amendments.

The same fate will befall many opportunities to increase housing supply.

Given the plethora of existing environmental protection legislation and regulation that any proposed development must comply with, it can be argued that what Tasmania doesn't have, and that the STRLUS should provide first and foremost, are economic development strategies and residential supply targets.

4.11 Recognition of Brighton as a District Centre

Council wishes to express its concern regarding the classification of Brighton as a Neighbourhood Centre within the draft STRLUS activity centre hierarchy. Council disagrees with this designation and considers that a higher-order classification to be far more appropriate.

Brighton functions as a key major hub for Southern Midlands, providing access to a wide range of higher-level goods and services not available at Oatlands. (This role is clearly demonstrated in the Spendmapp data included in the Brighton Activity Centre Strategy.)

It is critically important that the centre continues to evolve and prosper to meet the needs of our municipality. Recognising Brighton as a higher-order centre—a **District Centre**—would more accurately reflect its current and future role as identified in the extensive work undertaken in the Brighton Activity Centre Strategy, and help ensure the level of investment, activity, and services necessary to support its ongoing growth and long-term viability.

The Brighton township functions as a major regional hub for higher level retail, health services, education and a significant employment centre.

5 SOUTHERN MIDLANDS COUNCIL – SPECIFIC ISSUES

5.1 Categorisation of Oatlands as Service Hub

In Table 2: Oatlands should be recognised as a **Service Hub**. It is the hub for a large spatial area of Tasmania. It contains the council offices, district primary and high school, visitor infrastructure, Midlands Multipurpose Health Centre, Midlands Aquatic Centre, ambulance station, fire station, SES station, police station, pharmacy, service station/fuel, mechanics service, two hotels, bottle shop, IGA supermarket, childcare centre, bank branch, post office, Service Tasmania centre, rural supplies store, livestock saleyard (the only one in Southern Tasmania), multiple cafes and antique shops, Callington Mill Distillery, Centre for Heritage, multiple churches, 22 independent living units, retirement village, golf club, football club and sports oval, bowls club, RSL club, equestrian precinct and other community assets and facilities.

Furthermore, it is an hour drive from Greater Hobart, raising its importance as a centre for services to surrounding economic engines.

5.2 Settlement Growth Boundary - Oatlands

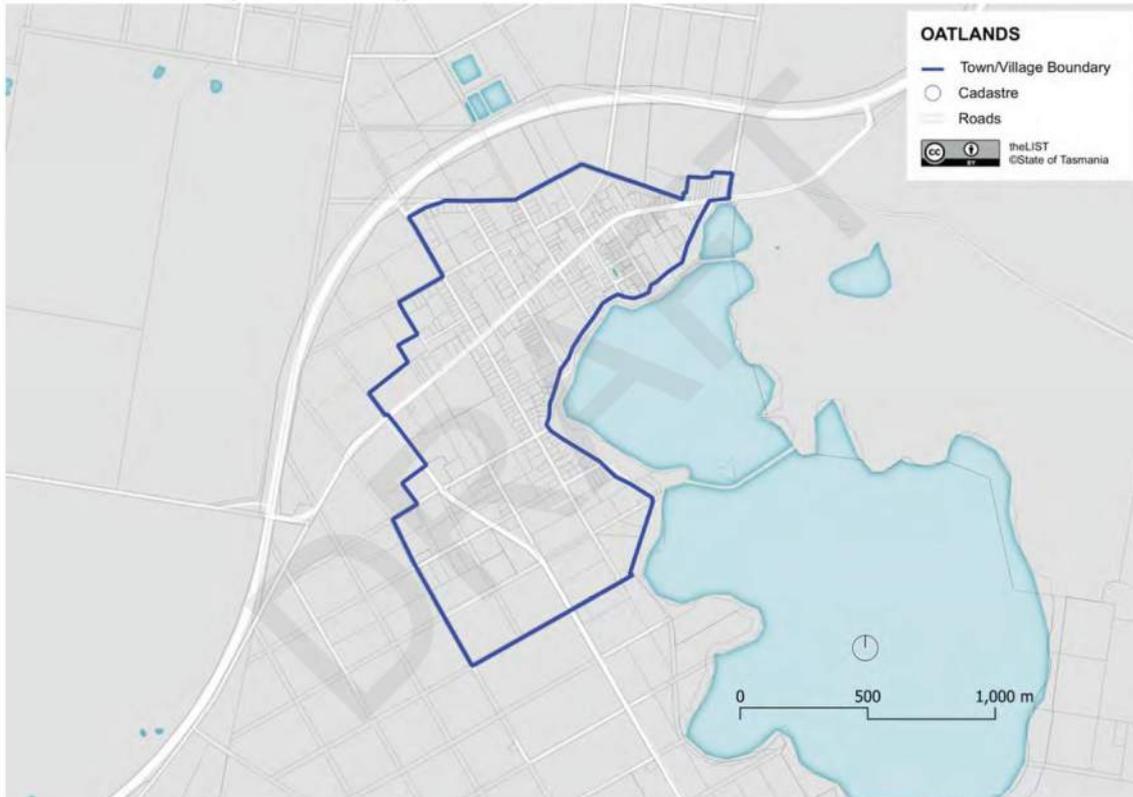
Oatland is proposed to have a mapped Settlement Growth Boundary (SGB) in the STRLUS. This is mostly in accordance with the recent Oatlands Structure Plan. However, one title is outside.

Council submits that the STRLUS SGB align with the Oatlands Structure Plan growth areas, (if a SGB must be applied at all – refer comments elsewhere in this submission regarding the unnecessary micro-managing of rural communities for no regional strategic rationale).

Refer maps below:

Draft STRLUS Settlement Growth Boundary:

DRAFT Southern Tasmania Regional Land Use Strategy



Excerpt from the Oatlands Structure Plan:

Priority Development Area 5 is outside the Draft STRLUS SGB:

Service Infrastructure
 Infrastructure Priority Development Areas



Figure 6 - Priority Development Areas 4 and 5

Priority Development Area 5 - Land serviced by sewer and water; required upgrades being rezoning (currently rural resource) and a main extension (storm water).
 Priority Development Area 4 - Land serviced by water; required upgrades being new SPS (sewer servicing) and main extension (stormwater).

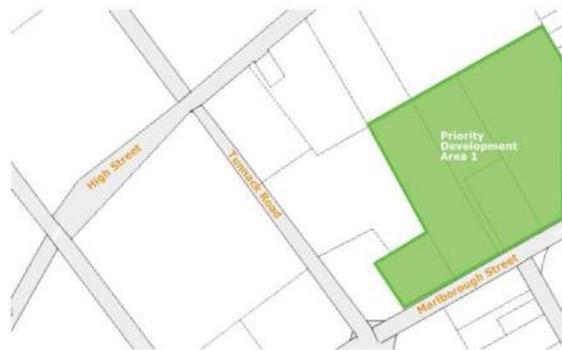


Figure 7 - Priority Development Area 1

Priority Development Area 1 - Land serviced by sewer, water and stormwater.



5.3 Residential Growth at Bagdad/Mangalore

Southern Midlands has produced a new Bagdad Mangalore Structure Plan

This facilitates growth:

- Around the Bagdad township in the form of new Village Zone land (from current Future Urban Zone) and Low Density Residential land, and;
- Around the Mangalore hamlet, in the form of new Low Density Residential (from Rural Living) and additions to the existing Rural Living A area.

Additionally, some existing Rural Living C zoned areas are proposed to change to Rural Living Zone A to enable the option of subdivision for some parcels.

Council notes that these areas are marginal in terms of their agricultural worth, do not contain high priority vegetation and represent 'low hanging fruit' in the drive to provide more housing options and with little new infrastructure needed.

This area is within commuting distance of Greater Hobart and, more particularly, very close to the growing Brighton industrial area and the freight hub. The area also provides ready access to the growing and intensifying commercial agricultural areas to the north, (around Kempton and beyond), providing vital workers' accommodation.

The Draft STRLUS correctly identifies Bagdad as a satellite town of Greater Hobart, yet its provisions will unnecessarily stifle growth. Council objects to this aspect of the Draft STRLUS.

If the SGB mechanism must be applied at all (refer comments elsewhere in this submission regarding the unnecessary micro-managing of local communities for no regional strategic rationale) then Council submits that an SGB be applied to Bagdad and that it accord with the new Bagdad Mangalore Structure Plan. Refer attached Structure Plan and comments from Jensen Plus, the consultants that undertook this work.

5.4 Settlement Growth Boundary - Campania

Campania is proposed to have an unmapped Settlement Growth Boundary in the STRLUS by virtue of the clauses that inhibits growth outside existing zones.

Campania is correctly recognised as a satellite town of Greater Hobart, yet the Draft STRLUS will unnecessarily stifle growth, to which Council objects.

5.5 Worker Housing and Short Stay, (3.4.2.2(d) and 3.4.2.3 (c))

Council supports these strategies providing for greater flexibility in Tourism Towns to provide for more worker housing and tourist accommodation.

5.6 Physical infrastructure (3.5.2)

This is focused on growth, but also needs to include something about maintenance and upgrades. The Upper Derwent Valley currently has unreliable infrastructure and much of TasWater's infrastructure is below standards. . Consider addition of words to the effect: "ensure that infrastructure to existing towns is upgraded and maintained so that it is stable, reliable and compliant."

5.7 Implementation (4.2.2)

Section 4.2.2 of STRLUS states that the next step of STRLUS will be to prepare an implementation plan to specify actions, performance measures and outcomes to deliver on the Regional Strategies. This is likely to include infrastructure upgrades to support development. Council requests the following are noted:

- TasWater – establishment of a sewer main from Bagdad to the Brighton STP, and the decommissioning of the Bagdad STP.

260128 P2324 Bagdad-Mangalore Response to STRLUS_ DRAFT

28th January 2026

Damian Mackey
Project Manager
Southern Midlands Council

By email: dmackey@southernmidlands.tas.gov.au

Dear Damian,

Submission to the Southern Tasmanian Regional Land Use Strategy (STRLUS) – Bagdad-Mangalore Structure Plan

Jensen PLUS is acting on behalf of the Southern Midlands Council, to prepare a submission to the Southern Tasmanian Regional Land Use Strategy (STRLUS) following our recent work with the Bagdad-Mangalore Structure Plan.

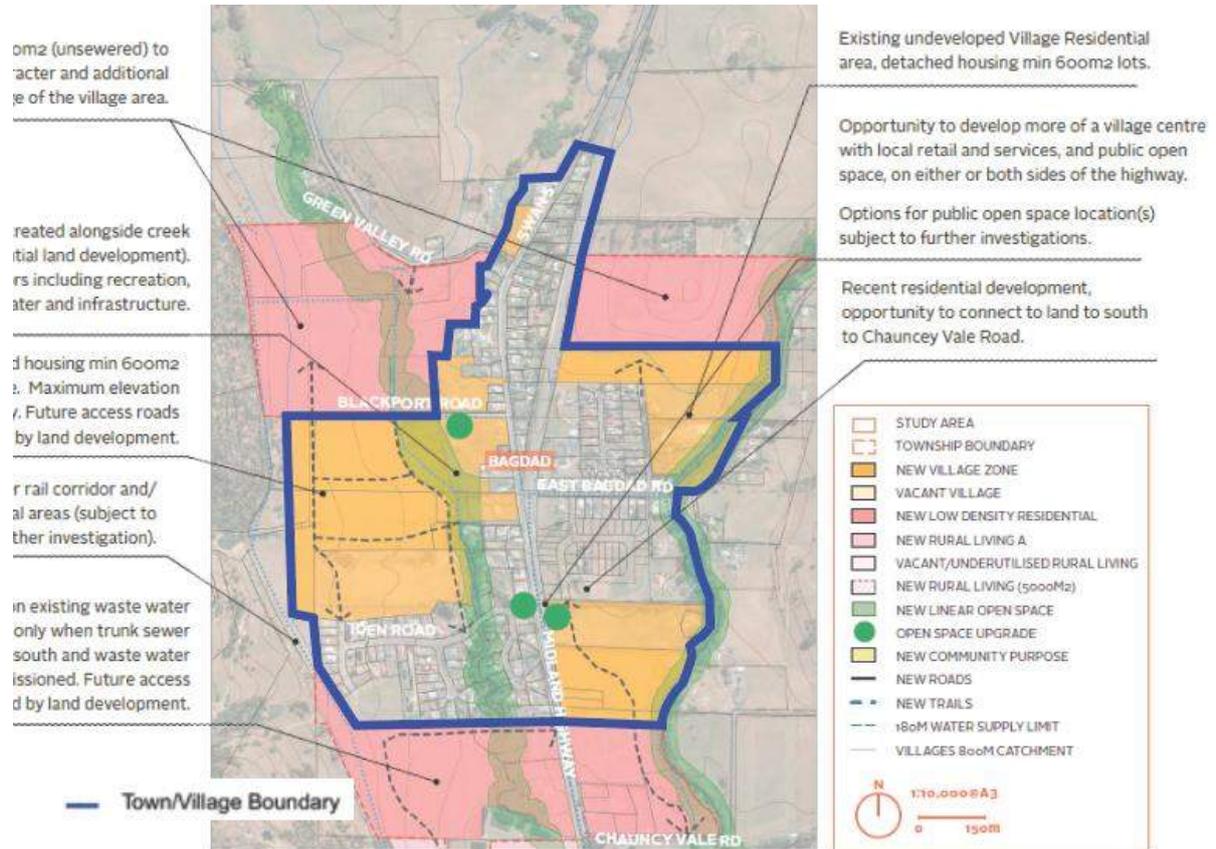
Firstly, it is important to recognise that a substantial amount of work has been undertaken since August 2024 to prepare the Bagdad-Mangalore Structure Plan. This work was initiated in response to many changes in the area, including the delivery of the new Bridgewater Bridge, the proposed Youth Detention Centre, as well as the outdated Structure Plan and in response to the STRLUS. The updated Structure Plan establishes a new vision and spatial framework for Bagdad-Mangalore and has been developed through extensive consultation. It now represents Council's adopted position.

Please refer to attached document.

Following a review, we request the following changes to the Draft STRLUS:

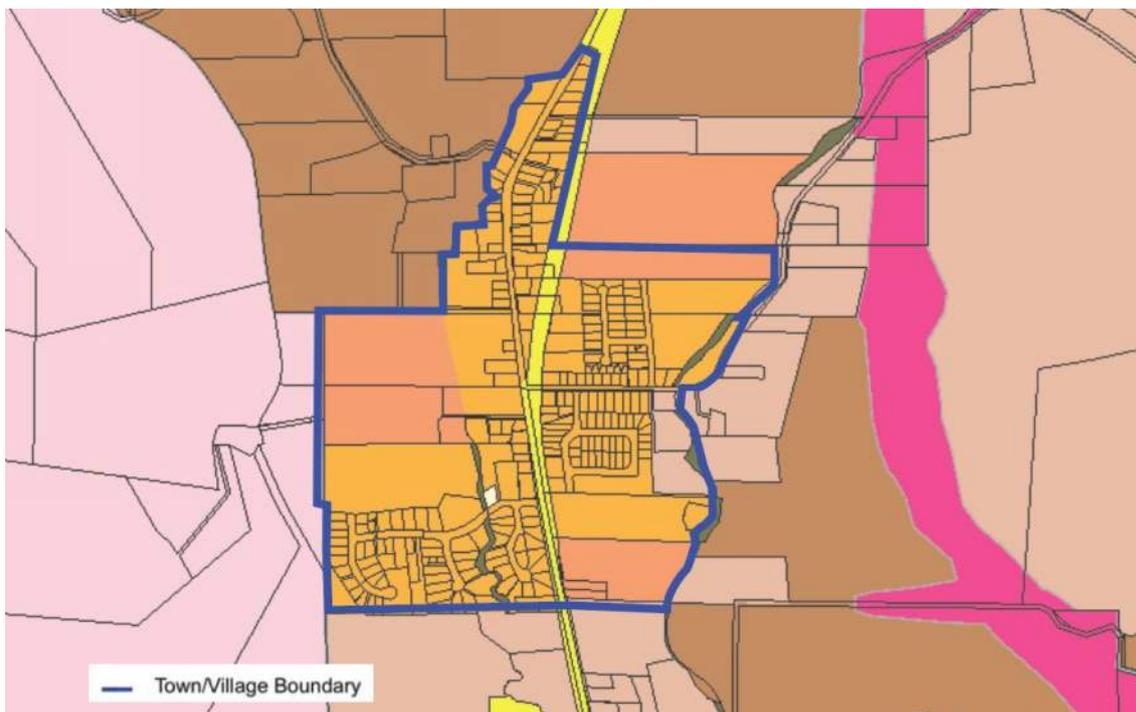
- 1. Establish the township of Bagdad as a 'Village with a nominated boundary'**
 - Chapter 3.1.2.2 – Growth Management in Towns and Villages provides guidance that population growth should be prioritised in towns with nominated growth boundaries.
 - Extensive work has been undertaken with strong community support, to prepare the Bagdad-Mangalore Structure Plan, which has now been adopted by Council. To enable the development of the adopted growth in the Structure Plan, it is necessary for Bagdad to be identified with a nominated boundary, as shown below.





Extract from the Bagdad- Mangalore Structure Plan with recommended nominated boundary (shown in blue)

- The areas identified for growth in Bagdad align with the STRLUS planning horizon (within 25 years).
- The land within the proposed nominated boundary comprises existing and future urban areas, which includes areas of the Village and Future Urban Zone, as shown below. Notably, there are other towns and villages with nominated boundaries have similarly identified Future Urban areas (for example, Geeveston).



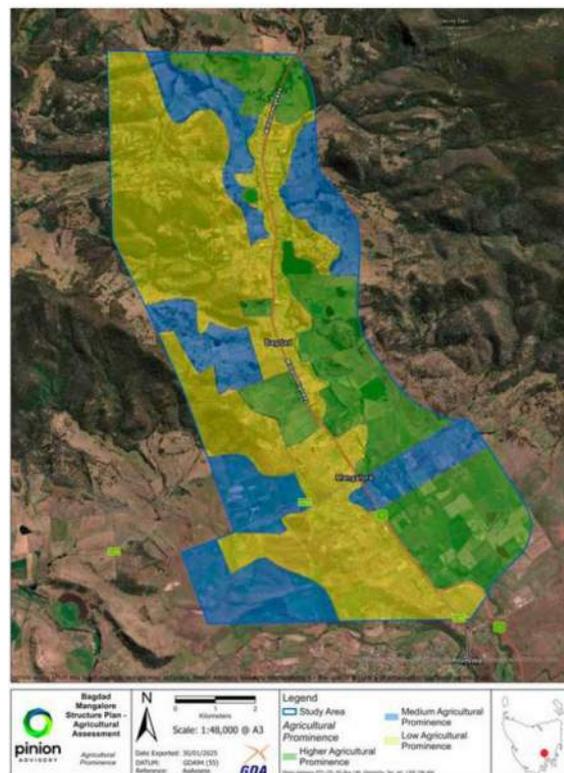
Extract of the existing zones being Village and Future Urban Zone identified within recommended nominated boundary (shown in blue)



- As a note, it is unclear why Bagdad has not been provided with a nominated boundary. The REMPLAN projections (20 February 2024) indicate population growth from 533 persons in 2021 to 617 persons by 2046, resulting to an average annual growth rate of approximately 0.59%. However, historical ABS Census data demonstrates stronger growth trends, with the Bagdad township population increasing by 59 people between 2011 and 2016 (an average annual growth rate of 2.91%), and by a further 36 people between 2016 and 2021 (1.55% per annum). Overall, this represents an average annual growth rate of approximately 2.06% between 2011 and 2021. While the documentation acknowledges and stresses that the population projections is not the only determinant, it is an important factor that has not been fully addressed.
- Finally, the strategic work has been completed and adopted with the Southern Midlands Council. Without recognition of Bagdad as a 'Village with a nominated boundary', the implementation of the Structure Plan will be difficult and inconsistent with the planned future of the town.

2. Changes to the Agriculture and Marine Farming Areas Map

- Throughout preparation of the Bagdad–Mangalore Structure Plan (BMSP), supported by the Agricultural Assessment undertaken by Pinion Advisory, the agricultural qualities of land within the Bagdad–Mangalore area were reviewed.
- The Agricultural Assessment by Pinion Advisory (May 2025) identifies that, for the majority of agricultural land within the BMSP study area, the land has a low level of local and regional agricultural significance. This is due to factors including the extent of available land, lower land capability, limited access to irrigation water, and the fragmented nature of land holdings, with a high proportion of lifestyle and residential allotments, as shown to the right.
- While the importance of protecting agricultural land is acknowledged, we request changes to the Agriculture and Marine Farming Areas Map to more accurately reflect the agricultural land qualities within the area, as below.



Extract from Agricultural Assessment by Pinion Advisory (May 2025) demonstrating the agricultural prominence



Suggest changes to the area shown in red of the Agriculture and Marine Farming Areas Map

3. Support for the protection of future road corridors (Bagdad bypass)

- Chapter 3.5.2.4 (Roads) (b) identifies and protects future road corridors, including the Bagdad bypass, Rokeby bypass, and the future duplication of the South Arm Highway to Lauderdale.
- We strongly support the identification and protection of the Bagdad bypass.
- Further development of land identified in the Bagdad–Mangalore Structure Plan is likely to be facilitated in the long term only if the future bypass road is delivered. The bypass is a critical and integral component of the Structure Plan, providing significant benefits for the township, including improved traffic management, enhanced safety, and public realm improvements.

We welcome Council to consider this correspondence when they prepare their submission to the Draft STRLUS.

If you require any further information, please contact me on 8338 5511.

Yours faithfully



Pat Iwanyshyn

Senior Urban Designer and Urban Planner

Jensen PLUS

Black Gate Road Traffic Counter Report 3 – 17 February 2026



Location of counter



Weekly Vehicle Counts

Site: Black Gate Road
 Filter time: 8.30am Tuesday 3 February 2026 to 9.00am Tuesday, 17 February 2026
 Scheme: Vehicle classification (AustRoads94)
 Filter: Class(1-12) Dir (NESW) Speed (10,160)

Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
	02 Feb	03 Feb	04 Feb	05 Feb	06 Feb	07 Feb	08 Feb	1 - 5	1 - 7
0000-0100	*	0	0	0	0	0	1	0.0	0.2
0100-0200	*	0	0	0	0	0	0	0.0	0.0
0200-0300	*	0	0	0	0	0	0	0.0	0.0
0300-0400	*	0	0	0	0	2	0	0.0	0.3
0400-0500	*	0	0	0	0	0	0	0.0	0.0
0500-0600	*	0	1	1	1	0	1	0.8	0.7
0600-0700	*	0	4	3	5	1	0	3.0	2.2
0700-0800	*	0	5	4	5	2	6	3.5	3.7
0800-0900	*	3	5	9	9	2	6	6.5	5.7
0900-1000	*	6	9	9	6	8	9	7.5	7.8
1000-1100	*	4	4	6	7	4	4	5.3	4.8
1100-1200	*	5	2	4	8	8	13	4.8	6.7
1200-1300	*	11	7	8	5	11	6	7.8	8.0
1300-1400	*	5	8	6	7	8	7	6.5	6.8
1400-1500	*	4	4	5	8	8	3	5.3	5.3
1500-1600	0	5	9	9	15	6	4	7.6	6.9
1600-1700	0	6	13	9	11	4	6	7.8	7.0
1700-1800	0	10	4	11	11	7	6	7.2	7.0
1800-1900	0	11	2	4	3	7	2	4.0	4.1
1900-2000	0	6	7	2	4	2	5	3.8	3.7
2000-2100	0	6	0	3	1	2	0	2.0	1.7
2100-2200	0	0	3	6	4	4	6	2.6	3.3
2200-2300	0	0	0	0	1	2	0	0.2	0.4
2300-2400	0	0	0	0	0	0	0	0.0	0.0
Totals									
0700-1900	*	70	72	84	95	75	72	73.6	73.8
0600-2200	*	82	86	98	109	84	83	85.0	84.7
0600-0000	*	82	86	98	110	86	83	85.2	85.1
0000-0000	*	82	87	99	111	88	85	86.0	86.3
AM Peak	*	0900	0900	0900	0800	1100	1100		
	*	6	9	9	9	8	13		
PM Peak	*	1800	1600	1700	1500	1200	1300		
	*	11	13	11	15	11	7		

* - No data.

Weekly Vehicle Counts

Site: Black Gate Road
 Filter time: 8.30am Tuesday 3 February 2026 to 9.00am Tuesday, 17 February 2026
 Scheme: Vehicle classification (AustRoads94)
 Filter: Class(1-12) Dir (NESW) Speed (10,160)

Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
	09 Feb	10 Feb	11 Feb	12 Feb	13 Feb	14 Feb	15 Feb	1 - 5	1 - 7
0000-0100	0	0	0	0	0	0	1	0.0	0.1
0100-0200	0	0	0	0	0	0	1	0.0	0.1
0200-0300	0	0	1	0	0	2	0	0.2	0.4
0300-0400	0	0	0	0	0	0	0	0.0	0.0
0400-0500	0	0	0	0	0	0	0	0.0	0.0
0500-0600	1	2	0	0	0	0	2	0.6	0.7
0600-0700	1	4	5	6	4	0	0	4.0	2.9
0700-0800	1	2	6	7	6	1	1	4.4	3.4
0800-0900	1	9	13	9	5	8	7	7.4	7.4
0900-1000	7	5	5	8	9	7	6	6.8	6.7
1000-1100	7	8	6	10	5	12	6	7.2	7.7
1100-1200	8	4	4	8	6	3	3	6.0	5.1
1200-1300	7	7	8	10	7	5	9	7.8	7.6
1300-1400	6	3	6	8	7	7	9	6.0	6.6
1400-1500	10	6	8	10	5	8	12	7.8	8.4
1500-1600	7	18	17	9	15	2	3	13.2	10.1
1600-1700	6	6	6	4	14	2	8	7.2	6.6
1700-1800	4	9	7	12	5	4	2	7.4	6.1
1800-1900	8	3	7	3	4	2	8	5.0	5.0
1900-2000	3	6	2	1	4	4	5	3.2	3.6
2000-2100	1	3	3	6	4	2	0	3.4	2.7
2100-2200	0	2	1	4	3	2	1	2.0	1.9
2200-2300	2	1	0	3	0	1	0	1.2	1.0
2300-2400	0	0	0	0	0	0	0	0.0	0.0
Totals									
0700-1900	72	80	93	98	88	61	74	86.2	80.9
0600-2200	77	95	104	115	103	69	80	98.8	91.9
0600-0000	79	96	104	118	103	70	80	100.0	92.9
0000-0000	80	98	105	118	103	72	84	100.8	94.3
AM Peak	1100	0800	0800	1000	0900	1000	0800		
	8	9	13	10	9	12	7		
PM Peak	1400	1500	1500	1700	1500	1400	1400		
	10	18	17	12	15	8	12		

* - No data.

Weekly Vehicle Counts

Site: Black Gate Road
 Filter time: 8.30am Tuesday 3 February 2026 to 9.00am Tuesday, 17 February 2026
 Scheme: Vehicle classification (AustRoads94)
 Filter: Class(1-12) Dir (NESW) Speed (10,160)

Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
	16 Feb	17 Feb	18 Feb	19 Feb	20 Feb	21 Feb	22 Feb	1 - 5	1 - 7
0000-0100	0	0	*	*	*	*	*	0.0	0.0
0100-0200	0	0	*	*	*	*	*	0.0	0.0
0200-0300	0	0	*	*	*	*	*	0.0	0.0
0300-0400	0	0	*	*	*	*	*	0.0	0.0
0400-0500	0	0	*	*	*	*	*	0.0	0.0
0500-0600	0	0	*	*	*	*	*	0.0	0.0
0600-0700	4	6	*	*	*	*	*	5.0	5.0
0700-0800	3	5	*	*	*	*	*	4.0	4.0
0800-0900	14	7	*	*	*	*	*	10.5	10.5
0900-1000	3	0	*	*	*	*	*	1.5	1.5
1000-1100	6	0	*	*	*	*	*	3.0	3.0
1100-1200	9	*	*	*	*	*	*	9.0	9.0
1200-1300	12	*	*	*	*	*	*	12.0	12.0
1300-1400	6	*	*	*	*	*	*	6.0	6.0
1400-1500	11	*	*	*	*	*	*	11.0	11.0
1500-1600	14	*	*	*	*	*	*	14.0	14.0
1600-1700	14	*	*	*	*	*	*	14.0	14.0
1700-1800	8	*	*	*	*	*	*	8.0	8.0
1800-1900	7	*	*	*	*	*	*	7.0	7.0
1900-2000	3	*	*	*	*	*	*	3.0	3.0
2000-2100	2	*	*	*	*	*	*	2.0	2.0
2100-2200	2	*	*	*	*	*	*	2.0	2.0
2200-2300	2	*	*	*	*	*	*	2.0	2.0
2300-2400	0	*	*	*	*	*	*	0.0	0.0
Totals									
0700-1900	107	*	*	*	*	*	*	100.0	100.0
0600-2200	118	*	*	*	*	*	*	112.0	112.0
0600-0000	120	*	*	*	*	*	*	114.0	114.0
0000-0000	120	18	*	*	*	*	*	114.0	114.0
AM Peak	0800	*	*	*	*	*	*		
	14	*	*	*	*	*	*		
PM Peak	1600	*	*	*	*	*	*		
	14	*	*	*	*	*	*		

* - No data.

Comparisons-

Total number of vehicles travelling on Black Gate Road (near Tunnack Road) 8.30am Tuesday 3 February 2026 to 9.00am Tuesday, 17 February 2026 was 1350.

- The average number vehicles travelling on Black Gate Road is 96 per day x 7 = 672 average per week.

Vehicles per week-

Eldon Road - 1025

Yarlington Road - 841

Native Corners (May 2016) - 1316

Black Brush - 1183

Woodsdale (near Tasman H'way) - 1823

Woodsdale (near Stonehenge) - 1050

Woodsdale (near New Country Marsh) - 994

Broadmarsh Road (August 2016) – 3164

York Plains Road - 560

Stanley Street - 770

Huntingdon Tier Road – Green Valley Rd intersection - 1491

Huntingdon Tier Road – Clifton Vale Rd intersection - 1029 (includes waste transfer station traffic - 245)

Rhyndaston Road just north of Eldon Road (Nov 2017) - 938

Native Corners Road (Dec 2017) - 1554

Interlaken Road - 1029

Oatlands (northern end) (February 2018) – 4837

Oatlands (southern end) (February 2018) – 6853 (counter placed just off highway so didn't include traffic from Tunnack Road)

Brown Mountain Road - 1141

Tunnack Road - 1498 (1575 adjusted)

Elderslie Road near Broadmarsh – 3318

School Road Bagdad – 994

Buckland Road (July 2019) – 700

Rhyndaston Road near Tiberias Road (Nov 2019) – 581

Buckland Road (Dec 2019 to Jan 2020) – 637

Green Valley Road (July – August 2020) – 973

Woodsdale Road (quarry) (April – May 2021) – 2128 & 1708

Bluff Road (Dec 2021) - 637

Interlaken Road (Feb 2022) - 861

Climie Street Campania (May 2022) – 3,439

Mud Walls Road (just north of Climie Street) Campania (June 2022) - 17,847

Climie Street towards Estate Road Campania (June 2022) – 1,788

Mud Walls Road (just south of Climie Street) Campania (June 2022) - 22,214

Reeve Street (southern end) Campania (February 2023) - 25,977

Rhyndaston Road near Youngs Road (May 2024) – 812

Oatlands High Street (May/June & Dec 2024) see reports

Oatlands Stutzer Street near High Street (March/April 2025) – 693

Reeve Street (between Lee and Hall Street) Campania – 23,311 (May 2025)

Black Gate Road (near Tunnack Road) – 672 (February 2026)



Class 1
Short Vehicle



Class 2
Short Vehicle Towing



Class 3
Two Axle Truck



Class 4
Three Axle Truck



Class 5
Four Axle Truck



Class 6
Three Axle Articulated Vehicle



Class 7
Four Axle Articulated Vehicle



Class 8
Five Axle Articulated Vehicle



Class 9
Six Axle Articulated Vehicle



Class 10
B Double



Class 11
Double Road Train



Class 12
Triple Road Train

Vehicles = 1350

Posted speed limit = 60 km/h, Exceeding = 84 (6.22%), Mean Exceeding = 63.14 km/h

Maximum = 74.9 km/h, Minimum = 10.1 km/h, Mean = 47.4 km/h

85% Speed = 56.40 km/h, 95% Speed = 60.46 km/h, Median = 48.15 km/h

20 km/h Pace = 39 - 59, Number in Pace = 1022 (75.70%)

Variance = 89.37, Standard Deviation = 9.45 km/h

Speed Statistics by Hour

Site: Black Gate Road
 Filter time: 8.30am Tuesday 3 February 2026 to 9.00am Tuesday, 17 February 2026
 Scheme: Vehicle classification (AustRoads94)
 Filter: Class(1-12) Dir (NESW) Speed (10,160)

Hour Bins (Partial days)

Time	Bin	Min	Max	Mean	Median	85%	95%	>PSL 60 km/h
0000	2 0.1%	37.5	48.5	43.0	42.9	48.5	48.5	0 0.0%
0100	1 0.1%	46.1	46.1	46.1	46.1	46.1	46.1	0 0.0%
0200	3 0.2%	48.9	56.4	52.3	51.5	56.4	56.4	0 0.0%
0300	2 0.1%	59.2	59.8	59.5	59.5	59.8	59.8	0 0.0%
0400	0 0.0%	0.0	0.0	0.0	216.0	216.0	216.0	0 0.0%
0500	9 0.7%	39.5	60.4	49.9	48.6	57.6	60.4	1 11.1%
0600	43 3.2%	32.4	62.3	49.8	50.4	57.6	61.8	2 4.7%
0700	54 4.0%	27.4	61.4	46.2	48.7	56.0	59.4	2 3.7%
0800	107 7.9%	25.6	66.4	44.4	45.4	51.9	57.8	3 2.8%
0900	97 7.2%	11.0	67.2	48.8	48.8	59.3	63.7	12 12.4%
1000	89 6.6%	11.6	74.9	47.2	49.0	58.1	63.7	9 10.1%
1100	85 6.3%	11.2	65.0	45.5	46.8	55.5	58.8	3 3.5%
1200	113 8.4%	10.1	65.3	47.4	48.2	55.2	60.5	6 5.3%
1300	93 6.9%	23.9	66.5	48.6	48.2	56.9	63.8	7 7.5%
1400	102 7.6%	23.4	63.4	48.8	50.0	57.4	61.3	8 7.8%
1500	133 9.9%	11.0	73.2	45.9	47.2	55.6	61.5	7 5.3%
1600	109 8.1%	12.4	70.6	47.0	48.2	57.2	59.6	4 3.7%
1700	100 7.4%	10.4	62.9	48.8	49.1	57.8	61.0	9 9.0%
1800	71 5.3%	26.0	65.3	49.2	49.9	56.7	61.6	6 8.5%
1900	54 4.0%	20.6	62.0	47.0	47.8	54.9	57.2	1 1.9%
2000	33 2.4%	28.3	63.6	47.0	45.5	56.9	59.5	1 3.0%
2100	38 2.8%	10.4	64.3	45.8	47.4	55.6	63.7	3 7.9%
2200	12 0.9%	26.3	59.4	48.0	51.8	58.5	59.4	0 0.0%
2300	0 0.0%	0.0	0.0	0.0	216.0	216.0	216.0	0 0.0%
----	1350 100.0%	10.1	74.9	47.4	48.1	56.4	60.5	84 6.2%

Class Speed Matrix

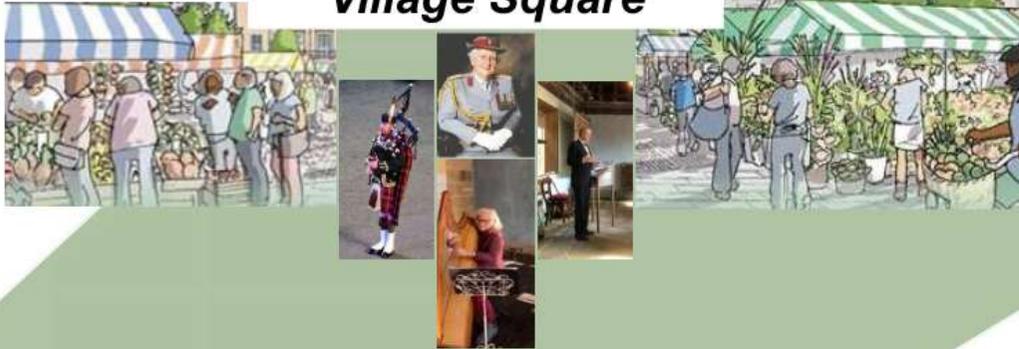
Site: Black Gate Road
Filter time: 8.30am Tuesday 3 February 2026 to 9.00am Tuesday, 17 February 2026
Scheme: Vehicle classification (AustRoads94)
Filter: Class(1-12) Dir (NESW) Speed (10,160)

km/h	Class												Total	
	SV 1	SVT 2	TB2 3	TB3 4	T4 5	ART3 6	ART4 7	ART5 8	ART6 9	BD 10	DRT 11	TRT 12		
10- 20	16	.	1	2	.	.	.	.	.	.	.	.	19	1.4%
20- 30	34	1	7	1	.	1	.	1	.	.	.	.	45	3.3%
30- 40	137	11	30	5	.	.	2	1	1	.	.	.	187	13.9%
40- 50	401	21	106	1	.	1	.	.	1	.	.	.	531	39.3%
50- 60	379	12	90	.	.	1	1	.	1	.	.	.	484	35.9%
60- 70	63	2	14	.	.	.	.	.	.	.	.	.	79	5.9%
70- 80	4	.	1	.	.	.	.	.	.	.	.	.	5	0.4%
80- 90	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
90-100	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
100-110	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
110-120	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
120-130	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
130-140	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
140-150	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
150-160	.	.	.	.	.	.	.	.	.	.	.	.	0	0.0%
Total	1034	47	249	9	0	3	3	2	3	0	0	0	1350	
	76.6%	3.5%	18.4%	0.7%	0.0%	0.2%	0.2%	0.1%	0.2%	0.0%	0.0%	0.0%		



COMMUNITY CONSULTATION PRESENTATION
We would appreciate you sharing your thoughts on the preliminary project aspects with us.

Oatlands Civic & Cultural Precinct
Creating a Vibrant Village Square



We have an opportunity to reimagine the area around the Council Chambers, Roche Hall and also connecting access to the public buildings across High Street, namely the Aquatic Centre as well as the Midlands Memorial Community Centre, to create a new focal point for residents and visitors alike that reinforces the character within the heritage village of Oatlands.

This set of slides provides the background of the Oatlands Village Square project as well as an explanation of the preliminary work that needs to be considered prior to embarking on the actual Village Square detailed design.

**Report on the
Community
Consultation
Survey
Responses to
the Oatlands
Village
Square
Consultation
Process**

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PREABLE

This document contains the information compiled from the *Community Consultation Survey*, under the 'Yes or No' category of the Survey. It also includes the 'Other Comments' responses. It has been packaged into the *Slides*, the *Yes/No* responses and the 'Other Comments' for ease of interpretation. A copy of the *Community Consultation Survey Questionnaire* is provided as an attachment to this document. The names of the Respondents are not shown in this document. A separate file exists with the correlation of names to responses identified, but will not be made available. That information will be accessed if further clarification at the design stage is required. This is a large document which includes all of the information from the survey that will be carried forward into the next stage of the project. The information contained within this Report documents the actual outputs of the tabulated data, along with a design concept that one of the Respondents provided. Where a Respondent has offered more than one response to the question, under the 'Other Comments' section, that has been numerated to show that the inputs are from the one Respondent, which provides some additional context to their other responses.

All of the information contained in this document faithfully represents the responses provided by the Respondents.

This information has been compiled and tabulated by Andrew Benson to enable the Oatlands Village Square Working Group and the Council to be able to appreciate the views and aspirations articulated by Community members. It provides a structured manner in which to undertake an analysis of the responses and consider, where possible, Community suggestions can be explored in the design tranche of the project.



Project Manager

On behalf of the Oatlands Village Square Working Group

THE NOTICE OF CONSULTATION

Community Consultation

15th July to 22nd July at the Oatlands Supreme Court

During the development of the Oatlands Structure Plan in 2021, the community strongly shared, that their top priority was the creation of a Village Square at the heart of Oatlands, providing a connection to many of the buildings and spaces.

This is the first stage of the Community Consultation, which is seeking to address a range of matters that will be a precursor to the development of the Village Square. These matters need to be resolved and comments from the Community will shape the Council decision making for many of them. Some matters are required to be addressed as they are a legislative requirement, eg access, but it is important to put them into context with the overall project.

The Community are invited to review the Project Display Panels and then respond to a Community Survey about the way forward.

<i>Where will the Display be?</i>	<i>Supreme Court Building, 7 Campbell St Oatlands</i>
<i>When and at What Times?</i>	<i>Tues. 15th July 2025 from 11.00am to 2.30pm</i>
	<i>Wed. 16th July 2025 from 11.00am to 2.30pm</i>
	<i>Wed. 16th July 2025 from 4.00pm to 6.30pm</i>
	<i>Thur. 17th July 2025 from 11.00am to 2.30pm</i>
	<i>Fri. 18th July 2025 from 11.00am to 2.30pm</i>
	<i>Mon. 21st July 2025 from 11.00am to 2.30pm</i>
	<i>Mon. 21st July 2025 from 4.00pm to 6.30pm</i>
	<i>Tues. 22nd July 2025 from 11.00am to 2.30pm</i>

No appointments required, just roll up and a Council Officer will be there to talk you through the Project Display Panels.

For further information contact Andrew Benson, Deputy General Manager,
abenson@southernmidlands.tas.gov.au or mobile 0429 852 730 or
visit the website www.southernmidlands.tas.gov.au/OatlandsVillageSquareProject

The Community Consultation notice was posted at the following.

- Southern Midlands Regional News
- Council website
- Council Facebook page
- Council Office front door
- Post Office
- IGA
- Bargain Centre
- Oatlands Historical Society



INTRODUCTION



Deputy Mayor, Karen Dudgeon,
Chairperson - Oatlands Village
Square Working Group

Oatlands is home to many cherished buildings and spaces that are central to our village's civic and cultural life. This has been recognised across Australia, with Oatlands having the largest number of intact Georgian buildings in a village environment in the country.

During the development of the Oatlands Structure Plan in 2021, the community strongly shared, that their top priority was the creation of a Village Square at the heart of Oatlands, providing a connection to many of these buildings and spaces.

The idea is to combine key locations like the Council Chambers, Roche Hall (the old Oatlands School), the Aquatic Centre, and the Midlands Memorial Community Centre into a more integrated space. This would improve the use of these buildings, create a lively and connected village centre, as well as boost community activity levels, all whilst preserving the character of the village.

The project's starting point is understanding what is special about this location and what can be done to vary existing elements/infrastructure that have the potential to clutter or impact on a vibrant village square. The goal is to create a space that stands out but also strengthens Oatlands' identity.

This presentation explains the background of the project and the initial work needed before designing the Village Square in detail.

Community input is encouraged by the way of the attached survey form, to assist us in our decision making for these preliminary elements of the project. I look forward to your involvement in the process.

Karen

Slide 2 of 26

CONSULTATION VENUE – SUPREME COURT OATLANDS



Slides 3 to 5 – What Gave Rise to this Project?

What gave rise to this project?

The Oatlands Structure Plan was adopted by Council in September 2021 and there were a number of strong recommendations that came out of the Community Consultation and the final report that acknowledged the creation of the Oatlands Village Square, as identified below in extracts from the Oatlands Structure Plan below.



Oatlands Structure Plan 2021 [Extracts]

Fostering and Enlivened and Historic Rural Town

1. Executive Summary

Key Actions of the Structure Plan are to:

- Create an Oatlands Town square in front of Oatlands Town Hall and the Old State School fronting High Street as a focal point for town events;

(Oatlands Structure Plan page 5)

Action 1.1 - Create an Oatlands Town square fronting High Street.

Improved Passive Recreation Opportunities

There is an opportunity to create a space to support public events, festivals and markets in an area that includes the forecourts of both the Oatlands Town Hall and the Old State School fronting High Street. The space crosses Stutzer Street, thus some urban design treatment would be required to delineate this part of the street as a pedestrian zone. The Town square can connect to the Barrack Street Car Park (via Mason Street) or the Military Precinct and Lake Dulverton via Campbell Street. The hub would act as an important focal point on the main street and the main pedestrian link to and from Lake Dulverton, the residential areas to the south-west, and the community facilities to the north of High Street.

A town square design plan (or master plan) is to be prepared and include public consultation prior to a final design being prepared for implementation.

(Oatlands Structure Plan page 43)

Slide 3 of 26

Survey Responses after Slide 5

Slides 3 to 5 – What Gave Rise to this Project?

What gave rise to this project?

(Oatlands Structure Plan page 57)

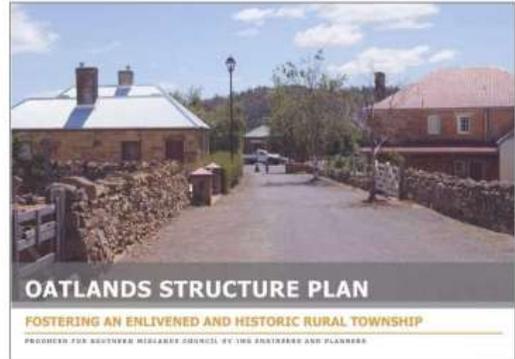
4.0 The Structure Plan

Land Use and Development Directions



Key actions for Oatlands have been translated into development directions for the township and those with a spatial application are shown in the following maps (excluding those for traffic found in Appendix C).

- 1 Create an Oatlands Town square in front of Oatlands Town Hall and the Old State School fronting High Street.
- 2 Construct new toilets facilities at the new playground and upgrade the toilet facilities at the rear of the Council chambers to a high standard.
- 3 Strengthen trail links around Oatlands.
- 4 Create a Conservation-based interface with Lake Dulverton.
- 5 Unlock residential growth in the town through site identification, master planning and services upgrades.
- 7 Foster development of a knowledge-based economy development centred around Heritage.
- 8 Create a Village Design Plan that focuses on strengthening the High Street's identity, and provides a cohesive and unified approach to streetscape works in the township.



OATLANDS STRUCTURE PLAN

FOSTERING AN ENLIVENED AND HISTORIC RURAL TOWNSHIP

PRODUCED FOR SOUTHERN MIDLANDS COUNCIL BY THE ENGINEERS AND PLANNERS

Slide 4 of 26

Survey Responses after Slide 5

Slides 3 to 5 – What Gave Rise to this Project?

What gave rise to this project?

Appendices (Oatlands Structure Plan page 62)

A. Implementation Plan

STRATEGIES	ACTION	RESPONSIBILITY	EST. COSTS (APPROX)	PRIORITY (HIGH, MEDIUM, LOW) & TIMING
1. IMPROVED PASSTIVE RECREATION OPPORTUNITIES:	Action 1.1 - Design and co-create an Oatlands Town Square in front of Oatlands Town Hall and the Old State School fronting High Street.	Council	\$500K	High
	Action 1.2 - Continued development of the playground.	Council	\$800K	High
	Action 1.3 - Construct new toilets facilities at the new playground; upgrade the toilet facilities at the rear of the Council chambers (potentially other public spaces) to a high standard.	Council	\$1M	High
	Action 1.4 - Promote the former rodeo paddock for events.	Council	Operational Budget	Medium
	Action 1.5 - Increase conservation-based activities on Lake DuVerton (pathway and focal point).	Council	\$850K	Medium
	Action 1.6 - Increase walking activities around Lake DuVerton (pathway and interpretation point)	Southern Midlands Council/ PWS	\$350K	Medium

4.0 The Structure Plan

Proposed Town Square Precinct



In August 2024 Council resolved to establish a Working Group to progress the implementation of a framework for the Oatlands Village Square Project. The Project Plan and the makeup of the Working Group are detailed on slide 9/26. The Working Group found that there were a number of issues that required resolving as a precursor to progressing with the Village Square design. These slides represent those issues for community feedback, prior to progressing to the next stage of the project.

Slide 5 of 26

Survey Responses next page



Slide No. and Description	Statement / Question	Yes %	No %
Slides 3 to 5 – What Gave Rise to this Project?	<i>I participated in the Oatlands Structure Plan Consultation in 2021</i>	23%	77%
	<i>I agree with the Consultant's recommendations in the Report</i>	78%	22%
	Other comment(s): There were nineteen comments made in respect of these Slides		

Survey / Slides 3 to 5 – What Gave Rise to this Project?

Other Comments provided by Survey Respondents

Person's Response	Project Working Group Comments
1. I have read the Structure Plan and agree with most of its content.	<i>Noted. No further action on this item.</i>
2. Refer to my site plan (attached) for my suggested extension of the proposed square footprint. I think that greater use of the space can be achieved if it is extended.	<i>Comments noted, we will provide a linkage to other areas once this space has been designed and developed. Need to start somewhere and make it a scalable. At this point in time there are many elements, more at this point in time would make it unwieldy. Will note the elements in the 'precinct' for consideration at the design stage. Take this forward to the next tranche</i>
3. (Slide 9 – Project Working Group) I think that you need to include a Community Reference Group in the development of the design. And members should be of all ages, abilities, disciplines and professions, including reps from the Business Association (if there is one). I would like to be in the group. I am a project manager in infrastructure with a background in urban design and public open space.	<i>Comments noted. The Working Group represents a broad range of professional skills and design capability as well as meaningful Community input through the Elected Members. We will go out for Consultation at various stages throughout the project for input. No further action on this item.</i>
While not involved @ inception, the values & virtues of the proposal in this location are logical & compelling in the only contiguous open space in High Str.	<i>Comments noted and appreciated. No further action on this item.</i>
1. The DDA seems to be driving a lot of this.	<i>Correct, to some extent. No further action on this item.</i>



Person's Response	Project Working Group Comments
<p>2. Can a stone and gravel ramp to the original front doors of Council chambers, starting to the Stutzer St side, save a lot of this?</p> <p>As Hobart City Council have done (albeit rather unsympathetically with a steel ramp). It would save shifting the traditional entry focus and circulation pattern of this building and give the DDA customers the full traditional entry pleasure?</p>	<p><i>This is worthy of consideration, but gravel is not a DDA acceptable surface so it will be unable to be used.</i></p> <p><i>BW: This option is unlikely to be acceptable from a heritage perspective. We have had an Access Consultant assess the building and the preferred (in fact only) viable option is to utilise the side door as the 'main' entrance. This will provide equitable access to a designate accessible parking space at rear and provide a more functional entrance. The front door will remain usable, however the public will be encouraged to use the side entrance.</i></p>
<p>I participated in the 90s+/- when Mr Harry Oldmeadow put forward a similar plan to close Stutzer St.</p>	<p><i>Comment noted. No further action on this item.</i></p>
<p>Seats need to be installed along High St, these could be put out in front of Roche Hall. Several elderly visitors have commented there is nowhere to rest while walking around Oatlands. This needs to be addressed urgently</p>	<p><i>Agreed and also in the Village Square. This will be an important element in the final design. Plans.</i></p> <p><i>Take this forward to the next tranche</i></p>
<p>1. Why do we not know how much the project is going to cost? The costings that are shown, \$600,000.00 (in the Oatlands Structure Plan) are well out of date and I would estimate todays cost would be around \$2-3million and by probably 2028, probably \$5million.</p>	<p><i>At this stage it is a concept, once the project has gone through the consultation process and a preliminary design is developed and cost framework can be developed.</i></p> <p><i>Take this forward to the next tranche</i></p>
<p>2. Why are we spending money on a project with no costings?</p>	<p><i>Ditto 1 above in this suite of responses.</i></p> <p><i>Take this forward to the next tranche</i></p>
<p>3. Where are the finances coming from if it were to proceed? I understand the need for grants to undertake such a project but with both the State and Federal Government's being in such debt</p>	<p><i>Comments noted. The project will be packaged into various components that are relatively 'standalone' and then grant submissions will be developed, with maybe some capital works funding where required. Take this forward to the next tranche</i></p>
<p>4. The money being spent and planned to be spent would I believe, be better spent on badly needed infrastructure in the SMC</p>	<p><i>Comments noted. Much of the infrastructure that we are referring to in this concept plan are well overdue for infrastructure improvements.</i></p> <p><i>No further action on this item.</i></p>



Person's Response	Project Working Group Comments
<p>5. I have not seen any evidence of a Village Square in the history of Oatlands and we do not use our existing facilities (Callington Park) enough now and this would simply be doubling up on those facilities;</p>	<p><i>Comments noted.</i></p> <p><i>BW: This is correct. Some Macquarie-era towns had a formal town square, which survive in only in New Norfolk and George Town, which appear to be truly Macquarie-planned towns (i.e. 1811-1821). Towns from slightly later (e.g. 1820s – which were located by Macquarie, but not necessarily planned by his instructions) lacked the formal town square that he favoured (e.g. Ross, Campbell Town, and Perth). Both Oatlands and Hamilton had squares planned (post-Macquarie), but never achieved (Oatlands was to be bounded by Gay, Church, William Streets and South parade.) However Bothwell and Westbury (both 1820s – non-Macquarie planned) retain a planned square. Often, ‘town squares’ were repurposed from early colonial administrative uses – such as that at Richmond (which is what Oatlands would be akin to.</i></p> <p><i>We do need to think about how the square will be used in-line with other public spaces. We have Callington Park, and soon the gaol yard as versatile public open spaces within the town centre.</i></p> <p><i>Take this forward to the next tranche</i></p>
<p>6. If the current Council Chambers in Oatlands is inadequate, then perhaps we should look at transferring some functions to the Kempton location;</p>	<p><i>Comment noted. The basic staffing services are vastly inadequate and outdated in the main Council offices. The public toilet dates from the 1970s and does not accommodate all abilities usage. (DDA). No further action on this item.</i></p> <p><i>BW: The plans for the upgrade of the Oatlands Council Chambers and Town Hall have been finalised, and are subject to consultation with Heritage Tasmania ahead of a DA. Council has a forward financial plan for funding these as part of the overall Oatlands building upgrade which will take some years to fully achieve.</i></p>
<p>7. Can I suggest you use the money to upgrade the toilet facilities upstairs at the Council Chambers;</p>	<p><i>Comment noted. That definitely needs to happen as part of the project.</i></p> <p><i>BW: The plans for the upgrade of the Oatlands Council Chambers and Town Hall have been finalised, and are subject to consultation with Heritage Tasmania ahead of a DA. Council has a forward financial plan</i></p>



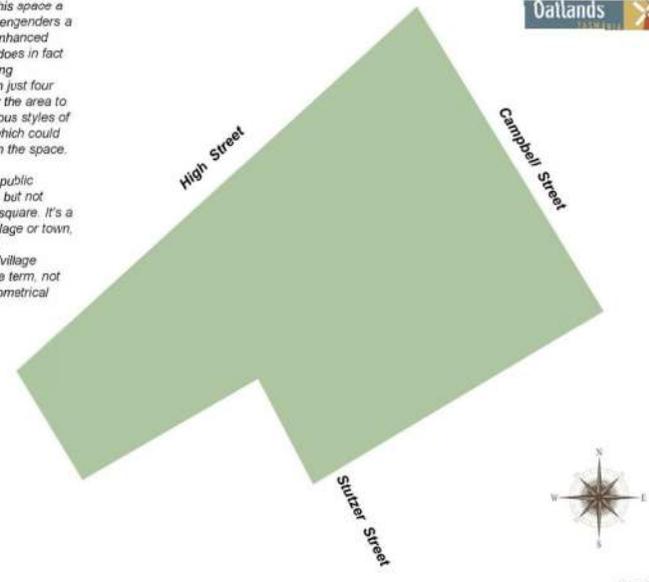
Person's Response	Project Working Group Comments
	<i>for funding these as part of the overall Oatlands building upgrade (which will take some years to fully achieve). Toilets within the building (as well as access) are the highest priority.</i>
8. Focus should be on infrastructure, not woke ideas like dog parks and village square.	<i>Comment noted. The needs of a Community are wide and varied. No further action on this item.</i>
Fantastic idea would serve the community well as it does at Ross / Richmond	<i>Comment noted and appreciated. No further action on this item.</i>
Our market is now in the School Gym. A great place. Don't have to worry about weather. Not enough room for events.	<i>Comment noted. No further action on this item.</i>
Implementation Plan is a very important aspect of having a structural plan. Kudos to the Oatlands Village Square Working Group	<i>Comment noted and appreciated. No further action on this item.</i>

End of Other Comments in respect of Slides 3 – 5.

Slides 6 to 9 - No Response was required as these slides were providing project context only

Whilst we are calling this space a Village Square, which engenders a sense of history and enhanced Community activity, it does in fact have six sides of varying dimensions rather than just four sides, which allows for the area to be segmented for various styles of events and activities which could be run in parallel within the space.

A "village square" is a public gathering space, often but not always, geometrically square. It's a central location in a village or town, usually surrounded by buildings. The name "village square" is a descriptive term, not necessarily a strict geometrical definition.



Oatlands

Slide 6 of 26




Oatlands

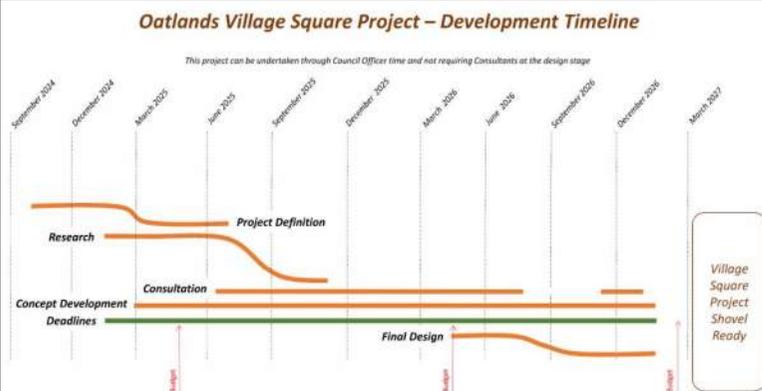
**Oatlands Village Square
Project Considerations**

Commenced January 2025

Slide 8 of 26

Oatlands Village Square Project – Development Timeline

This project can be undertaken through Council Officer time and not requiring Consultants at the design stage



Project Working Group
 Deputy Mayor Karen Dudgeon (Chair)
 Cllr Tony Bisdee OAM
 Cllr Don Fish
 Deputy General Manager, Andrew Benson (Project Manager)
 Mgr Heritage Projects, Brad Williams
 Mgr Community & Corporate Development, Wendy Young
 Mgr Infrastructure & Works, David Richardson
 Mgr Natural Resources, Maria Weeding
 Mgr Development & Env. Services, Grant Finn
 Executive Assistant, Jemma Crosswell

Village Square Project Shovel Ready

Slide 9 of 26



Oatlands
TASMANIA

The Site Scope

This project is in response to the outputs of the Oatlands Structure Plan, quote 'Create an Oatlands Town/Village Square in front of the Oatlands Town Hall and the Old State School fronting High Street, as a focal point for town/village events'.

There are elements that are required to be a benchmark of understanding as the project is established and progressed, eg

1. We need to understand the sense of 'place', from an individual level, a Council level and a broader Community level?
2. What has occupied this space in the past and how relevant is it to today?
3. What interpretation/representation is appropriate of former uses?
4. What are Community expectations in respect of a Village Square?

*Oatlands Village Square
Project Considerations*

Slide 10 of 26

Slide No. and Description	Statement / Question	Yes %	No %
Slide 10 – Project Site Scope	<i>I agree with the Project Site Scope</i>	90%	10%
	<i>Other comment(s): There were six comments made in respect of this Slide</i>		

Survey / Slide 10 – Project Site Scope

Other Comments provided by Survey Respondents

<i>Person's Response</i>	<i>Project Working Group Comments</i>
I think that it is a great idea. I would like to be on the Community Reference Group as the design is developed. I strongly suggest you get a landscape architect to design the space and do a streetscape master plan	<i>Comments noted and appreciated.</i> <i>Take this forward to the next tranche</i>
As discussed the Precinct could extend to include the Goal interpretation & the forecourt between the Gaol and the former Police Residence	<i>Comments noted. That will be the next stage as a natural extension of this project.</i> <i>BW: Fully agree.</i> <i>Take this forward to the next tranche</i>
1. If Stutzer St only has 50 traffic movements a day, and you are moving the toilets, surely, there will be even less.	<i>Comments noted. Stutzer has currently 100 traffic movements per day, reducing down to 50/day with the removal of the public toilets.</i>
2. Please don't give Oatlands a one way street there and narrow it. It is a very quiet country town not a city.	<i>Comment noted.</i> <i>BW: Noting that Stutzer Street was not an originally planned street – it came about only after the break-up of the Military Precinct in 1858, so a re-think of that street is not out of the question in terms of maintaining the original street grid (as the heritage precinct provisions seek).</i> <i>Take this forward to the next tranche</i>
I don't accept that we need to understand the 'sense of place'	<i>Comment noted. A sense of place is the deep, subjective emotional and psychological connection people form with a specific location, turning a mere "space" into a meaningful "place" through feelings of belonging, comfort, identity, or even negativity like fear. It's shaped by physical environment, history, culture, and personal memories, making a spot unique and fostering attachment, as seen in community identity or a childhood home.</i>
The plaques along the High St (WW2) need to be placed in the Military Section of the proposed Village Square.	<i>Comment noted. This could be considered for the detailed design stage.</i> <i>Take this forward to the next tranche</i>

End of Other Comments in respect of Slides 10.

Slide 11 - No Response was required as this slide was providing project context only





Slide No. and Description	Statement / Question	Yes %	No %
Slide 12 – Identified Issues within the Site Scope	I agree that the issues identified require addressing	90%	10%
	Other comment(s): There were twenty one comments made in respect of this Slide		

Survey / Slide 12 – Identified Issues within the Site Scope

Other Comments provided by Survey Respondents

<i>Person's Response</i>	<i>Project Working Group Comments</i>
1. Power Poles – don't underground it's too costly and the money is better spent on improving amenity for people. Focus on landscaping, seating, creating space for people and gatherings (markets/events).	<i>Comments noted. Many. Many people have made positive comments about the undergrounding of the power poles and wires in the High St. The other comments will be incorporated in the detail design of the square. Take this latter portion of the comments forward to the next tranche.</i>
2. Remove impediments ie fence around council office.	<i>Comment noted. BW: The Town Hall Conservation Management Plan (CMP) provides a pathway for optional removal of this fence (keep a representative sample). It does encourage enclosure with a traditional Victorian Fence – this may not need to be complete enclosure however, noting the desire for ease of passage between the sites). This can be worked-through. Take this forward to the next tranche</i>
3. Provide power and lighting for evening events/markets.	<i>Comments notes. For consideration in the detailed design of the Square. Take this forward to the next tranche</i>
4. Remove car parking from the area (except parking M-F).	<i>Comments unclear.</i>
5. Keep toilets and upgrade in the same location.	<i>Comment noted. This would impact on other elements in the concept design, eg parking and DDA parking. In the survey 83% of Respondents wanted the toilets to be removed. No further action on this item.</i>
6. Town Hall access – rental space/public events – commercialise this space. Also refer to my attachments for site plan – just ideas to consider in the design phase.	<i>Comments noted. For consideration in the detailed design. Take this forward to the next tranche</i>
1. Stutzer St's cross sectional profile is an issue as well as access to council staff carpark;	<i>Comment noted. Take this forward to the next tranche</i>



Person's Response	Project Working Group Comments
2. Retaining 2 way access has turning issues, but allows the northern area to be closed and incorporated into the Square, while the one way, narrowed solution addresses the turning issue & if surface treatment changed would calm traffic.	<i>Comments noted. Take this forward to the next tranche</i>
1. Moving the toilets to the proposed spot behind the police station, undergrounding the power lines and removing the cement post and steel rod fence, will make it much easier to use and look tidier.	<i>Comments noted. Take this forward to the next tranche</i>
2. The road of Stutzer St is quite charming, properly macadamized and redolent of good civic pride. New poured cement edging will just make it look naff.	<i>Comments noted. Detailed design elements for consideration. Take this forward to the next tranche</i>
1. Power should be placed underground;	<i>Comment noted. Take this forward to the next tranche</i>
2. Toilets should be relocated;	<i>Comment noted. Take this forward to the next tranche</i>
3. Stutzer should be a temporary traffic way NOT blocked off completely;	<i>Comment noted. Take this forward to the next tranche</i>
4. A public car park should be available elsewhere.	<i>Comment noted. Take this forward to the next tranche</i>
5. The car park behind the Council Chambers should be for staff and for accessible parking.	<i>Comment noted. BW: The works plan for the Council Chambers and Town Hall limits parking to staff and accessible parks.</i>
Safe pedestrian access across High St – no additional work required. If you need to have a raised road in front of the Council Chambers to reduce speed, then that is a separate issue	<i>Comments noted. All of these projects are interrelated.</i>
Stutzer St – I think that it should be closed off and removed in the area adjacent to the length of the Council Chambers	<i>Comment noted.</i>



Person's Response	Project Working Group Comments
Village Square should go to Callington Park	<p><i>Comment noted.</i> <i>Take this forward to the next tranche</i></p>
1. Landscaping MUST include permeable paving, decent large deciduous street trees, a drinking fountain, seating (ie benches)	<p><i>Comment noted. Part of detailed design tranche.</i> <i>Take this forward to the next tranche</i></p>
2. Undergrounding of the poles and wire is fabulous	<p><i>Comment noted.</i></p>
3. Perhaps some public art and interpretation of the precinct	<p><i>Comment noted. Detailed design tranche elements.</i> <i>Take this forward to the next tranche</i></p>

End of Other Comments in respect of Slides 12.



Decision required to be made



**ISSUE - PUBLIC TOILETS
AT THE REAR OF THE
COUNCIL CHAMBERS**

Current Description
The public toilets are at the rear of the Council staff car park. The design was circa 1975. The construction is of standard brick size, in Heather Brown colour. This is a very well used public amenity and according to our traffic counter approximately 50 vehicles per day use Stutzer Street just to access the public toilets.

To Change or Not to Change
Given this public amenity is well patronised it means that Stutzer Street does receive an inordinate amount of traffic, in a very tight traffic space. If we wish to reduce the traffic to enable a safer public use of the Village Square and maybe an ephemeral road way, or maybe the roadway has to be moved, to reduce traffic flow. If moving the toilet is preferred where is an alternative location?

Constraints

1. The roof pitch / form is not consistent with the heritage style of the Council Chambers building;
2. The internal layout is dated and not in line with contemporary design and universal access requirements;
3. From a location perspective having the public walk through a car park is not the safest access.

Opportunities

1. There are few options for an alternative public amenities site in the centre of the Village;
2. Alternative options/locations should be scoped prior to a determination being made.
3. The removal of the public amenity would provide additional parking space




Google Earth
Slide 13 of 26

Slide No. and Description	Statement / Question	Yes %	No %
Slide 13 – Identified Issue: Public Toilets at the Rear of the Council Chambers	<i>I understand the issue and agree that the issues identified require addressing.</i>	96%	4%
	Other comment(s): There were ten comments made in respect of this Slide		

Survey / Slide 13 – Public Toilets at the Rear of the Council Chambers

Other Comments provided by Respondents

<i>Person's Response</i>	<i>Project Working Group Comments</i>
1. Refer to my attached – site plan ideas	<i>Attachment noted. Take this forward to the next tranche</i>
2. Retain toilets here, new building, increase the size and make for all abilities with a 10 minute parking spot in front of them off Stutzer St.	<i>Comment noted. In the survey 83% of Respondents wanted the toilets to be removed.</i>
The toilets service council staff, removal necessitates in-house facility upgrade, so may be happens at a later stage as part of a different but associated project	<i>Comment noted.</i>
The extra space, simplicity, car parking and logic work. Why not have it all in the fine brown gravel as Hobart Domain Car parking / rivulet paths?	<i>Comment noted. Fine brown gravel is a significant maintenance issue and line marking is challenging. In relatively flat surfaces stormwater ponding does become an issue. It is acknowledged that all surface finishes require detailed consideration in a heritage setting. Take this forward to the next tranche.</i>
New public toilets are needed. Somewhere more accessible than behind the Council Chambers. Along the main street area would be best. A new toilet block in keeping with the heritage style	<i>Comment noted. Take this forward to the next tranche.</i>
1. The existing toilets are well known and in a central location similar to the location in Campbell Town. They should be re-built in their current location.	<i>Comment noted. In the survey 83% of Respondents wanted the toilets to be removed. No further action on this item.</i>
2. If they are moved to the Barrack St location, then it would make sense that Callington Park should be the Village Square. It would have space/toilet facilities / excellent parking area;	<i>Comment noted. In the survey 70% of Respondents did not want the toilets to be relocated to the Barrack St car park.</i>
3. Moving the toilets to a separate location would place a strain on the Aquatic Centre and staff.	<i>Comment noted. That would depend on where the alternative location was to be sited. Take this forward to the next tranche.</i>



Person's Response	Project Working Group Comments
<p>1. Replace the toilets, not removed. After all they are in a good central location and easy access for tourists and locals.</p>	<p><i>Comment noted. In the survey 83% of Respondents wanted the toilets to be removed. No further action on this item.</i></p>
<p>2. I suggest we remove the toilets in Stutzer St and replace with a disability toilet, etc.</p>	<p><i>Comment noted. Any new toilet will be required to be all abilities accessible. No further action on this item.</i></p>

End of Other Comments in respect of Slides 13.



**POSSIBLE RESPONSE
– A NEW PUBLIC
TOILET IN BARRACK
STREET CARPARK**

Examples of New Public Toilets and Amenities.

Oatlands TASMANIA

This location would work for servicing the proposed Village Square, also events in Callington Park and also general traveller requirements.

Slide 14 of 26

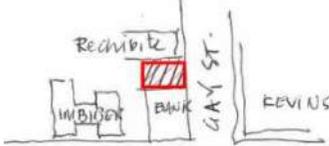
Survey Responses on the next page



Slide No. and Description	Statement / Question	Yes %	No %
Slide 14 – Possible Response - Remove the current Public Toilets - New Public Toilets - New Location	<i>I agree with the proposed concept to address the issue.</i>	89%	11%
	<i>I agree that we remove the existing public toilet facility</i>	83%	17%
	<i>I agree that we need a new public toilet facility in another location.</i>	74%	26%
	<i>I agree with the proposed location in Slide 14, in the Barrack St car park.</i>	30%	70%
	<i>Do you have another site suggestion (if so, please share with us the location and reason for that location in the comments below).</i>	69%	31%
	<i>Other comment(s):</i> There were fifteen comments made in respect of this Slide		

**Survey / Slide 14 – Possible Response: Remove the current Public Toilets; New Public Toilets; New Location
Other Comments provided by Survey Respondents**

Person's Response	Project Working Group Comments
Possible New Location - What about behind the Post Office in the exchange in Church St	<i>Comment noted. Good suggestion – need to explore the possibility</i>
Possible New Location – Back of Roche Hall adjacent to Campbell St	<i>Comment noted.</i> <i>BW: Archaeological issues would preclude this. This would also be a lost opportunity for interpretation of the convict probation/road station on that site. This would probably not gain Tasmanian Heritage Council approval due to likely heritage impact.</i>
Possible New Location – Back of Roche Hall adjacent to Campbell St	<i>Comment noted.</i> <i>BW: Archaeological issues would preclude this. This would also be a lost opportunity for interpretation of the convict probation/road station on that site. This would probably not gain Tasmanian Heritage Council approval due to likely heritage impact.</i>

Person's Response	Project Working Group Comments
Refer to my attachment	<i>Comment noted.</i>
1. The suggested second location at the Park site concerns me from a safety and vandalism perspective;	<i>Comment noted. In the survey 70% of Respondents did not want the toilets to be relocated to the Barrack St car park.</i>
2. Maybe an alternative suggested location could be the 'handle' at Brad Williams site on Gay St, could be purchased.	<p><i>Comment noted.</i></p> <p><i>BW: Brad Williams has declared an interest in this item and is not to be included in any discussion on this matter as part of the working group.</i></p> <p><i>Take this forward to the next tranche.</i></p>
	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
A good idea & frees up the space behind the council chambers	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
I think that the public toilets need to be in a location close to the town centre, preferably behind the Town Hall, in the same site but new building	<p><i>Comment noted.</i></p> <p><i>In the survey 83% of Respondents wanted the toilets to be removed. No further action on this item.</i></p>
Try the Church St area	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
If there is enough room – 20 Church St, opposite the Kentish Hotel and the Post Office.	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
The design of the new toilet facilities on slide 14 is good, but leave in current position.	<p><i>Comment noted.</i></p> <p><i>In the survey 83% of Respondents wanted the toilets to be removed. No further action on this item.</i></p>
There are already new facilities in the lake side camp and at the children's playground.	<i>Comment noted.</i>



Person's Response	Project Working Group Comments
<p>When events are underway the proposed location for the public toilets is a distance away and I believe it would be better if it was much closer to the Square & Town Centre. Possible alternative may be on the side of Roche Hall car park being more central to both proposed Square and Callington Park</p>	<p><i>Comment noted.</i> <i>Take this forward to the next tranche.</i></p>
<p>If Stutzer St is to be closed off at the High St end it would make sense to move the loos to the Barrack St site. HOWEVER putting my staff hat on this toilet block MUST NOT BE MOVED until the ladies toilet sink upstairs in the Council Chambers building is fixed and the lift installed. Why couldn't the Roche Hall toilets be modified to become the new and upgraded & safer public toilets. This corner of the building (Roche Hall) is handy to Campbell St, the Village Square, pedestrians and passing locals as well as tourist alike and doesn't need to be accessed through a car park.</p>	<p><i>Comment noted.</i> <i>BW: as per above re – upgrade of Council toilets. Consideration was given to public toilets in the rear of Roche Hall, however this cannot be accommodated with the space requirements of the PRISM program which Council has endorsed and works are underway. This option is also not likely to gain Tasmanian Heritage Council approval.</i></p>
<p>In addition to the existing toilet maximise the space at the back of 68 High St (MMCC) and part of 70 High St (Aquatic Centre)</p>	<p><i>Comment noted.</i> <i>Take this forward to the next tranche.</i></p>

End of Other Comments in respect of Slides 14.



ISSUE - CAR PARKING AT THE REAR OF THE COUNCIL CHAMBERS

Current Description
The car parking is currently used by Council staff. Members of the public wander through to use the public toilet. The access into the car park is constrained by the rather narrow access through the fence line opening. Currently the car park accommodates – seven vehicles

To Change or Not to Change
With the opportunity to relocate the public toilets it will provide additional parking spaces plus a Disability Access parking area

Constraints
The Council staff car parking is currently constrained by the public toilet and the rather tight access through the boundary fence, plus the power pole.

Opportunities
Ideally there needs to be a disability parking space on this flat area with convenient access to the Council Chambers side entrance

Decision required to be made






Slide No. and Description	Statement / Question	Yes %	No %
Slide 15 – Identified Issue: Car Parking at the Rear of the Council Chambers	I understand the issue and agree that the issues identified require addressing.	83%	17%



POSSIBLE RESPONSE - CAR PARKING AT THE REAR OF THE COUNCIL CHAMBERS

As shown a revamped car park at the rear of the Council Chambers has the ability to provide additional parking spaces plus a new Disability Access Parking, with a DDA compliant walkway to the proposed side entrance of the Council Chambers building. The removal of the fence and the power pole would greatly enhance the accessibility of the car park.



Slide 16 of 26

Slide No. and Description	Statement / Question	Yes %	No %
Slide 16 – Identified Issue: Install Accessible parking bay	I agree with the proposed concept to install Accessible Parking in the carpark.	100%	0%
	Other comment(s): There were eleven comments made in respect of this Slide		

Survey / Slide 16 – Identified Issue: Install Accessible Parking Bay Other Comments provided by Survey Respondents

<i>Person's Response</i>	<i>Project Working Group Comments</i>
See attachment – plan	<i>Comment noted. Take this forward to the next tranche.</i>
1. Extra spaces: Equitable access (DDA)	<i>Comment noted.</i>
2. Remove fence & poles for safety and improved visibility	<i>Comment noted. Take this forward to the next tranche.</i>
3. Essential & improve the secondary council building entrance. Removal or rationalise alternative egress.	<i>Comment noted. Take this forward to the next tranche.</i>
Make a car park behind Roche Hall	<i>Comment noted. This cannot be a finite solution but can still be used as an interim solution.</i>
A disability parking space can be installed now, it isn't reliant on a Village Square.	<i>Comment noted. Take this forward to the next tranche.</i>
I think that this is a practical solution	<i>Comment noted.</i>
Agree with disability parking the rest should remain.	<i>Comment noted.</i>
1. I agree that this area needs looking at. I would like to see the area designated as Staff Car Parking 08.30 – 5.00 but disabled for all hours.	<i>Comment noted.</i>
2. Great idea to remove entrance pillars and modify/remove fencing as well as the power pole	<i>Comment noted. Take this forward to the next tranche.</i>
3. I would personally like to see the lawn area in the corner redeveloped as an outdoor staff lunch area with appropriate shade/shelter/seating instead of being turned into more car parking	<i>Comment noted. Take this forward to the next tranche.</i>



STUTZER STREET

Decision required to be made

Current Description
The public toilets in the carpark at the rear of the Council Chambers generate a considerable amount of traffic in Stutzer Street (100 vehicle movements per day according to the traffic counter across the road with 50 of those for the Public Toilet usage alone). The geometry of the road and footpath is less than satisfactory, with the grades and angles that people have to navigate. Clearly improvements are required for the access to the Council Office. The reason for minimising traffic on Stutzer Street is to try reduce movement across a connected Village Square between the two frontages of the Council Chambers and Roche Hall, as referred to in the Oatlands Structure Plan.

To Change or Not to Change
There are very good reason to change the traffic arrangements and the geometry of the pavement surfaces. Removing the current constrains of dangerous footpath and poor road surface, plus the power pole & stay, plus narrow access for the Council staff carpark

Constraints
The need to underground the power and consider the implications of stormwater disposal from the road pavement.

Opportunities
Close off Stutzer Street between the Council Chambers and High Street. Or, making Stutzer Street a one way street would greatly improve pedestrian and motorist safety. The one way traffic movement (narrower width) would allow a viable transition 'treatment' in the extension of the Village Square section between the frontage of the Council Chambers and Roche Hall.

Current traffic flow

Stutzer Street looking North West
Current Pavement Surfaces which are not Pedestrian Friendly

Should there be a connection between these two High St. facing public spaces either side of Stutzer St as suggested in the Community consultation ?

Google Earth
Slide 17 of 26

Survey Responses on the next page



Slide No. and Description	Statement / Question	Yes %	No %
Slide 17 – Possible Response Stutzer St Footpath is has too much slope Roadway camber makes parking a problem	<i>I agree that the issues identified require addressing</i>	96%	4%
	<i>I agree that the footpath has too much slope and is dangerous</i>	92%	8%
	<i>I agree that the camber on the roadway makes parking a problem.</i>	91%	9%
	<i>I agree that there should be a Village Square connection between the frontage of Roche Hall and the frontage of the Council Chambers.</i>	86%	14%



Survey Responses on the next page

Slide No. and Description	Statement / Question	Yes %	No %
Slide 18 – Possible Response: Reconstruction of Stutzer Street	<i>I agree with the proposed concept to reconstruct Stutzer St with one-way for half the road, then with one-way plus parking for the balance of Stutzer St to the rear boundary of Roche Hall.</i>	48%	52%
	NOTE: <i>Notwithstanding the abovementioned question/response, a more detailed analysis of this question and the comments of the Respondents, realised that whilst Yes / No referred to a one way design of part of Stutzer St, ie should the Village Square go right across in front of Roche Hall and the Council Chambers in one large space, unencumbered by a roadway, or should a one way roadway be included as an ephemeral or transient roadway that could be blocked off when events take up the whole of the space in front of the buildings. There was another response that people made, ie maintain the street as it is. So the figures read Yes = 11 (48%), No = 8 (35%) & 'Leave as is' = 4 (17%).</i>	48% One way part of Stutzer St	35% Block off Stutzer St 17% Leave as is
	<i>Other comment(s): There were twenty three comments made in respect of this Slide</i>		

Survey / Slide 18 – Possible Response: Reconstruction of Stutzer Street

Other Comments provided by Respondents

Person's Response	Project Working Group Comments
1. See attached	<i>Comment noted. Take this forward to the next tranche.</i>
2. I think it should be closed to through traffic from High St to the end of Council's office title (rear carpark).	<i>Comment noted. Take this forward to the next tranche.</i>
3. Parking access only for staff from the rear, with 10 minute parking for toilet use, including DDA parking.	<i>Comment noted. In the survey 83% of Respondents wanted the toilets to be removed. No further action on this item.</i>
4. The road pavement should be raised to create a walkable area as part of the Village Square – refer to my attachment, there is a sketch that show this.	<i>Comment noted. Take this forward to the next tranche.</i>
5. With Stutzer St closed, opportunities are opened up, including;	<i>Comment noted.</i>



Person's Response	Project Working Group Comments
<ul style="list-style-type: none"> a. Better access into the building; b. Better visual amenity; c. Increased public open space; d. A welcoming council office building; e. Space for an external glass lift (DDA compliance); f. Space for a larger Village Square with greater impact and use, and g. Space for people and not cars. 	<p><i>Take this forward to the next tranche.</i></p>
<p>1. It solves the problem of being able to provide a cul-de-sac arrangement if the street is truncated & car park but retains two way traffic</p>	<p><i>Comment noted. Take this forward to the next tranche.</i></p>
<p>2. The confluence of people with vehicles becomes a management issue when events are held and may simply be dealt with by removable or retractable bollards.</p>	<p><i>Comment noted. Take this forward to the next tranche.</i></p>
<p>1. It is over thinking a simple location and wasting money. In the past it was the height of good urban street works.</p>	<p><i>Comment noted. The geometry of the road pavement and the footpath create safety and access issues are required to be resolved. Take this forward to the next tranche.</i></p>
<p>2. Removing the fencing, light poles, moving the toilets will give you the staff car parking and visitor parking needed. Making it one way will just make it look like you have nothing else to spend money on.</p>	<p><i>Comment noted. Take this forward to the next tranche.</i></p>
<p>I think that Stutzer St needs to be two way</p>	<p><i>Comment noted. Take this forward to the next tranche.</i></p>
<p>Close off Stutzer St between the Council and the old school</p>	<p><i>Comment noted. Take this forward to the next tranche.</i></p>
<p>Close off from High St to the back of the Council chambers</p>	<p><i>Comment noted. Take this forward to the next tranche.</i></p>
<p>1. Stutzer St should not be closed off, but be made into a one way street;</p>	<p><i>Comment noted. Take this forward to the next tranche.</i></p>



Person's Response	Project Working Group Comments
2. Power put underground;	<i>Comment noted. Take this forward to the next tranche.</i>
3. Drainage improved;	<i>Comment noted. Take this forward to the next tranche.</i>
4. Remove the slope of the road and footpath.	<i>Comment noted. Take this forward to the next tranche.</i>
Creating a Village Square in this location will increase traffic and parking problems in the area and I don't see how it will increase pedestrian safety. Entrance to the car park can be widened and is not subject to the creation of a Village Square. Yes, Stutzer St and the footpaths need attention, like many others in the Municipality, but you don't need a Village Square to do this. The money saved from a Village Square would be better spent on other roads that need attention	<i>Comment noted. Take this forward to the next tranche.</i>
1. I believe that maintaining traffic to Stutzer St from High St would become an 'accident risk' if grassed areas in front of the Town Hall and Roche Hall were combined	<i>Comment noted. Take this forward to the next tranche.</i>
2. I also believe that 'hedging / walling / or fencing' would be necessary along Roche Hall boundary as well as Campbell St for the same reason as 1 above.	<i>Comment noted. Take this forward to the next tranche.</i>
I think that part of Stutzer St along the length of the Council Chambers should be closed and renovated to form part of the Village Square. It would provide for better Square usage, additional parking spaces in the car park, better opportunities to develop the rear entrance to the Council building. It seems to me that there is good access to the rear of Council by cars by all other roads. There is good access to the lake side from other roads by vehicles.	<i>Comment noted. Take this forward to the next tranche.</i>



Person's Response	Project Working Group Comments
By doing this there is less parking, less access to the toilet and Council building	<p><i>Comment noted.</i></p> <p><i>In the survey 83% of Respondents wanted the toilets to be removed.</i></p> <p><i>No further action on this item.</i></p>
Close it off completely and perhaps have an expanded turn around area (or additional parking) at the little kink area at the High St end of the Roche Hall weatherboards.	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
Any new footpaths should be of a colour that tones into the surrounding buildings and NOT white cement, as seen in the recent footpaths.	<p><i>Comment noted.</i></p> <p><i>BW: The structure plan seeks a set of streetscape design guidelines that need to be progressed.</i></p> <p><i>Take this forward to the next tranche.</i></p>

End of Other Comments in respect of Slides 18.



PUBLIC ACCESS TO THE COUNCIL CHAMBERS

Access is required to comply with the legislation DDA 1992 – Must be done.



Current Description

The front entrance to the Council Chambers is a grand entrance in keeping with the status of this historic building. However under the disability Discrimination Act 1992 we need to provide all abilities access to the building which will need to enhance what we currently now have.

To Change or Not to Change

We have no alternative but to provide an all abilities access to the building under the Act. When combining the current 'limited' access through twin doorways, along with the substandard footpath and step to the entry path, the access does not comply.



Slide 19 of 26

Slide No. and Description	Statement / Question	Yes %	No %
Slide 19 – Identified Issue: Provide better access to the side entrance of Council Chambers	I agree that the issues identified require addressing	100%	0%
	Other comment(s): There were twelve comments made in respect of this Slide		

Survey / Slide 19 – Identified Issue: Provide better access to the side entrance of Council Chambers
Other Comments provided by Respondents

<i>Person's Response</i>	<i>Project Working Group Comments</i>
1. Arriving at a solution to this is an aesthetically sensitive one preserving the character & integrity of the Chambers while introducing equitable and seamless access	<p><i>Comment noted.</i></p> <p><i>BW: see comment above re Town Hall access.</i></p> <p><i>Take this forward to the next tranche.</i></p>
2. Also internal vertical transportation to activate the 1 st floor without foyer intrusion	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
1. The DDA is simply solved by having a ramp, with an Oatlands stone side, gravelled in brown gravel, to the present and original front doors of the Council Chambers. Please don't for the sake of DDA make the original façade redundant.	<p><i>Comment noted.</i></p> <p><i>BW: see comment above re Town Hall access.</i></p> <p><i>Take this forward to the next tranche.</i></p>
2. Also, a glass and steel tower to the side will not look invisible or polite. Glass and steel are reflective, intrusive and dominate.	<p><i>Comment noted.</i></p> <p><i>BW: The design resolution for this proposal has been a long-running and iterative process between the Designer, Fire Engineer, Building Surveyor and Access Consultant. I think this comment seems to be tied into the 'glass elevator' concept – which is not proposed (nor feasible). The next step is informal discussion with Heritage Tasmania ahead of a DA submission, where the public can make comment on the proposal.</i></p> <p><i>Take this forward to the next tranche.</i></p>
3. Can the fire stair be removed, and a lift tower, clad in weather boards, painted in a flat oil paint in a pale stone colour be considered? Secondary structures to Victorian stone buildings done in weatherboard have a receding and sympathetic feel. If it had a clerestory (roof lantern) it would be more energy efficient and be a nice space to be in. The Hobart City Council lift is very small, most people use the stairs. And it all complies and is used by thousands of people a year.	<p><i>Comment noted.</i></p> <p><i>BW: As above</i></p> <p><i>Take this forward to the next tranche.</i></p>



Person's Response	Project Working Group Comments
I think that the side entrance to the Council Chambers needs to be retained but upgraded.	<p><i>Comment noted.</i></p> <p><i>BW: As above</i></p> <p><i>Take this forward to the next tranche.</i></p>
The main entrance needs to remain at the front of the building. The side entrance should be made for people with disability needs. The footpath and roadway needs the slope and drainage addressed.	<p><i>Comment noted.</i></p> <p><i>BW: As above</i></p> <p><i>Take this forward to the next tranche.</i></p>
Again this issue is not subject to a Village Square.	<p><i>Comment noted.</i></p> <p><i>BW: Correct, the Town Hall upgrades need to happen – however the planning of such needs to integrate and not be something which may inhibit the town square project (and vice-versa).</i></p>
I agree that better access should be provided in particular for people with a disability. Remove the old fire escape, as it is not in keeping with the heritage of the building and is questionable as it has a window adjacent to it (fire safety). An internal stair should be considered in addition to the lift.	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p> <p><i>BW: The Fire Engineer has advised that the existing stair can become the fire stair when the lift is installed, therefore the exterior fire stair can go</i></p>
This has nothing to do with the Village Square. This is a Council matter	<p><i>Comment noted. All of these elements that have been discussed in the Slides are interrelated with the Village Square development within the precinct.</i></p>
Oh yes! Lovely glass sliding door thanks. Better weather protection too. Disabled access improves access for elderly and parents with pushers too!	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
Not to change the front entrance to the Council Chambers	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p> <p><i>BW: see comment above re Town Hall access.</i></p>

End of Other Comments in respect of Slides 19.



Slide 20 - No Response was required as this slide was providing project context only

**RESPONSE -
PUBLIC ACCESS
TO THE
COUNCIL
CHAMBERS**

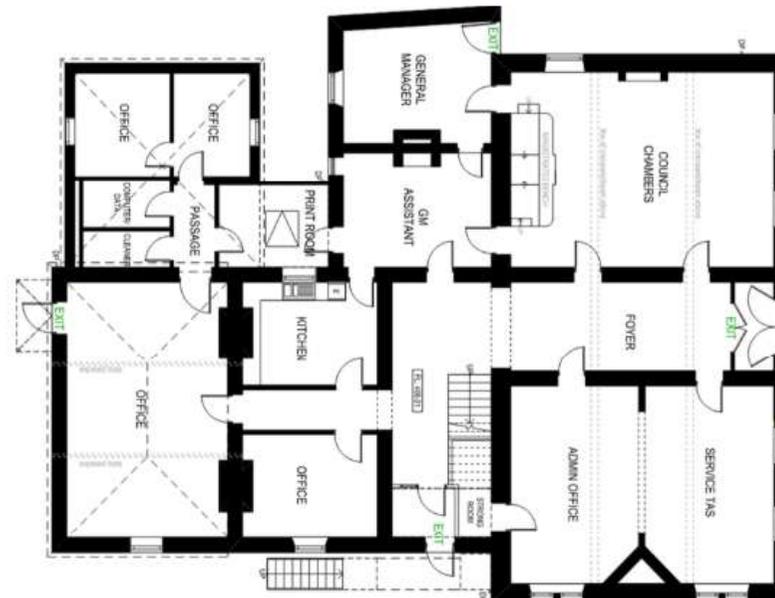
The side entrance of the Council Chambers and the Town Hall will become the 'main entrance' as part of the redevelopment works to comply with the DDA, therefore there needs to be a clear and prominent 'frontage' from Stutzer Street.

The new entry 'structure' isn't as simple as it would seem, as it has to incorporate fire-engineer advice (likely staged removal of the existing fire-stair in parallel with the progress (or not) of the lift) as well as Access Consultant advice.

Council's brief to the Manager Heritage Projects, was to incorporate that advice, and to provide a solution that is fit-for-purpose, but as 'ephemeral' as possible - lightweight and detached from the building, probably highly glazed, modern but sympathetic. How that might combine all of those requirements we are not sure as yet, so we are awaiting some concept designs.

This issue is Work in Progress

Existing North East Elevation
Stutzer Street View



Existing Floor Plan



FENCING AROUND THE COUNCIL CHAMBERS

Current Description
The cement rendered posts along with the tube rails that constitute the fence around the Council Chamber building was constructed following the removal of the picket fence that was previously in that location. The fence is a barrier to inclusion, it has been observed that during ANZAC and Remembrance Day services, the members of the public, by enlarge remain outside the fence line. This would also be a barrier in providing a connecting space between the front spaces of the Council Chambers and Roche Hall, ie an enhanced Village Square

To Change or Not to Change
The removal of the majority of the fence, apart from an historical cue, say the section that is on the boundary between the Council Chambers and 69 High Street would not impede the objectives of inclusion with the remaining of space. However a definition of the original title boundary is important and could be at ground level flush with the ground level.



Decision required to be made




Slide 21 of 26

Slide No. and Description	Statement / Question	Yes %	No %
Slide 21 – Identified Issue: Current fencing around the Council Chambers	I agree that the issue identified requires addressing by the fence being removed	91%	9%
	Other comment(s): There were eleven comments made in respect of this Slide		

Survey / Slide 21 – Identified Issue: Current fencing around the Council Chambers
Other Comments provided by Respondents

<i>Person's Response</i>	<i>Project Working Group Comments</i>
It's got to go!	<p><i>Comment noted.</i></p> <p><i>BW: as per above re fence.</i></p> <p><i>Take this forward to the next tranche.</i></p>
It's got to go!	<p><i>Comment noted.</i></p> <p><i>BW: as per above re fence.</i></p> <p><i>Take this forward to the next tranche.</i></p>
See attached – Great idea to remove the fence, take it all out.	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
It is a barrier that is obsolete & is an obstruction to the broader use of the contiguous open space. If you did nothing else – remove the fence and retain the High St piers with post top lights	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p>
<p>Removing all fences can give the buildings, as is the case at present, a lost in space, lunar and barren quality. There needs to be some containment. If by hedges, large trees planted to the corners of the six sided square (ie oak / Radiata / ornamental pink hawthorn), in a language of late 19th century plantings already found in Oatlands / as previous. Conversely, a carefully remade and scaled picket fence to 19th century evidence could contain the whole square and could look very jolly. It would present good heritage credentials is done with care, quality and grand scale.</p> <p>The town squares of Westbury and Bothwell are both wonderful examples of this. Also the intersection of William / Marlborough and Wellington St Longford serves as town square / cemetery / park.</p> <p>It is not particularly neat or municipal, but it is made beautiful by the good pinetum of interesting pines, plantings and simple gravel paths.</p>	<p><i>Comment noted.</i></p> <p><i>BW: The design of any revised landscaping will need to form part of a future public consultation program, which will also be required in the development approval process for any proposed works. A balance will need to be struck between making the spaces in front of Roche Hall and the Town Hall pleasant, practical and usable for a variety of purposes, but it is also considered important to retain each buildings' sense of identity. It is expected that a Landscape Architect with heritage credentials will need to be engaged, so that the plan is informed by appropriate historical precedent.</i></p> <p><i>Take this forward to the next tranche.</i></p>



Person's Response	Project Working Group Comments
<p>If the town square is done, could the hedges be in hawthorn, with animals or figures clipped – as at corner of High St and Wellington St Oatlands and along the Midlands Highway at Kenmore Arms / St Peter's Pass. Keep the vernacular, the charm and the rural / heritage language.</p> <p>Town squares in Brighton UK, Paris and Regent's Park in London have had a great revival by the use of gravels, grass, timber benches (not cold steel or iron) and traditional bedding plantings. It gives older people a source of delight, and is a subject of pride, beauty and colour. Please don't let architects design it – they are terrible landscapers that make cold and inhuman spaces based on irrelevant international / hip precedents. Anne Cripps is an excellent local landscape designer / garden historian.</p> <p>She did the landscape design for the Royal Engineers Building Hobart at the start of Davey St / Brooker Ave, the new old gardens for Narryna in Hampden Rd Battery Pt (which is now much used for weddings / spirit tasting events / community markets). Narryna's beautiful trad garden has become Battery Pts missing town square now - because it is well researched, intelligent and beautiful -like Oatlands!</p>	
<p>Close it off – about time</p>	<p><i>Comment noted.</i> <i>Take this forward to the next tranche.</i></p>
<p>The fence does not need to be removed, but should be replaced with something more in keeping with the heritage style of the town.</p>	<p><i>Comment noted</i> <i>BW: See comments above re – Town Hall fence.</i> <i>Take this forward to the next tranche.</i></p>
<p>The removal of the fence is not dependant on a Village Square. The fence does not enhance anything in the area. The comment re ANZAC Day and Remembrance Day is not a valid argument, in all situations/meeting, they fill from the rear, and people don't fill from the front.</p>	<p><i>Comment noted.</i> <i>BW: See comments above re – Town Hall fence.</i> <i>Take this forward to the next tranche.</i></p>
<p>Remove the brick rendered fence</p>	<p><i>Comment noted.</i> <i>Take this forward to the next tranche.</i></p>
<p>Sounds great! Some nice big shady trees along the lower Stutzer St precinct please!</p>	<p><i>Comment noted.</i> <i>Take this forward to the next tranche.</i></p>



SAFE PEDESTRIAN ACCESS ACROSS HIGH STREET

Decision required to be made

Current Description
Safe access across High Street is a challenge for some older folk in the community, with the road being so wide.

To Change or Not to Change
It needs to be improved.




Slide No. and Description	Statement / Question	Yes %	No %
Slide 22 – Identified Issue: Safe Pedestrian Access across High Street	I agree that the issue identified requires addressing	93%	7%



Slide No. and Description	Statement / Question	Yes %	No %
Slide 23 – Identified Issue: Comprehensive Pedestrian Crossing arrangement	I agree with the proposed concept to construct a wide-ranging elevated access arrangement to connect the public spaces on High Street	82%	18%
	Other comment(s): There were fifteen comments made in respect of this Slide		

**Survey / Slide 23 – Identified Issue: Comprehensive Pedestrian Crossing arrangement
Other Comments provided by Respondents**

<i>Person's Response</i>	<i>Project Working Group Comments</i>
Yes please	<i>Comment noted. Take this forward to the next tranche.</i>
The speed limit needs to be 40kph from Wellington St to just past Mill Lane	<i>Comment noted. State Government will not approve a 40kph speed limit in Oatlands. No further action on this item.</i>
The speed limit needs to be 40kph from Wellington St to just past Mill Lane	<i>Comment noted. State Government will not approve a 40kph speed limit in Oatlands. No further action on this item.</i>
1. See attachment;	<i>Comment noted. Take this forward to the next tranche.</i>
2. This can be achieved with landscaping and using the existing kerb extensions as well as planting large canopy trees in High St (you need a landscape architect for this not an engineer);	<i>Comment noted. Take this forward to the next tranche.</i>
3. Not on High Street – there are other ways to achieve the same thing, seek advice from a landscape architect, not an engineer.	<i>Comment noted. Take this forward to the next tranche.</i>
I would commend extending it as far as the Council car park entrance in Stutzer St.	<i>Comment noted. Take this forward to the next tranche.</i>
1. This could be done by putting 30km / pedestrian zone signs at the start of Wellington / High St and to northern end of town near history room to stop people speeding;	<i>Comment noted. State Government will not approve a 30kph speed limit in High St Oatlands. No further action on this item.</i>
2. It's a country town High St, not an inner urban zone. Please don't dumb it down and make it look too tame or municipal grandma.	<i>Comment noted. Take this forward to the next tranche.</i>



Person's Response	Project Working Group Comments
The same application could be applied to Stutzer St	<i>Comment noted. Take this forward to the next tranche.</i>
An extra pedestrian crossing is not subject to having a Village Square. I am not against another crossing, but just because there is a crossing, doesn't mean that people use it. A raised road pavement again can be done without a Village Square and would reduce speed for most people. But would present a challenge for hoons.	<i>Comment noted. Take this forward to the next tranche.</i>
The proposed road elevation and colour change would work.	<i>Comment noted. Take this forward to the next tranche.</i>
This is not a Village Square proposal. It is outside the Village Square	<i>Comment noted. But this is very relevant in close proximity to the Village Square for safety reasons.</i>
Excellent idea. How about some more large trees and permeable paving to support the? And for some innovative stormwater design that allows infiltration and filtering of roadway/footpath runoff through appropriately planted garden beds? Tassie is getting warmer and dryer. We need to plan for this.	<i>Comment noted. Take this forward to the next tranche.</i>
There needs to be a safer crossing further up High St near the restaurants and chemist.	<i>Comment Noted. There is one planned for the general location of Mill Lane.</i>

End of Other Comments in respect of Slides 23.



CAR PARK AT THE REAR OF ROCHE HALL

Current Description
Currently used as a car park on an adhoc basis, although it is a significant historical site, archeologically.

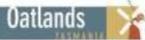
To Change or Not to Change
The Manager Heritage Projects states, "I have a good deal of reluctance in utilising the rear of Roche Hall as a designated carpark. I know it is informally used that way now, but being a very significant heritage / archaeological site (i.e. the Oatlands first settlement site, Barracks, road/probation station). It is inappropriate for it to be simply used as parking in any future sense.
There is definitely no scope for new buildings on this site.

Constraints
As detailed above.

Opportunities
Pending future archaeological investigation, it could be used as a performing arts, performance space.

This site is not an option for a formal car park due to its historical significance



Slide 24 of 26

CAR PARK AT THE REAR OF ROCHE HALL

This is the Oatlands first settlement site of 1822 and therefore a highly significant location.

Overlay of tracings of the 1835 (blue), 1850s (green, orange and brown) – depicting different building materials as well as parts of the old Barracks to be demolished and c1839 (yellow) surveys of the Barracks building, over a c2012 rectified aerial image (Southern Midlands Council).



Slide 25 of 26

**Survey / Slide 24 – Identified Issue: Identified Issue Possible use of the rear of Roche Hall as a formal car park
Other Comments provided by Respondents**

Person's Response	Project Working Group Comments
<p>This has been discounted due to its archaeological significance.</p> <p>Could it be gravelled to match Council carpark when toilets are demolished and work as an occasional overflow zone for square?</p> <p>If all surfaces match – ie all brown gravel, then you can use the car parking behind the council chambers, town square and Roche Hall archaeological zone as public open space from time to time</p>	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p> <p><i>BW: Continuation of informal parking behind Roche Hall is likely to continue. My advice is to not formalise this parking with impervious paving, infrastructure etc. The forthcoming Conservation Management Plan for Roche Hall will analyse the archaeological potential of the site and make recommendations for its use as an interpretive space (however this does not necessarily preclude other 'passive' uses that add to the amenity of the town centre.</i></p>



Person's Response	Project Working Group Comments
Roche Hall – if it is of such significant archaeological value it should be stopped being used as a car park now and some acknowledging it as such.	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p> <p><i>BW: As above.</i></p>
Please keep the adhoc parking at the rear of Roche Hall	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p> <p><i>BW: As above.</i></p>
The car park at the rear of Roche Hall should not interfere with remnants of past usage	<p><i>Comment noted.</i></p> <p><i>Take this forward to the next tranche.</i></p> <p><i>BW: As above.</i></p>



Slides 26 - No Response was required



Thank you for taking the time to review our documentation. It would be appreciated if you could please complete the Community Response Survey Questions for each slide to assist us in understanding your views.

Oatlands Village Square Working Group

Attachments from the Comments Section

Email along with Attachments

Hi Andrew,

Please refer to the two attachments that make my submission to the survey about the proposed Village Square. I would like to be included in public consultations ongoing and if a PRG is formed I would like to be on that PRG.

I have significant experience in open space planning and urban renewal, I am also a project manager in infrastructure with over 20 years' experience. I am very pleased to see the proposed village square, the main street of Oatlands needs better amenity and visual appeal, it also needs reasons for people to stop and gather. This is lacking now, which is a shame as Oatlands is the most intact sandstone village in Australia which is something that should be built upon and exploited to get more visitation and therefore attract investment.

I am a resident of Rhyndaston and my husband and I have many friends in Oatlands and visit regularly. I currently work in Hobart, but when I retire, I plan to get a part time job in Oatlands, I think it's a great little town with much potential yet to be realised from its main street. Perhaps the village square will be a catalyst for more improvements.

I look forward to hearing about the next steps and I welcome the opportunity to be on the Community PRG.

Thank you.

Attachment 2 of 3 from the Comments Section



NOTES:
Square design themes - Develop these with the community. High level themes could include; Georgian, space for people, multifunctional, celebratory of historical significance, enabling space for the community.
Square uses - Fully identify these and build them into the brief for the concept design. High-level uses could be; passive gatherings, community events, bookable spaces, a central hub for all. This might lead onto the development of an annual calendar to activate the space.

Create a centre island the full length of High St and plant large canopied trees – this will provide a more attractive township and create visual interest it will also calm traffic

Raised table to elevate surface for better pedestrian access and use for markets/events across the whole square (Campbell St remains open to thru traffic, but closes during events)

Plant ornamental trees in existing kerb outstand in front of council office (Malus ioensis Plena – Crab Apple)

Remove fence from all around council office

Landscape the ANZAC memorial garden (include seating/lighting) and landscape grass area in front of Roche Hall to connect into Stutzer St which becomes closed to thru traffic

Stutzer St - Close to thru traffic from High St to Council car park with access only for council staff to car park from rear

Explore ways to tell the story of this space (First settlement site 1822) – include interpretation (visual/audio), maintain as open space with multiple functionality (not a car park)

New and larger all abilities toilets with car access (not car parking..10 min zone for toilet use for public)

Attachment 3 of 3 from the Comments Section



Notes and ideas:
 The Square should be designed as part of a larger network of spaces (High St, Oatlands Recreational Ground, Lake Dulverton, Etc.), with walkability as a focus, featuring historical stops and interpretation along the way.

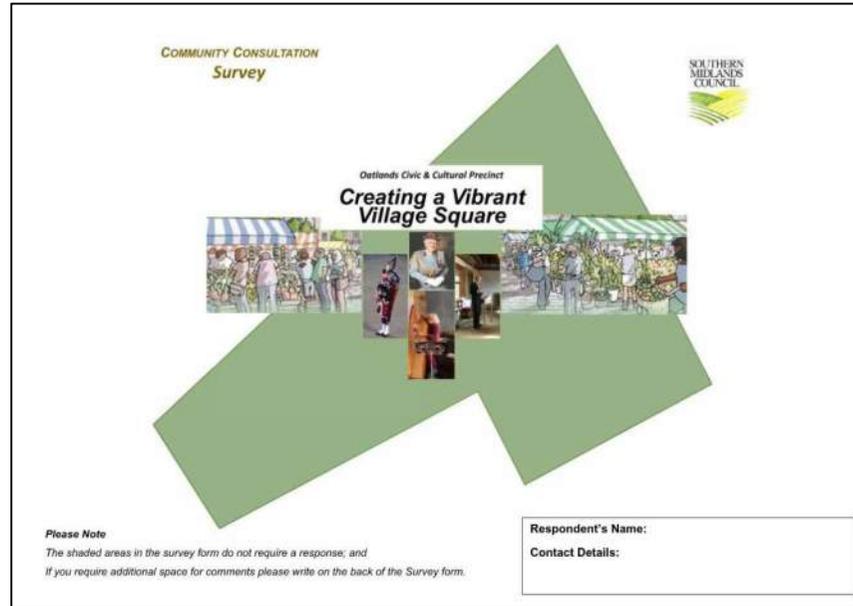
Large canopy trees along High Street would offer amenity, scale, seasonal interest, scale, shade and calm traffic. They would make Oatlands 'feel' more welcoming.

The square should have a good mix of hard and soft landscaping with seating to encourage casual gatherings as well as catering for organized events i.e., Heritage festival, ANZAC Day etc.

Decorative paving can define areas and create interest (materials should be in keeping with Georgian era)

Feature lighting can add ambience to a space and extend its use into evening.

Attachment – Survey Form Pages 1 to 3



Community Consultation Survey – Oatlands Village Square Preliminary Works

Slide No. and Description	Statement / Question	Yes ✓ No x
Slide 1 - Cover		
Slide 2 – Deputy Mayor’s Introduction		
Slides 3 to 5 – What Gave Rise to this Project?	I participated in the Oatlands Structure Plan Consultation in 2021	
	I agree with the Consultant’s recommendations in the Report	
	Other comment(s):	
Slide 6 – The Village Square as a public gathering space & its orientation		
Slide 7 – The Village Square overlaid on an aerial image		
Slide 8 – Street View of the Proposed Site		
Slide 9 – Project Timeline & Working Group		
Slide 10 – Project Site Scope	I agree with the Project Site Scope	
	Other comment(s):	

Page | 2 of 6

Community Consultation Survey – Oatlands Village Square Preliminary Works

Slide No. and Description	Statement / Question	Yes ✓ No x
Slide 11 – Public Ownership in the Site Scope		
Slide 12 – Identified Issues within the Site Scope	I agree that the issues identified require addressing.	
	Other comment(s):	
Slide 13 – Identified Issue Public Toilets at the Rear of the Council Chambers	I understand the issue and agree that the issues identified require addressing.	
	Other comment(s):	
Slide 14 – Possible Response Remove the current Public Toilets New Public Toilets New Location	I agree with the proposed concept to address the issue.	
	I agree that we remove the existing public toilet facility	
	I agree that we need a new public toilet facility in another location.	
	I agree with the proposed location in Slide 14.	
	Do you have another site suggestion (if so, please share with us the location and reason for that location in the comments below).	

Page | 3 of 6

Attachment – Survey Form Pages 4 to 6

Community Consultation Survey – Oatlands Village Square Preliminary Works

Slide No. and Description	Statement / Question	Yes ✓ No x
	Other comment(s):	
Slide 15 – Identified Issue Car Parking at the Rear of the Council Chambers	I agree that the issue identified requires addressing.	
Slide 16 – Possible Response Install Accessible bay	I agree with the proposed concept to install Accessible Parking in the carpark. Other comment(s):	
Slide 17 – Identified Issue Stutzer St Footpath is has too much slope Roadway camber makes parking a problem	I agree that the issues identified require addressing.	
	I agree that the footpath has too much slope and is dangerous.	
	I agree that the camber on the roadway makes parking a problem.	
	I agree that there should be a Village Square connection between the frontage of Roche Hall and the frontage of the Council Chambers.	

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Community Consultation Survey – Oatlands Village Square Preliminary Works

Slide No. and Description	Statement / Question	Yes ✓ No x
Slide 18 – Possible Response Reconstruction of Stutzer Street	I agree with the proposed concept to reconstruct Stutzer St with one-way for half the road, then with one-way plus parking for the balance of Stutzer St to the rear boundary of Roche Hall. Other comment(s):	
Slide 19 – Identified Issue Provide better access to the side entrance of Council Chambers	I agree that the issue identified requires addressing. Other comment(s):	
Slide 20 – Response	Currently this is in the concept design phase with the Architect.	

Page | 5 of 6

Community Consultation Survey – Oatlands Village Square Preliminary Works

Slide No. and Description	Statement / Question	Yes ✓ No x
Slide 21 – Identified Issue Current fencing around the Council Chambers	I agree that the issue identified requires addressing by the fence being removed.	
	Other comment(s):	
Slide 22 – Identified Issue Safe Pedestrian Access across High Street	I agree that the issue identified requires addressing.	
Slide 23 – Possible Response Comprehensive Pedestrian Crossing arrangement	I agree with the proposed concept to construct a wide-ranging elevated access arrangement to connect the public spaces on High Street.	
	Other comment(s):	
Slide 24 – Identified Issue Possible use of the rear of Roche Hall as a formal carpark	This has been discounted due to its archaeological significance	
Slide 25 – Response	Because of the location being a significant archaeological site this is unable to be a long term car parking location, but can continue to be used as an adhoc parking space, until that is required to cease.	

Thank you for taking the time to review our documentation and provide feedback.

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Address all correspondence to: The General Manager, PO Box 21 Oatlands, Tasmania 7120
Oatlands Office: 71 High Street, Oatlands Phone (03) 62545000 Fax (03) 62545014
Kempton Office: 85 Main Street, Kempton Phone (03) 62545050 Fax (03) 62545014
Email Address: mail@southernmidlands.tas.gov.au Web: www.southernmidlands.tas.gov.au

ABN 68 653 459 589



Deed of Termination

The Crown in Right of Tasmania
(represented by the Department of State Growth)
(Grantor)

and

Southern Midlands Council
(Recipient)

OCS NS PRECEDENTS
General Docs-Deed template-1-2014
(April 2014)

REFERENCE AND CONTACT DETAILS
Department: State Growth
Address: 2 Salamanca Square, Hobart, TAS, 7000
Telephone: (03) 6166 3368
Email: jasmin.bond@stategrowth.tas.gov.au

Doc Ref: Deed of Termination | Southern Midlands Council | \$150,000 | November 2025

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Deed of Termination

Details and recitals

Parties:

Name	The Crown in Right of Tasmania (represented by the Department of State Growth)
Short form name	Grantor
Notice details	Network Planning, 2 Salamanca Square, Hobart, TAS, 7000 Telephone: (03) 6166 3368 Email: jasmin.bond@stategrowth.tas.gov.au Attention: Jasmin Bond, Policy Project Officer

Name	Southern Midlands Council
ACN/ARBN/ABN	68 653 459 589
Short form name	Recipient
Notice details	71 High Street, Oatlands, Tasmania, 7120 Telephone: (03) 6254 5000 Email: abenson@southernmidlands.tas.gov.au Attention: Andrew Benson, Deputy General Manager

Recitals:

- A. The Grantor and the Recipient are parties to the Grant Deed.
- B. The parties enter into this Deed to:
- (i) terminate the Grant Deed by mutual consent on 18 November 2025 being the Effective Date; and
 - (ii) release each party from further obligations and liabilities arising under or in connection with the Grant Deed,

In each case on and subject to the terms of this Deed.

Operative provisions

The parties agree as follows:

1 Definitions and interpretation

1.1 Definitions

In this Deed, unless the context otherwise requires:

Business Day means a day that is not a Saturday, a Sunday, Easter Tuesday or a statutory holiday (as defined in the *Statutory Holidays Act 2000* (Tas)) generally observed in Hobart.

this Deed means this deed and includes all its annexures, appendices, attachments and schedules (if any).

Details means the details and recitals set out above.

Effective Date means 18 November 2025.

Government Body includes a body politic, a government (federal, state or local), a governmental, judicial or administrative body, a tribunal, a commission, a department or agency of any government, and a statutory authority or instrumentality.

Grant means the Grant Amount as detailed in Item 2 in the Information Table of the Grant Deed.

Grant Deed means the deed between the Grantor and the Recipient dated 18 February 2025 (a copy of which is set out as an attachment to this Deed).

1.2 Interpretation

In this Deed, unless the context otherwise requires:

- (a) Expressions defined in the Grant Deed have the same meaning when used in this Deed; and
- (b) rules of interpretation set out in the Grant Deed apply to the interpretation of this Deed.

2 Termination, release and other matters

2.1 Termination of Grant Deed by mutual consent

With effect on and from the Effective Date, but subject to the terms of this Deed, the Grantor and the Recipient agree that the Grant Deed is terminated by mutual consent.

2.2 Mutual release

Except as expressly set out in this Deed, each of the Grantor and the Recipient releases the other from all obligations under the Grant Deed arising on or after the Effective Date, including the Recipient's obligation to continue undertaking the Approved Purpose.

2.3 Antecedent breaches of Grant Deed

The termination of the Grant Deed is without prejudice to the Grantor's Rights and the Recipient's liabilities with respect to a breach by the Recipient, prior to the

Effective Date, of any of its obligations under the Grant Deed.

2.4 No admission of liability

This Deed is entered into without any admission by the Grantor as to any liability whatsoever, whether under the Grant Deed, at Law or otherwise.

2.5 No merger

This clause 2 does not merge on termination of the Grant Deed.

3 Further action

- (a) The parties agree to do or cause to be done all such acts, matters and things (including passing resolutions and executing documents) as are necessary or reasonably required to give full force and effect to this Deed.
- (b) This clause 3 does not merge on termination of the Grant Deed.

4 GST

- (a) Unless otherwise stated in this Deed, all amounts payable by one party to another party are exclusive of GST.
- (b) If GST is imposed or payable on any supply made by a party under this Deed, the recipient of the supply must pay to the supplier, in addition to the GST exclusive consideration for that supply, an additional amount equal to the GST exclusive consideration multiplied by the prevailing GST rate. The additional amount is payable at the same time and in the same manner as the consideration for the supply.
- (c) A party that makes a taxable supply under this Deed must provide a valid tax invoice to the recipient of the supply.
- (d) A party's right to payment under clause 4(b) is subject to a valid tax invoice being delivered to the party liable to pay for the taxable supply.
- (e) If the consideration for a supply under this Deed is a payment or reimbursement for, or contribution to, any expense or liability incurred by the supplier to a third party, the amount to be paid, reimbursed or contributed in respect of the expense or liability will be the amount of the expense or liability net of any input tax credit to which the supplier is entitled in respect of the expense or liability.
- (f) Where any amount payable under this Deed is paid by being set-off against another amount, each amount must be calculated in accordance with this clause 4 as if it were an actual payment made pursuant to this Deed.
- (g) Unless the context otherwise requires, expressions used in this clause 4 that are defined in the GST Laws have the meanings given to those expressions in the GST Laws.

5 Governing Law

This Deed is governed by the Law in force in Tasmania. Each party submits to the

non-exclusive jurisdiction of the courts of Tasmania.

6 Counterparts

This Deed may be executed in any number of counterparts. All counterparts will be taken to constitute one deed. The parties agree that if appropriate, they may exchange emailed copies of their signed counterparts, and those emailed copies will be taken to constitute one deed. The parties must exchange their original signed counterparts as soon as practicable afterwards.

Executed as a deed

Signing

Execution by the Grantor

Executed as a deed on behalf of **The Crown in Right of Tasmania** by the person named below in the presence of the witness named below:

Signature:
→

A person authorised to sign this Deed on behalf of the Grantor

*Print name and position:

Witness' signature:
→

* Use BLOCK LETTERS

*Witness print name and position:
PUBLIC SERVANT

*Witness print address:
4 SALAMANCA PLACE
HOBART TAS 7000

Date:
→

To be inserted at time of execution by the Grantor

Signing by the Recipient

The common seal of **Southern Midlands Council** has been hereunto affixed this _____ day of _____ pursuant to a resolution of Council delegating authority to the General Manager to affix the Corporation's Seal:

General Manager's signature:
→

Print Name*

Seal:
→

*Use BLOCK LETTERS

Attachment A: Grant Deed



Grant deed

The Crown in Right of Tasmania
(Represented by the Department of State Growth)
(Grantor)

and

Southern Midlands Council
(Recipient)

OCS NS PRECEDENTS
Grant Docs-Grant deed (short form) template-3-2014
(December 2014)

REFERENCE AND CONTACT DETAILS

Department: Department of State Growth
Contact officer: Dustin Moore
Telephone: (03) 6165 5064
Email: dustin.moore@stategrowth.tas.gov.au
CM reference: 116788/4

Doc Ref: Southern Midlands Council, \$150,000, January 2025

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Grant deed

Details and recitals

Parties:

Name	The Crown in Right of Tasmania (Represented by the Department of State Growth)
Short form name	Grantor
Notice details	Network Planning 2 Salamanca Square, Hobart, TAS, 7000 Telephone: (03) 6165 5064 Email: dustin.moore@stategrowth.tas.gov.au Attention: Dustin Moore – Urban Mobility Planner

Name	Southern Midlands Council
ACN/ARBN/ABN	68 653 459 589
Short form name	Recipient
Notice details	71 High Street, Oatlands, Tasmania 7120 Telephone: (03) 6254 5000 Email: abenson@southernmidlands.tas.gov.au Attention: Andrew Benson, Deputy General Manager

Recitals:

- A. The Grantor has agreed to provide a monetary grant to the Recipient upon the terms and conditions set out in this Deed.
- B. The Recipient has agreed to accept the Grant on the terms and conditions set out in this Deed.

Information Table

Item 1 (clause 1.1):	Approved Purpose for which the Grant is provided
To assist the Recipient in upgrading the shared path between Bagdad Community Club and Bagdad Primary School.	
Item 2 (clause 2.1):	Grant Amount
\$150,000 (one hundred and fifty thousand dollars), GST exclusive, payable by instalments in accordance with Item 3.	
Item 3 (clause 3.1):	Payment method for the Grant
The Grant is payable in one instalment, as follows: Instalment 1: \$150,000. Payment of each Grant instalment is subject to the Recipient satisfying all applicable Conditions precedent set out in Item 4.	
Item 4 (clause 3.2(a)):	Conditions precedent to payment of the Grant
With respect to: Instalment 1: <ul style="list-style-type: none"> (a) upon execution of this Deed; and (b) the submission of a Project Plan for the shared path between Bagdad Community Club and Bagdad Primary School. Once each Condition precedent has been met to the satisfaction of the Grantor, payment of the related instalment will be made on receipt of a valid invoice from the Recipient.	
Item 5 (clause 4.2):	Date for commencement of the Approved Purpose
The date of this Deed.	
Item 6 (clause 4.3):	Date for completion of the Approved Purpose
2 March 2026	
Item 7 (clause 7.2):	Reporting requirements
In addition to the reporting requirements outlined in Item 4, the Recipient must provide to the Grantor: <ul style="list-style-type: none"> (a) a Progress Report at least once every six-months from the date of the execution of this Deed; and (b) a Final Report and Financial Acquittal by 2 March 2026. The Recipient agrees to provide any additional information or reports as reasonable requested by the Grantor. All information and reports requested by the Grantor of the Recipient must be provided within twenty (20) Business Days of the Grantor's written request. All information, submissions and reports must be in a form and of a substance satisfactory to the Grantor.	

The Recipient is responsible for all reporting costs.

Item 8 (clause 10): Special terms and conditions

The Recipient agrees:

- a) to be responsible for the delivery of a design for the pathway as per the Approved Purpose, taking into account any law that may influence a design including, but not limited to, those contained in the *Strategic Infrastructure Corridors (Strategic and Recreational Use) Act 2016*, *Local Government (Highways) Act 1982*, *Roads and Jetties Act 1935* (all Tasmania);
- b) that, in the event that the Recipient desires a Significant Change to the Approved Purpose, then it must first make a written application to the Grantor. The Recipient must obtain the written approval of the Grantor before undertaking the Significant Change to the Approved Purpose;
- c) to notify the Grantor of any changes to the scope of the Approved Purpose, apply for timeline adjustments, submit milestone and final reports (if requested by the Grantor), and submit of interim and final invoices in accordance with Clause 12;
- d) to obtain, at its own cost, any and all necessary approvals from any relevant Government Body required to undertake the Approved Purpose including those relating to environmental, cultural, flora, fauna and heritage matters;
- e) to ensure that the design and any subsequent installation (subject to funding) of the pathway as per the Approved Purpose is in accordance with any applicable Australian Standards and/or Austroads Guides;
- f) to ensure all relevant insurances are obtained in relation to the Approved Purpose to the satisfaction of the Grantor;
- g) that any interest received and/or accrued on the Grant Amount is to be used for the Approved Purpose;
- h) to participate in any funding evaluation that may be undertaken by the Grantor;
- i) to give the Grantor or an Authorised Officer of the Grantor, reasonable access to any premises for the purpose of carrying out a site visit to monitor the Recipient's compliance with this Deed;
- j) without limiting anything in Clause 5, to recognise the Grantor's assistance if and when promoting the Approved Purpose, including but not limited to information placed on the Recipient's website and social media accounts, media releases, media launches and in media interviews. In such instances the phrase "*the construction of the Bagdad shared path project is supported by the Tasmanian Government through the Department of State Growth*" must be adopted;
- k) without limiting anything in Clause 5, agrees that the Grantor reserves the Right to make public (including media) announcements in relation to, and otherwise report upon the Grant, the awarding of the Grant and any Relevant Matter; and
- l) to allow photos taken and information gathered for the purpose of auditing and completing the Approved Purpose, to be used by the Grantor in both internal and publicly available databases and other publications.

GLOSSARY

Australian Standards means a standard published by Standards Australia Limited.

Austroads Guides means a guide published by Austroads.

Austroads means the peak organisation representing Australian and New Zealand road authorities.

Final Report means a written report that demonstrates that the Approved Purpose has been completed, underpinned by evidence.

Financial Acquittal means a report that includes:

- a. a statement that all funding received was expended on the Approved Purpose in accordance with this Deed; and
- b. details of expenditure in respect of the Grant.

Government Body means any body politic, any government (federal, state, or local), or any governmental, administrative or judicial body, department, authority, commission, tribunal, delegate, instrumentality or agency.

Project Plan means a plan, in respect of the Approved Purpose, that includes:

- (a) objectives;
- (b) activities; and
- (c) income and expenditure forecast.

Progress Report means a brief report that details the Recipients progress in delivering the Approved Purpose.

Significant Change means a material change to any of the scope, program or cost of the Approved Purpose from that approved by the Grantor; including the use of different infrastructure treatments from those detailed in the Application.

Agreed terms and conditions

The parties agree as follows:

1 Definitions and interpretation

1.1 Definitions

In this Deed, unless the context otherwise requires:

Approved Purpose means the purpose for which the Grant is provided as set out in Item 1.

Authorised Officer means:

- (a) if a party is the Crown or a Minister of the Crown, each of the Secretary of the department responsible for the administration of the Grant, an Acting Secretary of that department, a Deputy Secretary of that department, and a nominee of any of them; or
- (b) for any other party, a person authorised in writing by that party.

Business Day means a day that is not a Saturday, a Sunday, Easter Tuesday or a statutory holiday (as defined in the *Statutory Holidays Act 2000 (Tas)*) generally observed in Hobart.

Crown means the Crown in Right of Tasmania.

this Deed means this deed and includes all its annexures, appendices, attachments and schedules (if any).

Default Event means each of the events specified in clause 9.1.

Details means the details and recitals set out above.

Government Body includes a body politic, a government (federal, state or local), a governmental, judicial or administrative body, a tribunal, a commission, a department or agency of any government, and a statutory authority or instrumentality.

Grant means the grant paid, or to be paid, by the Grantor to the Recipient pursuant to clause 2.1.

Grantor means the person or entity named above as Grantor and, where the context requires, includes the employees, authorised contractors and agents of that person. If the Grantor is a Minister of the Crown, a reference to the Grantor includes that Minister's predecessors and successors in office (as applicable).

GST means any goods and services tax or similar tax imposed by the Commonwealth of Australia (but excluding any penalty, fine, interest or similar payment).

GST Laws means applicable Laws relating to GST.

Information Table means the table titled 'Information Table' set out above.

Item means an item in the Information Table.

Law means:

- (a) principles of law or equity established by decisions of courts;
- (b) legislation and subordinate legislation; and

- (c) requirements, approvals (including conditions) and guidelines of any Government Body that have force of law.

month means calendar month.

Recipient means the person named above as the Recipient and, where the context requires, includes the officers and employees of the Recipient.

Relevant Matter means any matter or thing related to any of the following:

- (a) the performance by the Recipient of its obligations under this Deed;
- (b) the receipt, use or expenditure of the Grant;
- (c) the carrying out of the Approved Purpose (including the effectiveness of the Recipient's carrying out of the Approved Purpose);
- (d) any report provided, or to be provided, by the Recipient to the Grantor in accordance with this Deed;
- (e) any information provided by the Recipient to the Grantor in connection with any application for the Grant;
- (f) any breach of this Deed by the Recipient;
- (g) the occurrence, or possible occurrence, of any Default Event.

Right includes a right, a power, a remedy, a discretion or an authority.

1.2 Interpretation

In this Deed, unless the context otherwise requires:

- (a) the singular includes the plural and vice versa;
- (b) words importing a gender include all genders;
- (c) other parts of speech and grammatical forms of a word or phrase defined in this Deed have a corresponding meaning;
- (d) a reference to a thing (including property or an amount) is a reference to the whole and each part of that thing;
- (e) a reference to a group of persons includes a reference to any one or more of those persons;
- (f) a reference to an annexure, an appendix, an attachment, a schedule, a party, a clause or a part is a reference to an annexure, an appendix, an attachment, a schedule or a party to, or a clause or a part of, this Deed;
- (g) a reference to any legislation or legislative provision includes subordinate legislation made under it and any amendment to, or replacement for, any of them;
- (h) writing includes marks, figures, symbols, images or perforations having a meaning for persons qualified to interpret them;
- (i) a reference to a document includes:
 - (i) any thing on which there is writing;
 - (ii) any thing from which sounds, images or writings can be reproduced with or without the aid of any thing else;

- (iii) an amendment or supplement to, or replacement or novation of, that document; or
- (iv) a map, plan, drawing or photograph;
- (j) a reference to an agreement includes an undertaking, deed, agreement or legally enforceable arrangement or understanding, whether or not in writing;
- (k) a reference to a 'person' includes a natural person, a partnership, a body corporate, a corporation sole, an association, a Government Body, or any other entity;
- (l) a reference to a party includes that party's executors, administrators, successors and permitted assigns and substitutes;
- (m) a reference to a Minister includes, as applicable, that Minister's predecessors and successors in office;
- (n) a reference to a Government Body or other body or organisation that has ceased to exist, or that has been renamed, reconstituted or replaced, or the powers or functions of which have been substantially transferred, is taken to refer respectively to the Government Body or other body or organisation as renamed or reconstituted, or established or formed in its place, or to which its powers or functions have been substantially transferred;
- (o) a reference to an office in a Government Body or other body or organisation includes any person acting in that office, and if the office is vacant, the person who for the time being is substantially responsible for the exercise of the duties, functions or powers of that office;
- (p) mentioning any thing after the words 'includes', 'included' or 'including' does not limit the meaning of any thing mentioned before those words;
- (q) a reference to a day is to be interpreted as the period of time in Tasmania commencing at midnight and ending 24 hours later;
- (r) reference to a time or date in connection with the performance of an obligation by a party is a reference to the time or date in Hobart, Tasmania, even if the obligation is to be performed elsewhere; and
- (s) references to '\$' and 'dollars' are to Australian dollars.

1.3 Headings

Headings are included for convenience only and do not affect the interpretation of this Deed.

1.4 No rule of construction applies to disadvantage party

In relation to the interpretation of this Deed, no rule of construction is to apply to the disadvantage of a party because that party was responsible for the preparation of this Deed or any part of it.

1.5 Information Table

- (a) An Item that has not been completed will be taken to be 'not applicable'.
- (b) Unless the context otherwise requires, expressions defined in the Information Table have the same meanings when used in other parts of this Deed.

2 Grant

2.1 Agreement to provide Grant

Subject to the terms of this Deed, the Grantor will provide to the Recipient the monetary grant set out in Item 2 for use by the Recipient for the Approved Purpose in accordance with this Deed.

2.2 Acknowledgments

The Recipient acknowledges and agrees that:

- (a) the Grantor's financial assistance to the Recipient in respect of the Approved Purpose is limited to the Grant;
- (b) nothing in this Deed requires the Grantor to provide any further financial assistance to the Recipient in respect of the Approved Purpose; and
- (c) the Grantor is not responsible for any liabilities incurred by the Recipient, or any obligations entered into by the Recipient, as a result of or arising out of, the Recipient's obligations under this Deed or in respect of the Approved Purpose.

3 Payment of Grant to Recipient

3.1 Method of Grant payment

Subject to clause 3.2, the Grantor will pay the Grant to the Recipient in the manner specified in Item 3. If no method of payment is specified in Item 3, the method of payment will be as determined by the Grantor.

3.2 Conditions affecting Grant payment

- (a) **(Conditions precedent):** If Item 4 includes any conditions precedent to the payment of the Grant, then the obligation of the Grantor to pay the Grant or part of the Grant is subject to the prior and continuing satisfaction of those conditions precedent (except for any of those conditions precedent waived in writing by the Grantor).
- (b) **(Default Events):** The Grantor is not required to pay the Grant (or if the Grant is payable by instalments, any instalment of the Grant) to the Recipient if a Default Event has occurred and has not been remedied to the satisfaction of the Grantor.
- (c) **(Requirement for tax invoice):** If the Grant, or any instalment of the Grant, is subject to GST, the Grantor is not required to pay the Grant (or the relevant instalment) until the Grantor has received from the Recipient a correctly rendered tax invoice in accordance with clause 11.

4 Application of Grant and related matters

4.1 Application of Grant for Approved Purpose

- (a) The Recipient must only use the Grant to undertake the Approved Purpose.
- (b) The Recipient must not change the Approved Purpose without the prior written approval of the Grantor, which approval may be given or withheld in the Grantor's absolute discretion.

- (c) The Recipient must undertake the Approved Purpose exercising reasonable skill, care and attention.

4.2 Commencement of Approved Purpose

The Recipient must substantially commence (to the Grantor's satisfaction) the Approved Purpose by the date shown in Item 5 or such later date, if any, approved in writing by the Grantor.

4.3 Completion of Approved Purpose

The Recipient must complete the Approved Purpose by the date shown in Item 6 or such later date, if any, approved in writing by the Grantor.

4.4 Compliance with Law

The Recipient must comply with all applicable Laws in expending the Grant and in carrying out the Approved Purpose.

4.5 Financial records

- (a) The Recipient must keep and maintain proper accounts, records and financial statements showing, the receipt, use and expenditure of the Grant and the carrying out of the Approved Purpose.
- (b) The Recipient's financial statements must show, as separate items, the receipt, use and expenditure of the Grant.
- (c) The Recipient must allow the Auditor-General of Tasmania (or his or her nominee) to audit, inspect, and to take copies of, the Recipient's accounts, records and financial statements relating to the receipt, use and expenditure of the Grant.

4.6 Notice by Recipient of adverse matters

The Recipient must immediately notify the Grantor in writing of:

- (a) the occurrence of any matter, event or thing, occurring after the date of this Deed, that adversely affects or materially delays the Recipient carrying out the Approved Purpose in accordance with, or the performance by the Recipient of its obligations under, this Deed;
- (b) any breach of this Deed by the Recipient; or
- (c) the occurrence of any Default Event.

5 Publicity concerning Grant and Approved Purpose

The Recipient must include in any correspondence, promotional material, public (including media) announcement, advertising material, or other publication concerning the Approved Purpose, an acknowledgement that the Approved Purpose is assisted by a grant from the Grantor. The acknowledgement must be in a form and substance approved in writing by the Grantor.

6 Repayment of Grant by Recipient

6.1 Repayment obligation

The Recipient must repay to the Grantor on demand in writing by the Grantor:

- (a) any part of the Grant that is not required by the Recipient to carry out the Approved Purpose;
- (b) any part of the Grant that is used by the Recipient for a purpose that is not an Approved Purpose; and
- (c) the Grant (or such part of the Grant as may be determined by the Grantor in its absolute discretion) if:
 - (i) the Recipient does not complete the Approved Purpose by the date shown in Item 6 or such later date, if any, approved in writing by the Grantor;
 - (ii) this Deed is terminated by the Grantor in accordance with clause 9; or
 - (iii) a Default Event occurs.

7 Review, monitoring, audit, reports and related matters

7.1 Review, monitoring or audit of Relevant Matters

The Grantor may from time to time review, monitor or audit any Relevant Matter. The Recipient must in connection with any such review, monitoring or audit by the Grantor comply with any reasonable directions of the Grantor.

7.2 Reporting

- (a) The Recipient must provide to the Grantor the reports and other documents (if any) specified in Item 7.
- (b) The Recipient must provide to the Grantor such other reports and documents as required by the Grantor from time to time in connection with any Relevant Matter. Unless otherwise stated in Item 7, nothing in that Item limits the reports or frequency of reports that the Grantor may require under this clause 7.2(b).

8 Representations and warranties

8.1 Warranties

The Recipient represents and warrants to the Grantor that all information given, and each statement made, to the Grantor by the Recipient or its agents concerning any application for the Grant, is true, correct and not misleading in any way.

8.2 No reliance by the Recipient

The Recipient acknowledges that it has not entered into this Deed in reliance on any representation, warranty, promise, statement or undertaking made by the Grantor or any person on behalf of the Grantor.

9 Default Events and termination

9.1 Default Events

Each of the following events is a Default Event for the purposes of this Deed:

- (a) **(Breach not capable of being remedied):** If the Recipient breaches any of its obligations under this Deed and the breach is not capable of being remedied.

- (b) **(Failure to remedy breach):** If:
 - (i) the Recipient breaches any of its obligations under this Deed;
 - (ii) the breach is capable of being remedied; and
 - (iii) the Recipient fails to remedy the breach within the period (being a period of not less than five Business Days) specified by the Grantor in a notice given to the Recipient detailing the breach.
- (c) **(Repudiation):** If the Recipient repudiates this Deed.
- (d) **(Natural person):** If the Recipient is a natural person, at any time before the Recipient has performed all of its obligations under this Deed, the Recipient:
 - (i) dies;
 - (ii) becomes an insolvent under administration (as defined in section 9 of the *Corporations Act 2001* (Cwlth)), or any action is taken which could result in that event; or
 - (iii) ceases to be of full legal capacity.
- (e) **(Body corporate related events):** If the Recipient is a body corporate, at any time before the Recipient has performed all of its obligations under this Deed:
 - (i) the Recipient becomes an externally administered body corporate (as defined in section 9 of the *Corporations Act 2001* (Cwlth));
 - (ii) the Recipient is dissolved, wound-up or its registration is cancelled;
 - (iii) any process or action is commenced or taken which could lead to an event mentioned in clause 9.1(e)(ii); or
 - (iv) in the opinion of an Authorised Officer for the Grantor, the corporate governance or administration of the Recipient is materially deficient or unsatisfactory.
- (f) **(Representation):** If any representation or warranty by the Recipient in this Deed is untrue, false or misleading when made or repeated.

9.2 Termination - Default Events

In addition to any other Rights, if a Default Event occurs, the Grantor may terminate this Deed by notice in writing to the Recipient. The termination takes effect when the Grantor's notice is taken to have been received by the Recipient in accordance with clause 12.3 (or any later date specified in the notice).

10 Special terms and conditions

- (a) The special terms and conditions (if any) in Item 8 form part of this Deed.
- (b) If there is any inconsistency between the special terms and conditions in Item 8 and another provision of this Deed, the special terms and conditions override the other provision to the extent of the inconsistency.
- (c) A special term or condition in Item 8 is taken not to be inconsistent with another provision of this Deed if the special term or condition and the other provision of this Deed are both capable of being complied with.

- (d) To avoid doubt and without limiting the operation of clause 13.14, any Right contained in Item 8 is in addition to any other Rights provided for in this Deed or at Law.

11 GST

- (a) Unless otherwise stated in this Deed, all amounts payable by one party to another party are exclusive of GST.
- (b) If GST is imposed or payable on any supply made by a party under this Deed, the recipient of the supply must pay to the supplier, in addition to the GST exclusive consideration for that supply, an additional amount equal to the GST exclusive consideration multiplied by the prevailing GST rate. The additional amount is payable at the same time and in the same manner as the consideration for the supply.
- (c) A party that makes a taxable supply under this Deed must provide a valid tax invoice to the recipient of the supply.
- (d) A party's right to payment under clause 11(b) is subject to a valid tax invoice being delivered to the party liable to pay for the taxable supply.
- (e) If the consideration for a supply under this Deed is a payment or reimbursement for, or contribution to, any expense or liability incurred by the supplier to a third party, the amount to be paid, reimbursed or contributed in respect of the expense or liability will be the amount of the expense or liability net of any input tax credit to which the supplier is entitled in respect of the expense or liability.
- (f) Where any amount payable under this Deed is paid by being set-off against another amount, each amount must be calculated in accordance with this clause 11 as if it were an actual payment made pursuant to this Deed.
- (g) Unless the context otherwise requires, expressions used in this clause 11 that are defined in the GST Laws have the meanings given to those expressions in the GST Laws.

12 Notices

12.1 Notice requirements

- (a) A notice, certificate, consent, application, waiver or other communication (each a **Notice**) under this Deed must be:
 - (i) in legible writing in the English language;
 - (ii) subject to clauses 12.1(b) and 12.1(c), signed by or on behalf of the sender or by a lawyer for the sender;
 - (iii) marked for the attention of the person or position (if any) specified in the Details applicable to the intended recipient of the Notice or, if the intended recipient has notified otherwise, marked for attention in the way last notified; and
 - (iv) left or sent in accordance with clause 12.2.
- (b) A printed or copy signature is sufficient for the purposes of sending any Notice by facsimile.

- (c) A Notice sent by email is taken to have been signed by the sender.
- (d) A Notice must not be given orally.

12.2 Method and address for delivery

- (a) Subject to clause 12.2(b), a Notice must be:
 - (i) left at the intended recipient's address set out in the Details;
 - (ii) sent by prepaid ordinary mail (or prepaid airmail, if from one country to another country) to the intended recipient's address set out in the Details;
 - (iii) sent by facsimile to the intended recipient's facsimile number (if any) set out in the Details; or
 - (iv) sent by email to the intended recipient's email address (if any) set out in the Details.
- (b) If the intended recipient of a Notice has notified the sender of another address, facsimile number or email address for the purposes of receiving Notices, then subsequent Notices to that intended recipient must be left at or sent to the address, facsimile number or email address (as applicable) last notified by that intended recipient.

12.3 Time of receipt

- (a) Subject to clause 12.3(b), a Notice is taken to have been received by the intended recipient:
 - (i) if left at the intended recipient's address, at the time of delivery;
 - (ii) if sent by prepaid ordinary mail, on the third Business Day after the day of posting, or if sent by prepaid airmail from one country to another country, on the tenth Business Day after the day of posting;
 - (iii) if sent by facsimile, at the time shown in the transmission report as the time when the whole Notice was sent; and
 - (iv) if sent by email, four hours after the time the email was sent (as recorded by the device from which the email was sent) provided that the sender has not received an automated message that the email has not been delivered.
- (b) If a Notice is received by a recipient on a day that is not a Business Day or after 4.00pm on a Business Day, the Notice is taken to be received at 9.00am on the next Business Day.
- (c) A Notice is effective from the time it is taken to have been received in accordance with clauses 12.3(a) and 12.3(b) (unless a later time is specified in the Notice, in which case the notice takes effect from that time).

13 Miscellaneous

13.1 Governing law

This Deed is governed by the Laws applying in Tasmania.

13.2 Dispute jurisdiction

The parties submit to the non-exclusive jurisdiction of courts with jurisdiction in Tasmania, and any courts that may hear appeals from those courts, in respect of any proceedings in connection with this Deed.

13.3 Entire agreement clause

- (a) This Deed forms the entire agreement of the parties in respect of its subject matter. The only enforceable obligations of the parties in relation to the subject matter of this Deed are those that arise out of the provisions contained in this Deed. All prior agreements in relation to the subject matter of this Deed are merged in and superseded by this Deed unless expressly incorporated in this Deed as an annexure, an appendix, an attachment or by reference.
- (b) Nothing in clause 13.3(a) affects the Grantor's Rights in connection with this Deed in relation to any information given, or statement made, to the Grantor by the Recipient, its employees or agents concerning any application for the Grant.

13.4 Liability

An obligation of, or a representation, a warranty or an indemnity by, two or more parties (including where two or more persons are included in the same defined term) under or in respect of this Deed, binds them jointly and each of them severally.

13.5 Benefit

An obligation, a representation, a warranty or an indemnity in favour of two or more parties (including where two or more persons are included in the same defined term) is for the benefit of them jointly and each of them severally.

13.6 Severance

If a provision of this Deed is or at any time becomes illegal, prohibited, void or unenforceable for any reason, that provision is severed from this Deed and the remaining provisions of this Deed:

- (a) continue to be enforceable; and
- (b) are to be construed with such additions, deletions and modifications of language as are necessary to give effect to the remaining provisions of this Deed.

13.7 Counterparts

- (a) This Deed may be entered into in any number of counterparts.
- (b) A party may execute this Deed by signing any counterpart.
- (c) All counterparts, taken together, constitute one instrument.

13.8 Further assurance

The parties agree to do or cause to be done all such acts, matters and things (including, as applicable, passing resolutions and executing documents) as are necessary or reasonably required to give full force and effect to this Deed.

13.9 No partnership or agency

- (a) Nothing contained or implied in this Deed will:
 - (i) constitute, or be taken to constitute, a party to be the partner, agent or legal representative of another party for any purpose;

- (ii) create, or be taken to create, a partnership or joint venture; or
 - (iii) create, or be taken to create, an agency or trust.
- (b) The Recipient must not represent or hold itself out to be a partner, joint venturer, agent or representative of the Grantor.

13.10 Legal costs

Each party must bear their own costs in preparing and negotiating this Deed.

13.11 Amendment

This Deed may only be amended or supplemented in writing signed by the parties.

13.12 Waiver

- (a) A failure or delay in exercising a Right does not operate as a waiver of that Right.
- (b) A single or partial exercise of a Right does not preclude any other exercise of that Right or the exercise of any other Right.
- (c) A Right may only be waived in writing, signed by the party to be bound by the waiver. Unless expressly stated otherwise, a waiver of a Right is effective only in the specific instance and for the specific purpose for which it was given.

13.13 Successors and assigns

This Deed is binding on and benefits each party and, unless repugnant to the sense or context, their respective administrators, personal representatives, successors and permitted assigns.

13.14 Rights cumulative

Each Right of the Grantor provided for in this Deed:

- (a) operates independently of any other Right of the Grantor provided for in this Deed; and
- (b) is cumulative with, and does not exclude or limit, any other Right of the Grantor, whether at Law or pursuant to any other agreement, deed or document.

13.15 Set-off

The Grantor may set-off against any moneys payable by the Grantor to the Recipient under this Deed any debt or other moneys from time to time due and owing by the Recipient to the Grantor. This right of set-off does not limit or affect any other right of set-off available to the Grantor.

13.16 No assignment

The Recipient must not assign any of its Rights and obligations under this Deed except with the prior written consent of the Grantor.

13.17 Disclosure

- (a) Despite any confidentiality or intellectual property right subsisting in this Deed, a party may publish all or any part of this Deed without reference to another party.
- (b) Nothing in this clause derogates from a party's obligations under the *Personal Information Protection Act 2004* (Tas) or the *Privacy Act 1988* (Cwlth).

13.18 Determination

Where the Grantor is required or entitled to form or hold an opinion or view under or in relation to this Deed, that opinion or view may be formed or held by an Authorised Officer for the Grantor. This clause does not limit any other way in which the Grantor may otherwise form or hold an opinion or view under or in relation to this Deed.

13.19 Consent and approvals

- (a) This clause applies to any consent or approval which the Recipient must obtain from the Grantor in accordance with this Deed. For the avoidance of doubt, this clause does not apply to any consent or approval to be given under any legislation.
- (b) A request for consent or approval must be made in writing.
- (c) A consent or approval for the purposes of this Deed is not effective unless given in writing.
- (d) A consent or approval may be given subject to reasonable conditions.
- (e) A Recipient must comply with any conditions subject to which the consent or approval is given. To the extent that the Recipient fails to comply with the condition, that failure is taken to be a breach of this Deed.

13.20 Doctrine of merger

The doctrine or principle of merger does not apply to this Deed or to anything done under or in connection with this Deed. Accordingly, no Right or obligation of a party is merged in any thing done pursuant to this Deed.

13.21 No interference with executive duties or powers

Nothing in this Deed is intended to prevent, is to be taken to prevent, or prevents, the free exercise by the Governor, by any member of the Executive Council, or by any Minister of the Crown, of any duties or authorities of his or her office. Any provision of this Deed that is inconsistent with this clause is of no legal effect to the extent of the inconsistency.

13.22 Surviving provisions and termination

- (a) The termination of this Deed does not affect or limit the operation or effect of clauses or parts of this Deed:
 - (i) that are expressed to survive the termination of this Deed;
 - (ii) that, at Law, survive the termination of this Deed; or
 - (iii) that are necessary to survive the termination of this Deed:
 - (A) to give full force and effect to the parties' respective Rights, obligations and liabilities on or after the termination of this Deed;
 - (B) to enable a party to make, enforce or defend any claims related to this Deed; or
 - (C) to give full force and effect to the operation of clause 13.22(b) or clause 13.22(c).
- (b) The termination of this Deed does not affect any claims related to, or any Rights, releases, obligations or liabilities accrued or incurred under, this Deed before the date on which this Deed is terminated.

- (c) Nothing in this clause 13.22 affects or limits the operation of another provision of this Deed which gives a party Rights, or imposes obligations on a party, on or after the termination of this Deed.

Executed as a deed.

Signing

Execution by the Grantor

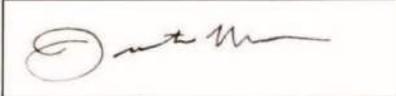
Executed as a deed on behalf of **The Crown in Right of Tasmania** by the person named below in the presence of the witness named below:

Signature: → 

A person authorised to sign this Deed on behalf of the Grantor

*Print name and position: **Adrian Gordon
A/ Manager Grant Services**

* Use BLOCK LETTERS

Witness' signature: → 

*Witness print name and position: **Dustin Moore
PUBLIC SERVANT**

*Witness print address: **4 Salamanca Place
Hobart TAS 7000**

Date: → **18 February 2025**

To be inserted at time of execution by the Grantor

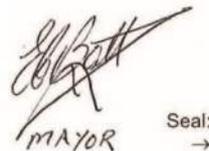
Signing by the Recipient

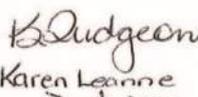
The common seal of **Southern Midlands Council** has been hereunto affixed this _____ day of _____ pursuant to a resolution of Council delegating authority to the General Manager to affix the Corporation's Seal:

General Manager's signature: → 

Print Name* **TK Kierwood
General Manager**

*Use BLOCK LETTERS


MAYOR


Karen Leanne Judgeon
Deputy Mayor

Seal: →

